



NATIONAL HIGHWAYS AUTHORITY OF INDIA

(Ministry of Shipping, Road Transport & Highways)

Consultancy services for preparation of detailed project report for
Rehabilitation and upgrading of existing 2-lane road to 4-lane divided Carriageway
Configuration of Jammu-Udhampur section of NH-1A (km 0.00 to km 66.00)
Including Jammu (part) Nagrota and Udhampur Bypass Phase-III Programme of
North-South-Corridors project, Contract Package C-III/18

DRAFT DETAILED PROJECT REPORT

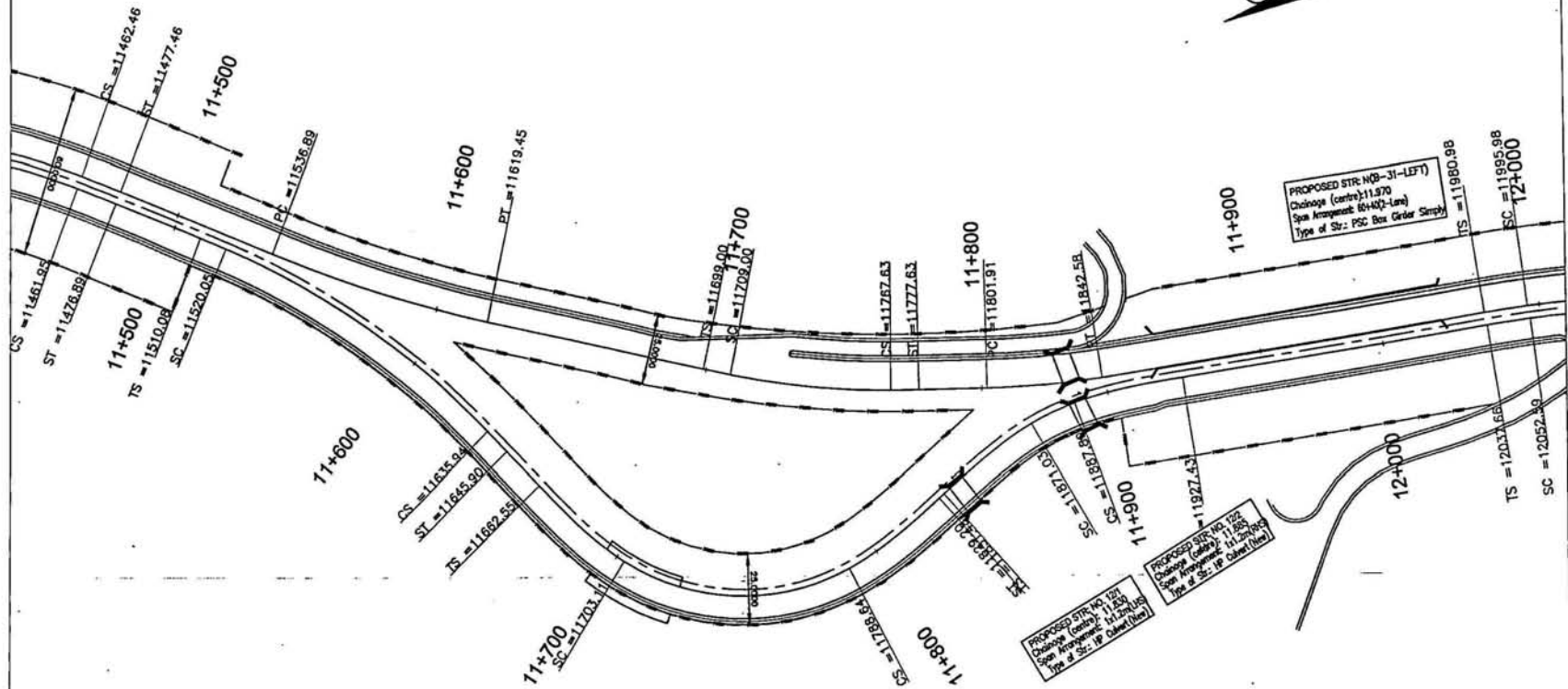
VOLUME - IX DRAWINGS

Part I A (Road Works)

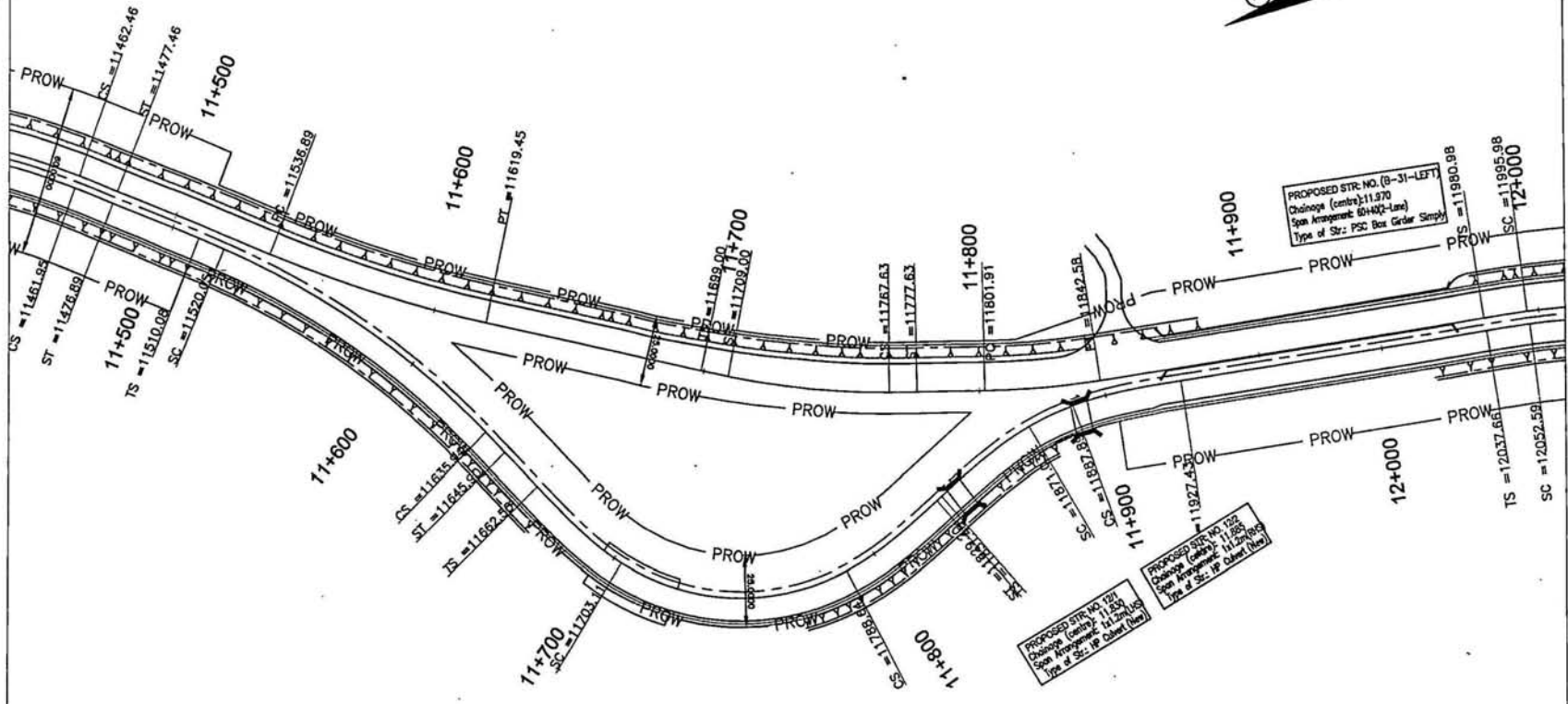
Soth
Wilson



LAND ACQUISITION PLAN CH : 11+500 TO CH: 12+000



LAND ACQUISITION PLAN CH : 11+500 TO CH: 12+000



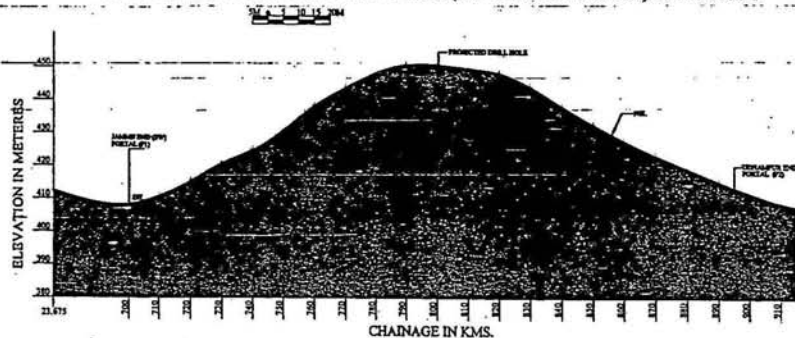
Slope Stability & Land Slide Protection Measures Package - II

| Section | Chainage | LHS | RHS | Protection Measures | Height in (m) | Type of BW | Package |
|--------------|---------------|-----|-----|---|---------------|------------|---------|
| Jammu Bypass | 19.860-20.000 | - | 140 | Breast wall 1 m along side drain | 1.00 | B1 | II |
| Jammu Bypass | 20.040-20.220 | - | 180 | Breast wall 2m along R.H.S of right carriageway | 2.00 | B1 | II |
| Jammu Bypass | 20.090-20.240 | 150 | - | Breast wall 2m along L.H.S of left carriageway | 2.00 | B1 | II |
| Main Road | 9.480-9.540 | 60 | - | Breast wall 1.5 m along side drain | 1.50 | B1 | II |
| Main Road | 23.060-23.195 | 135 | 135 | Breast wall 3 m | 3.00 | B1 | II |
| Main Road | 23.605-23.700 | 95 | 95 | Breast wall 12 m RCC at SW portal of T-1 | 12.00 | B5 | II |
| Main Road | 23.890-23.925 | 35 | 35 | Breast wall 11 m RCC at NE portal of T-1 | 11.00 | B5 | II |
| Main Road | 24.120-24.205 | - | 85 | 6 m Gabion | 6.00 | B4 | II |
| Main Road | 24.105-24.160 | 55 | - | Breast wall 6 m with Gabion | 6.00 | B4 | II |
| Main Road | 24.460-24.510 | 50 | - | Breast wall 11 m RCC at Western portal of T-2 | 11.00 | B5 | II |
| Main Road | 24.500-24.510 | - | 10 | Breast wall 6 m RCC at Western portal of T-2 | 6.00 | B5 | II |
| Main Road | 24.760-24.780 | 20 | - | Breast wall 5 m RCC at Eastern portal of T-2 | 5.00 | B5 | II |
| Main Road | 24.760-24.830 | - | 70 | Breast wall 11 m RCC at Eastern portal of T-2 | 11.00 | B5 | II |
| Main Road | 24.890-24.900 | - | 10 | Breast wall 7 m RCC at SW portal of T-3 | 7.00 | B5 | II |
| Main Road | 24.895-24.900 | 5 | - | Breast wall 7 m RCC at SW portal of T-3 | 7.00 | B5 | II |
| Main Road | 25.410-25.420 | 10 | - | Breast wall 6 m RCC at NE portal of T-3 | 6.00 | B5 | II |
| Main Road | 25.410-25.420 | - | 10 | Breast wall 6 m RCC at NE portal of T-3 | 6.00 | B5 | II |
| Main Road | 25.445-25.460 | 15 | 15 | Breast wall 8 m RCC at SW portal of T-4 | 8.00 | B5 | II |
| Main Road | 26.098-26.120 | 22 | 22 | Breast wall 6 m RCC at Northern portal of T-4 | 6.00 | B5 | II |
| Main Road | 26.180-26.260 | 80 | 80 | Breast wall 6 m with Gabion | 6.00 | B4 | II |
| Main Road | 26.320-26.510 | 190 | - | Breast wall 6 m with Gabion | 6.00 | B4 | II |
| Main Road | 26.360-26.520 | - | 160 | Breast wall 6 m with Gabion | 6.00 | B4 | II |
| Main Road | 26.520-26.550 | 30 | - | Breast wall 3 m | 3.00 | B1 | II |
| Main Road | 26.530-26.580 | - | 50 | Breast wall 3 m | 3.00 | B1 | II |
| Main Road | 26.560-26.620 | 60 | - | Breast wall 2m | 2.00 | B1 | II |
| Main Road | 26.645-26.700 | 55 | - | Breast wall 1 m along side drain L.H.S | 1.00 | B1 | II |
| Main Road | 26.800-26.870 | 70 | - | Breast wall 1.5 m | 1.50 | B1 | II |
| Main Road | 27.000-27.030 | - | 30 | Breast wall 1.5 m | 1.50 | B1 | II |
| Main Road | 27.000-27.030 | 30 | - | Breast wall 6 m with Gabion | 6.00 | B4 | II |
| Main Road | 27.030-27.090 | 60 | - | Breast wall 3 m | 3.00 | B1 | II |
| Main Road | 27.110-27.180 | 70 | - | Breast wall 2 m | 2.00 | B1 | II |



FINAL

NATIONAL HIGHWAY - 1A, JAMMU & KASHMIR GEOLOGICAL SECTION ALONG LEFT TUBE (KM-23.700 & KM-23.890) OF TUNNEL-1



NATIONAL HIGHWAY - 1A JAMMU & KASHMIR GEOLOGICAL SECTION ALONG RIGHT TUBE (KM-23.700 & KM-23.890) OF TUNNEL-1

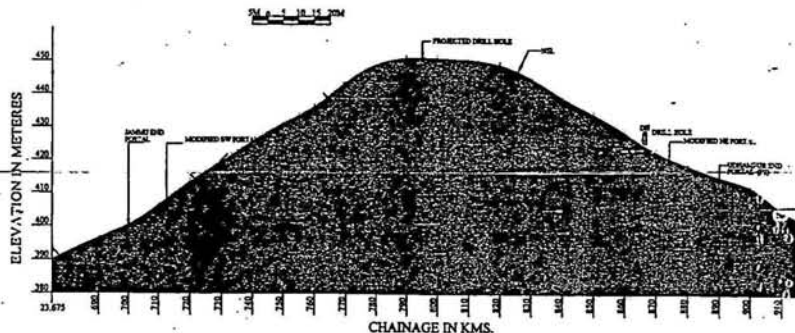


PLATE - VI

NOTES :-

1. S.W. HAS BEEN PROVIDED BY SCOTT WILSON
2. GROUND LEVEL AND CHAINAGE HAS BEEN PROVIDED BY SCOTT WILSON AT SITE.
3. JOINTS SHOWN AS SCHEMATIC.

INDEX

- SAND ROCK (COMPACT)
- SOFT SAND ROCK
- CLAY SHALE / CLAY STONE
- CLAY STONE WITH THIN BANDS SAND ROCK OR SILTY STONE

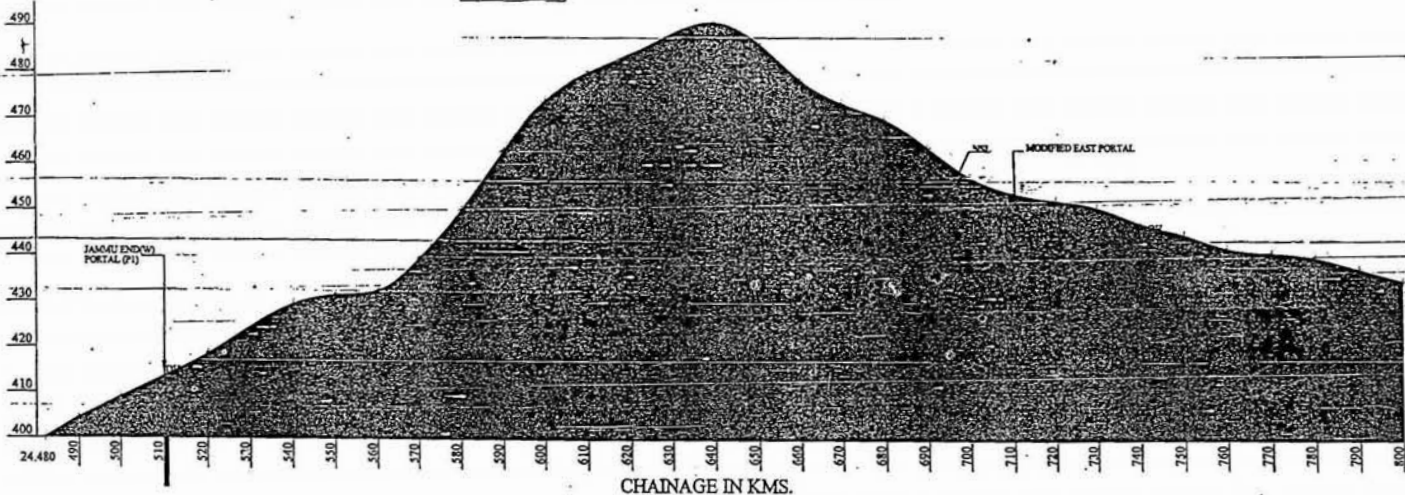
- NOTES : 1. S.W. HAS BEEN SUPPLIED BY SCOTT WILSON
2. JOINTS ARE SHOWN AS SCHEMATIC SCOTT WILSON



NATIONAL HIGHWAY - 1A, JAMMU & KASHMIR
 GEOLOGICAL SECTION ALONG LEFT TUBE (KM-24.510 & KM-24.750) OF TUNNEL-2

5M 0 5 10 15 20M

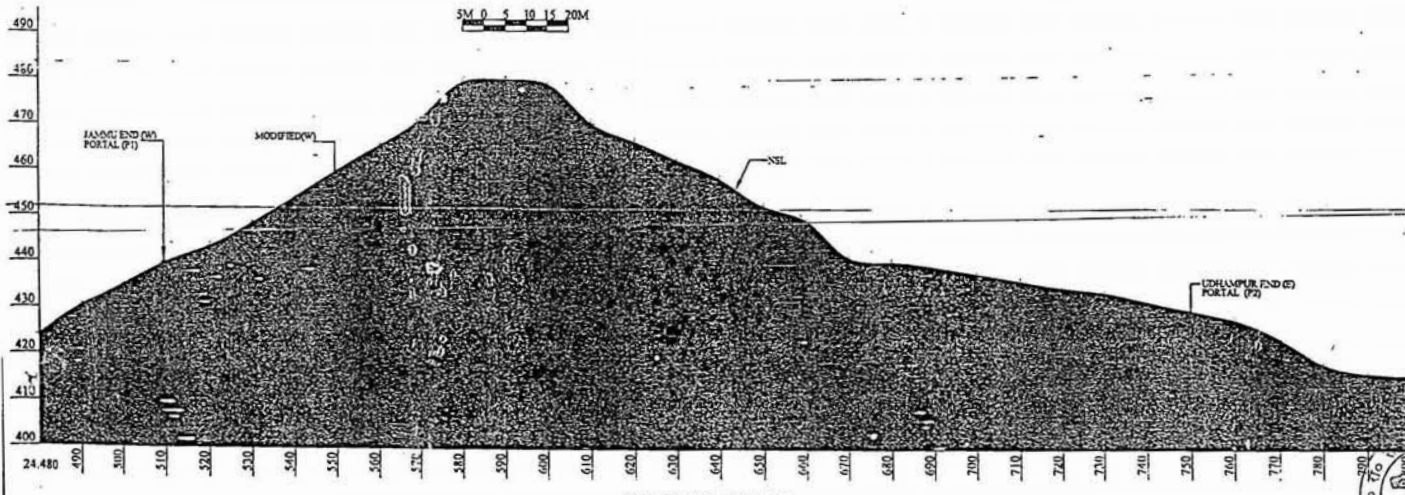
- NOTES :-
1. N.S.L HAS BEEN PROVIDED BY SCOT WILLSON
 2. GROUND LEVEL AND CHAINAGE HAS BEEN PROVIDED BY SCOT WILLSON AT SITE
 3. JOINTS SHOWN ARE SCHEMATIC.



CHAINAGE IN KMS.

NATIONAL HIGHWAY - 1A JAMMU & KASHMIR
 GEOLOGICAL SECTION ALONG RIGHT TUBE (KM-24.510 & KM-24.750) OF TUNNEL-2

5M 0 5 10 15 20M



CHAINAGE IN KMS.

5M 0 5 10 15 20M

INDEX

| | |
|--|--|
| | SAND ROCK (CALIFACT) |
| | SOIL |
| | CLAY SHALE / CLAY STONE |
| | CLAY STONE WITH THIN BANDS SAND ROCK OR SILT STONE |

- NOTES :
1. N.S.L HAS BEEN SUPPLIED BY SCOTT WILLSON
 2. JOINTS ARE SHOWN AS SCHEMATIC SCOTT WILLSON

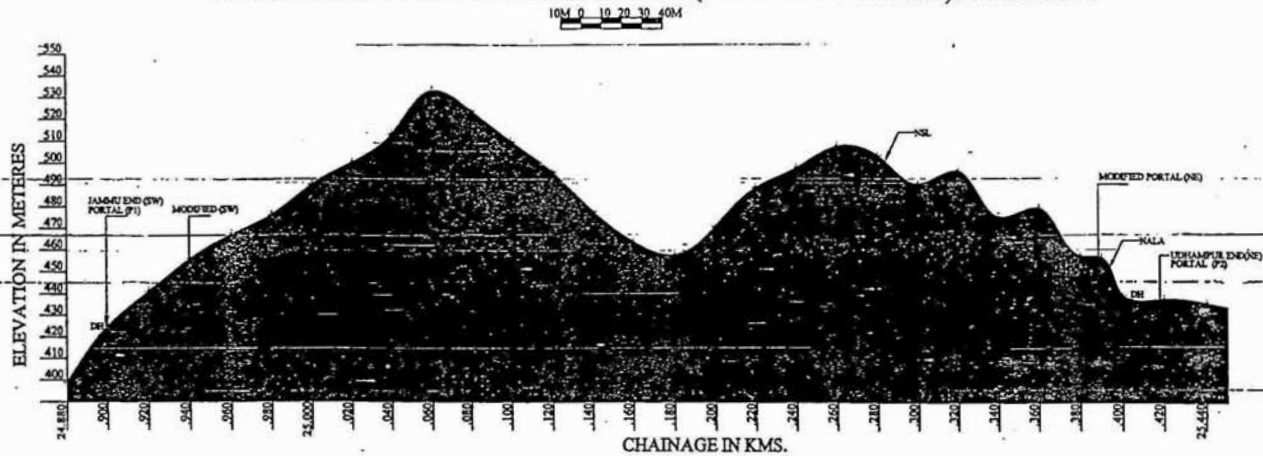


RIGHT TUBE
NATIONAL HIGHWAY - 1A, JAMMU & KASHMIR
GEOLOGICAL SECTION ALONG RIGHT TUBE (KM-24.900 & KM-24.410) OF TUNNEL-3

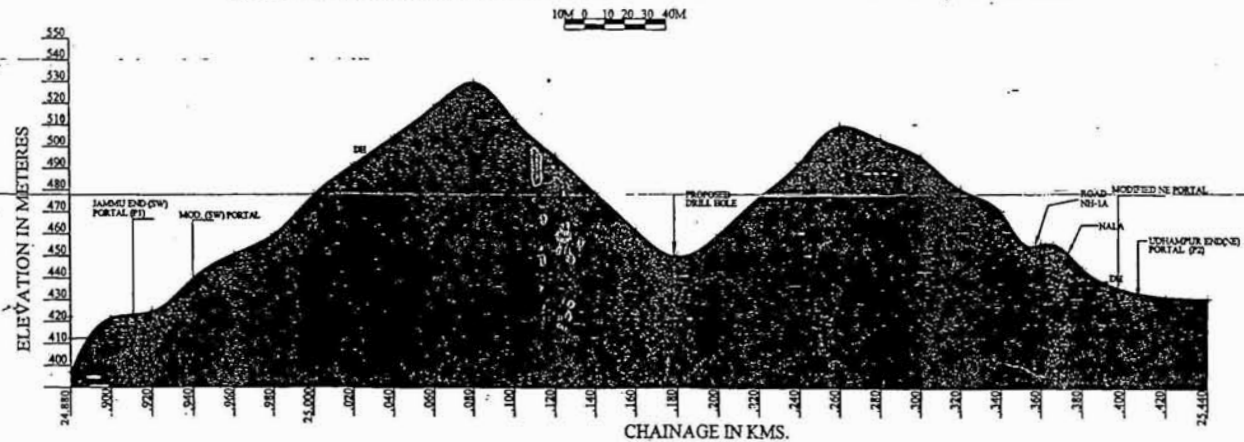
PLATE - VIII

NOTES :-

1. N.S.L HAS BEEN PROVIDED BY SCOTT WILSON
2. GROUND LEVEL AND CHAINAGE HAS BEEN PROVIDED BY SCOTT WILSON AT SITE.
3. JOINTS SHOWN ARE SCHEMATIC.



LEFT TUBE
NATIONAL HIGHWAY - 1A JAMMU & KASHMIR
GEOLOGICAL SECTION ALONG LEFT TUBE (KM-24.900 & KM-24.410) OF TUNNEL-3



INDEX

| | |
|--|--|
| | SAND ROCK (COMPACT) |
| | SOIL |
| | CLAY SHALE / CLAY STONE |
| | CLAY STONE WITH THIN BANDS SAND ROCK OR SILT STONE |

- NOTES : 1. N.S.L HAS BEEN SUPPLIED BY SCOTT WILSON
2. JOINTS ARE SHOWN AS SCHEMATIC. SCOTT WILSON



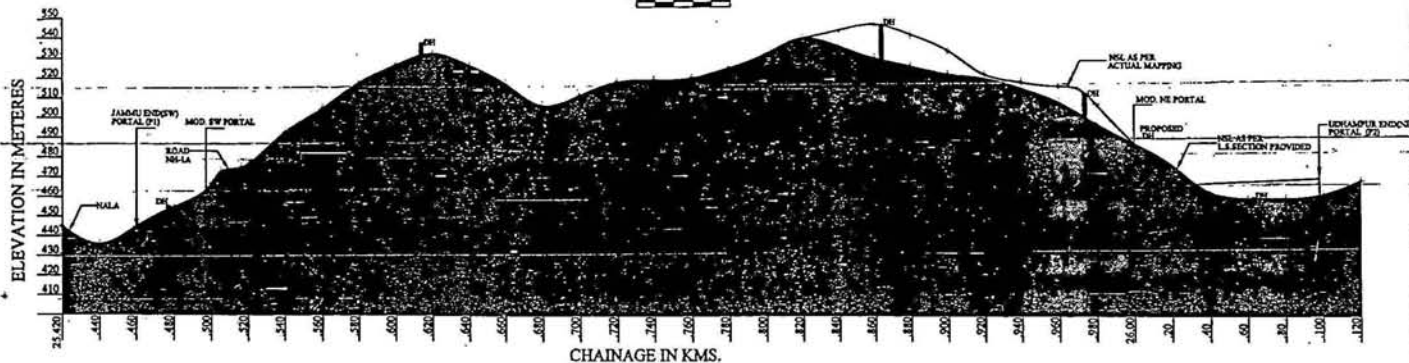
RIGHT TUBE
NATIONAL HIGHWAY - 1A, JAMMU & KASHMIR
GEOLOGICAL SECTION ALONG RIGHT TUBE (KM-25.460 & KM-26.098) OF TUNNEL-4

PLATE - VIV

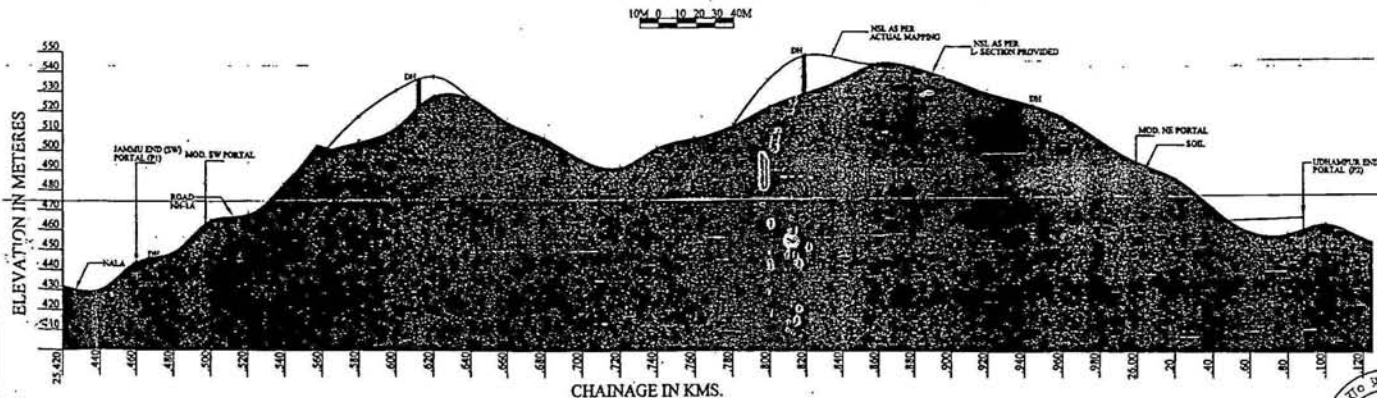
NOTES :-

1. N.S.L HAS BEEN PROVIDED BY SCOTT WILSON
2. GROUND LEVEL AND CHAINAGE HAS BEEN PROVIDED BY SCOTT WILSON AT SITE
3. JOINTS SHOWN ARE SCHEMATIC

SCALE
 10M 0 10 20 30 40M



LEFT TUBE
NATIONAL HIGHWAY - 1A JAMMU & KASHMIR
GEOLOGICAL SECTION ALONG LEFT TUBE (KM-25.460 & KM-26.098) OF TUNNEL-4



INDEX

- SAND ROCK (COMPACT)
- SOIL - FEATHERED ROCK
- CLAY SHALE / CLAY STONE
- CLAY STONE WITH THIN BANDS SAND ROCK OR SILT STONE

- NOTES :
1. N.S.L HAS BEEN SUPPLIED BY SCOTT WILSON
 2. JOINTS ARE SHOWN AS SCHEMATIC SCOTT WILSON

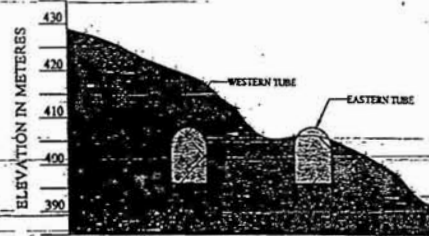


NATIONAL HIGHWAY - 1A, JAMMU & KASHMIR
 GEOLOGICAL CROSS-SECTIONS OF TUNNEL - 1 TO T - 4

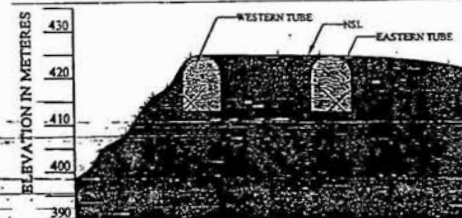
CROSS-SECTION
 AT KM 23.700 (T-1, P1)



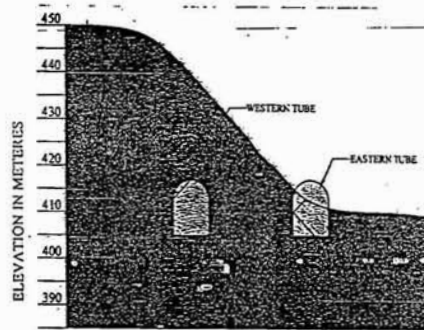
CROSS-SECTION
 AT KM 23.890 (T-1, P2)



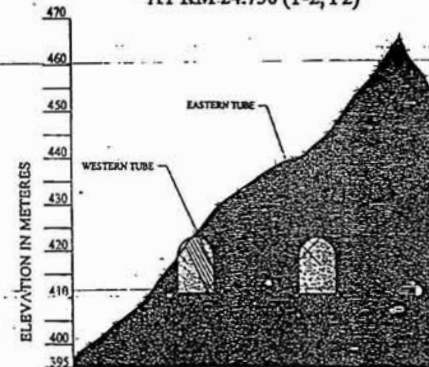
CROSS-SECTION
 AT KM 900 (T-3, P1)



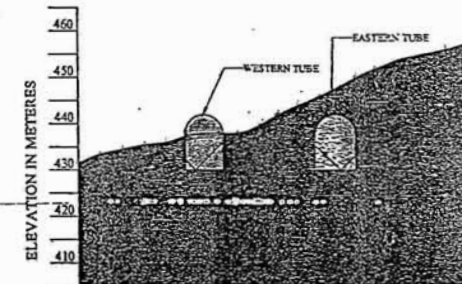
CROSS-SECTION
 AT KM 24.510 (T-2, P1)



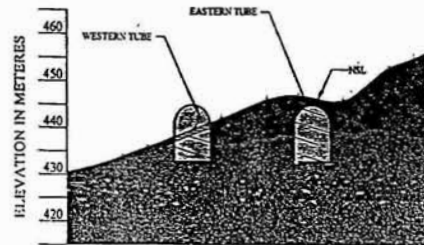
GEOLOGICAL CROSS-SECTION
 AT KM 24.750 (T-2, P2)



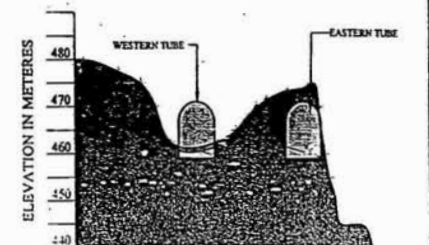
CROSS-SECTION
 AT KM 25.410 (T-3, P2)



CROSS-SECTION
 AT KM 25.460 (T-4, P1)



CROSS-SECTION
 AT KM 26.098 (T-4, P2)



NOTES :-

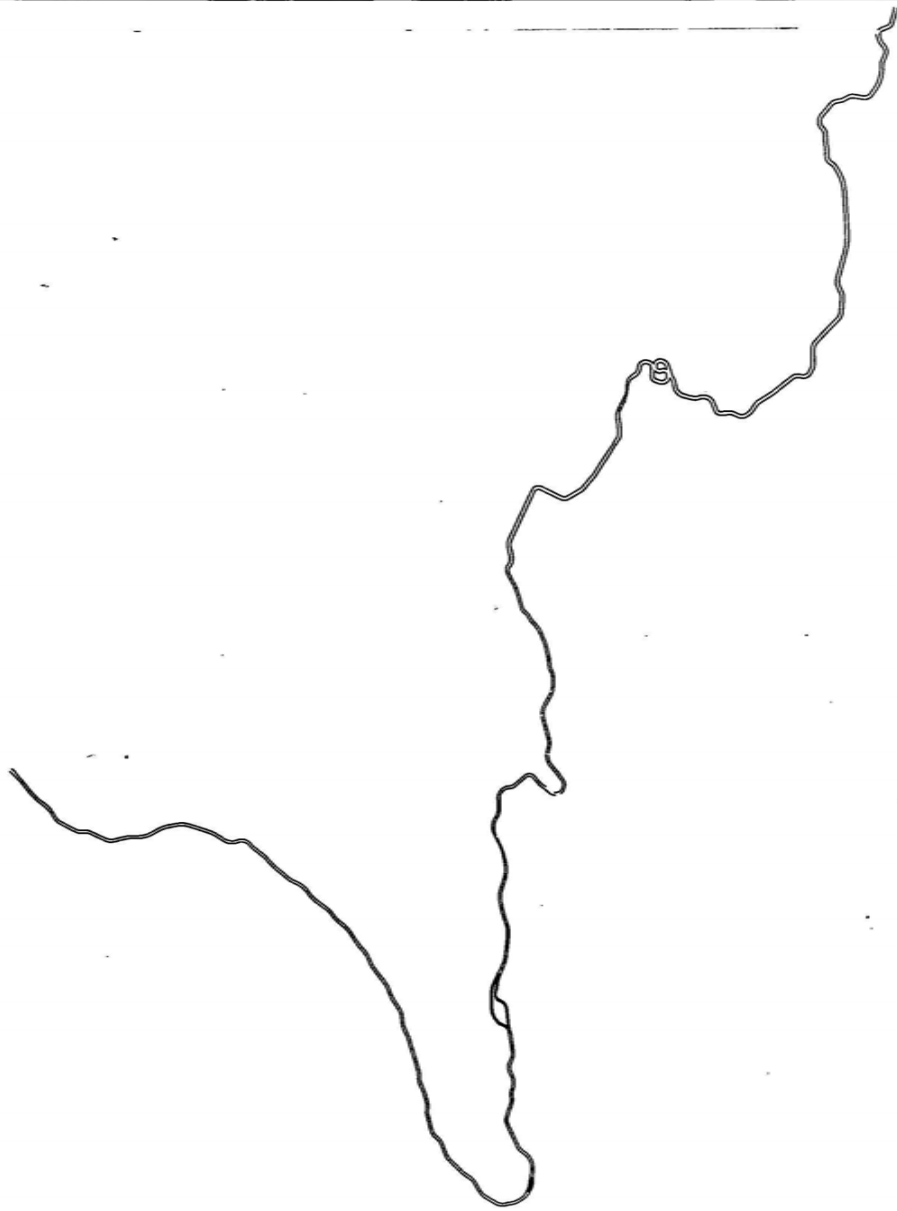
1. N.S.L. HAS BEEN PROVIDED BY SCOTT WILSON
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3. JOINTS SHOWN ARE SCHEMATIC.

-  SOFT SAND ROCK
-  SAND ROCK (COMPACT)
-  CLAY STONE WITH THIN BANDS SAND ROCK OR SILT STONE.
-  CLAY SHALE / CLAY STONE

NOTES :-

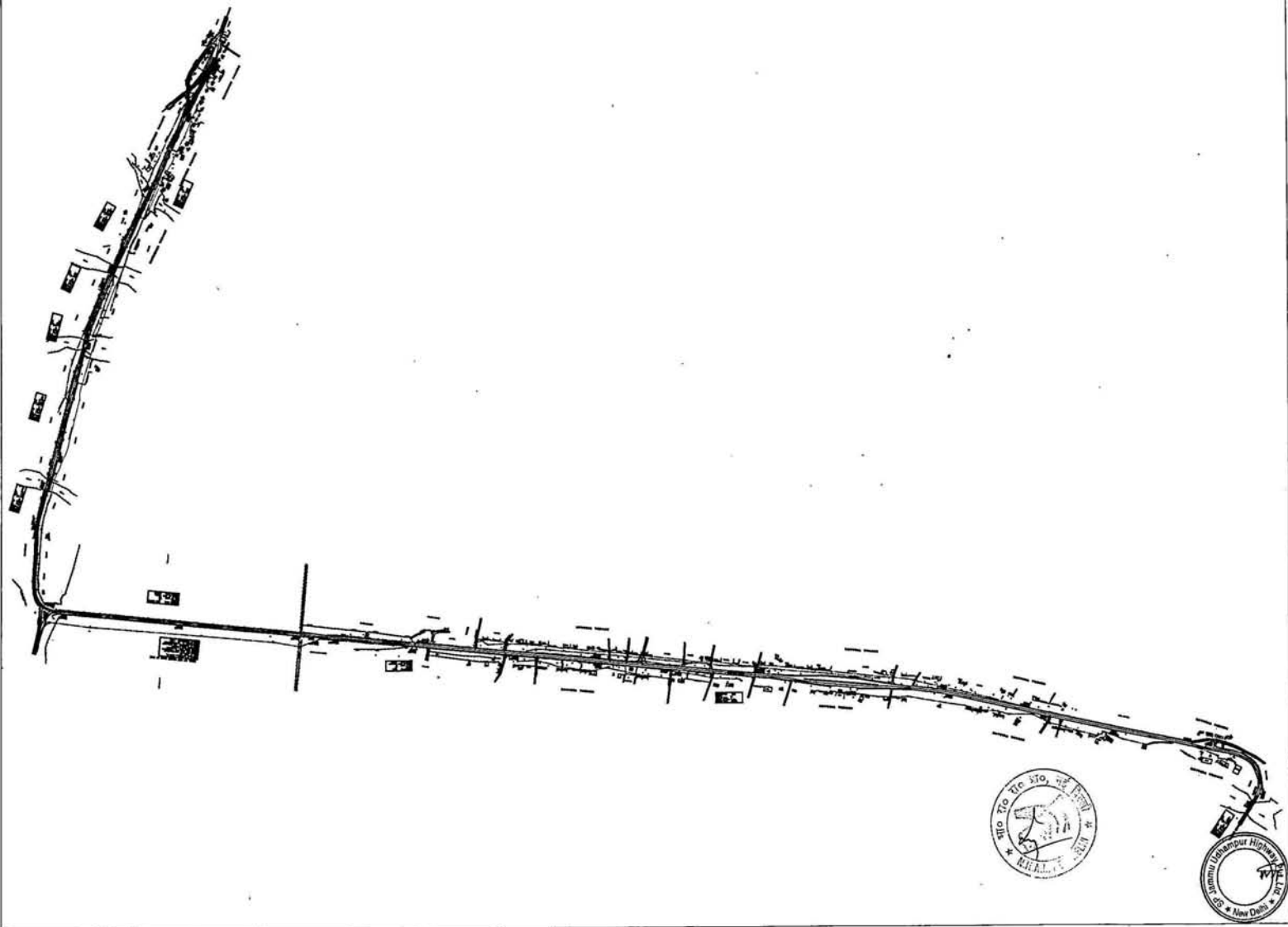
1. N.S.L. HAS BEEN SUPPLIED BY SCOTT WILSON
2. JOINTS ARE SHOWN AS SCHEMATIC SCOTT WILSON

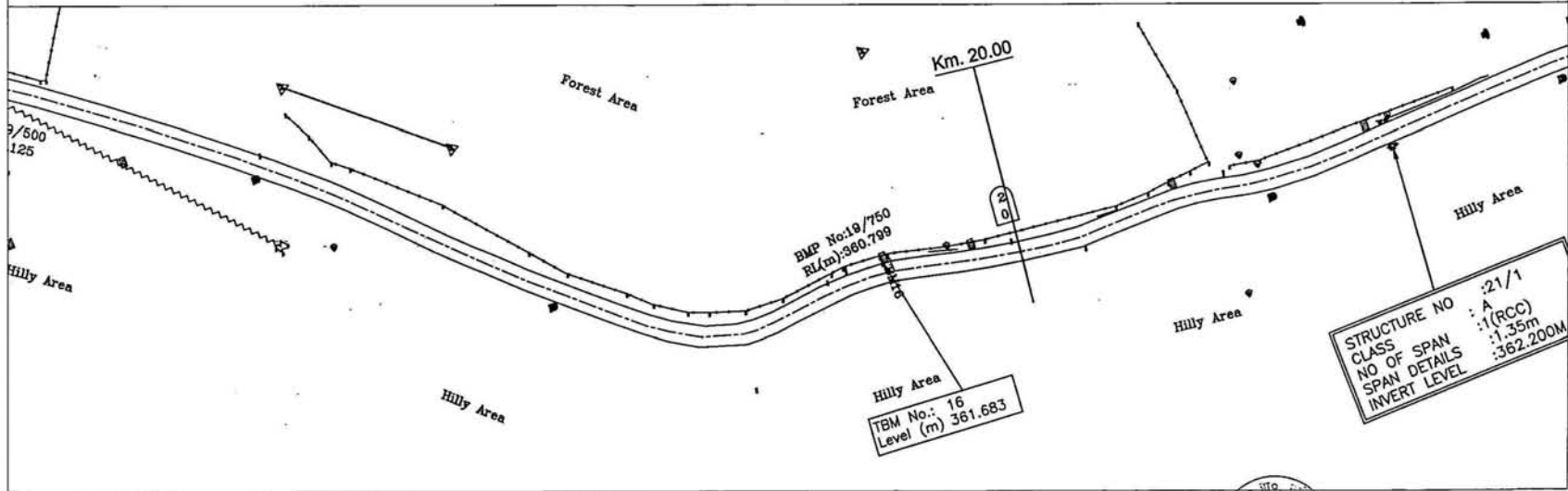
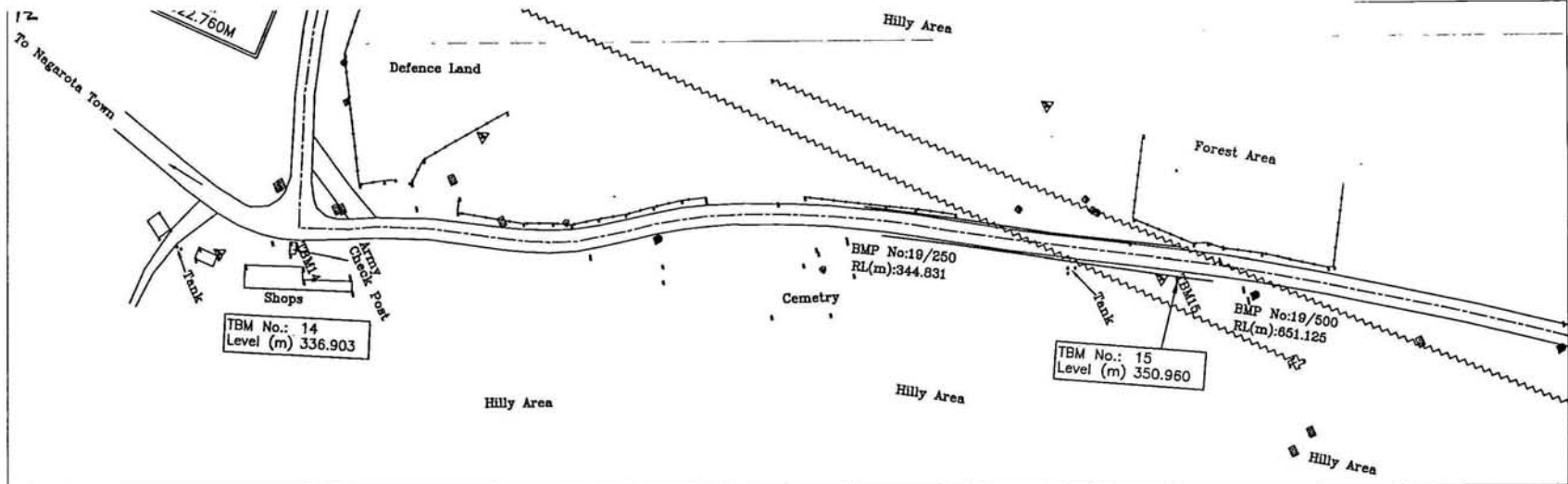


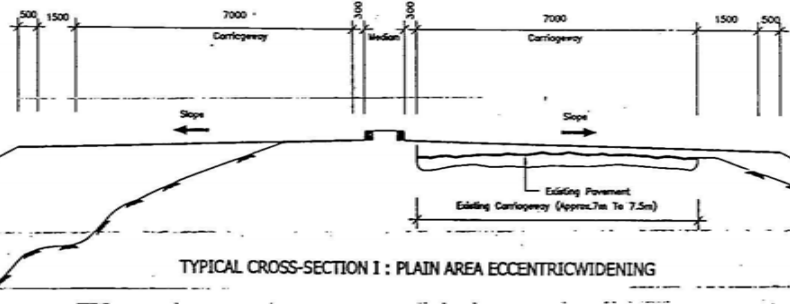


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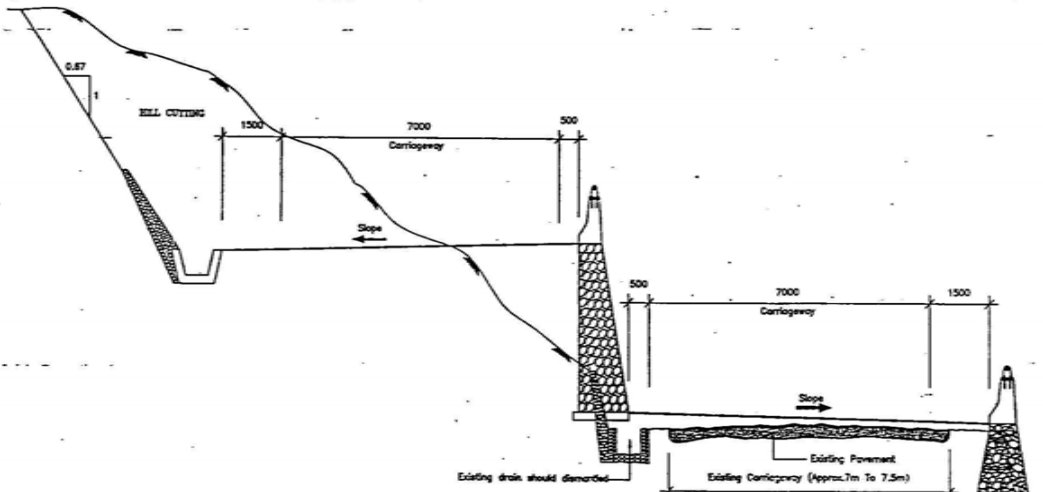
5







TYPICAL CROSS-SECTION I : PLAIN AREA ECCENTRIC WIDENING



TYPICAL CROSS-SECTION II : HILL SIDE WIDENING WITH TWO LANE (HIGH LEVEL ROAD)

| | |
|----------|---------|
| Revision | Details |
| | |
| | |
| | |

Project Title
 Preparation of Feasibility and Detailed Project Report for Four Laning of National Highway No.1 A from Samana to Udhampur including Jamma Nagrota and Udhampur Bypass to North South Corridor in the state of J&K

Original Scale: A3
 Path: 3000000000
 Plot Scale: 1:1000

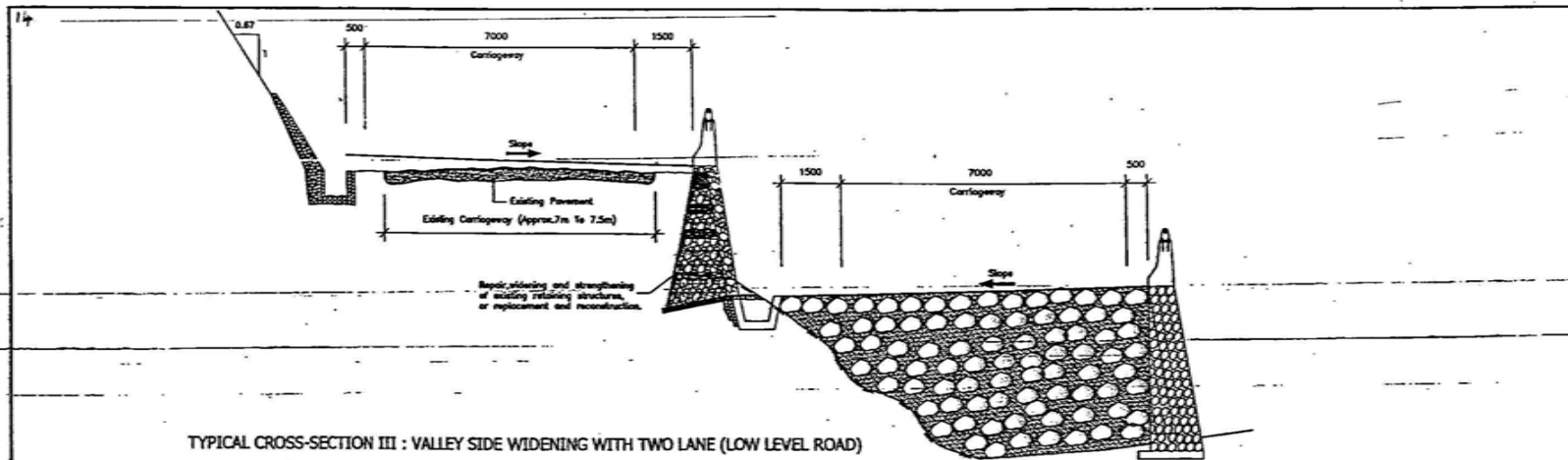
Client
NATIONAL HIGHWAYS AUTHORITY OF INDIA
 (Ministry of Road Transport & Highways)

Drawing Title: TYPICAL CROSS SECTIONS
 Drawing Number: SW/15/03/GENERAL/CS-41
 Sheet: 1
 Scale: 1:1000
 Date: DECEMBER-05

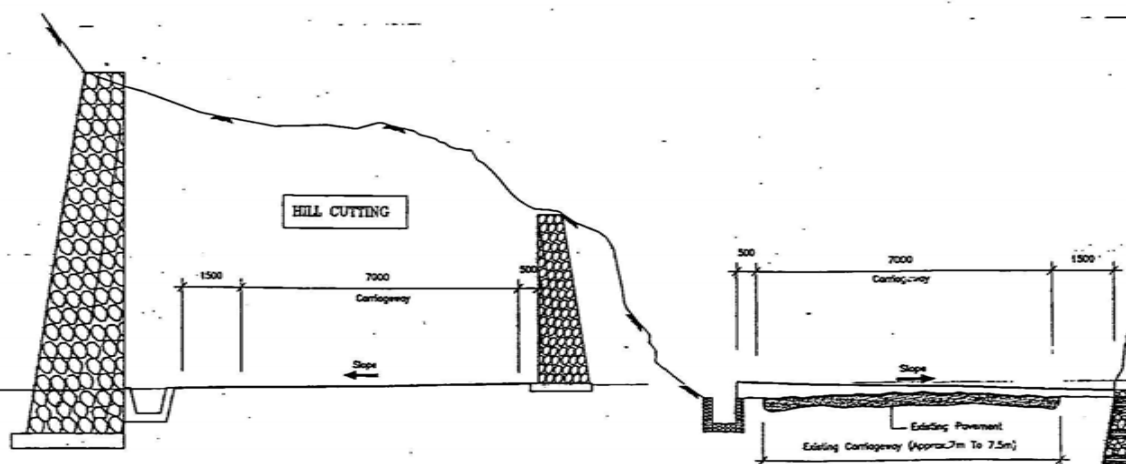


Scott Wilson Kirkpatrick & Co Ltd.
 &
 Scott Wilson Kirkpatrick (I) Pvt Ltd.
 1-25/1, JLN Chowk, Connaught Place, New Delhi-110029
 Tel: 26101111, 26101122 to 48
 Fax: 26101133, 26101144
 E-mail: swi@swi.com





TYPICAL CROSS-SECTION III : VALLEY SIDE WIDENING WITH TWO LANE (LOW LEVEL ROAD)



TYPICAL CROSS-SECTION IV : BOX CUTTING IN HILL SECTION

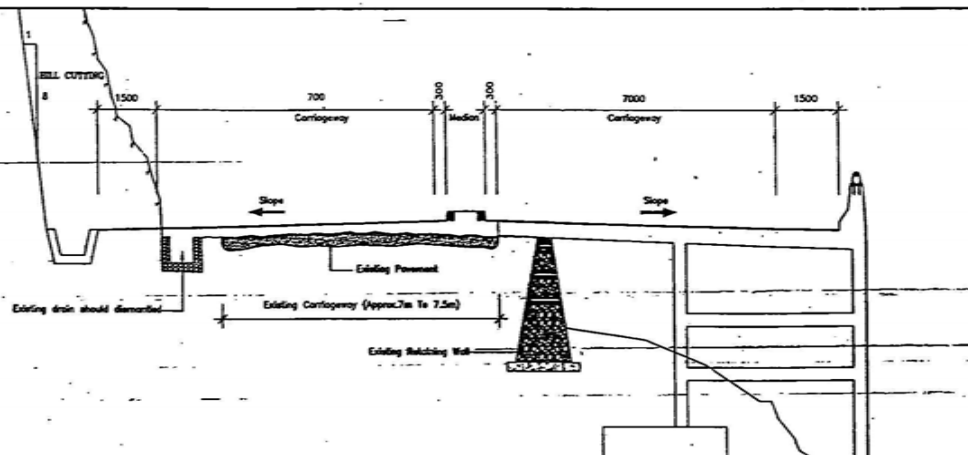


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|--|--|--|--|--|--|--|--|
| Project Title Preparation of Feasibility and Detailed Project Report for Four Lanes of National Highway No.1 & from Sonma to Uchamper including Jamma Nagrola and Uchamper Bypass to North South Corridor in the state of J&K | | Client NATIONAL HIGHWAYS AUTHORITY OF INDIA (Ministry of Road Transport & Highways) | | Drawing Title TYPICAL CROSS SECTIONS | | Client Scott Wilson Kirkpatrick & Co. (I) Pvt. Ltd. 1-2/4, SCHEME CORPORATE HSE, 10002 BANGALURU, K.C. NEW DELHI - 110044 THE INDIA SECTION NO. 46 FAX NO. 011-26192222 E-MAIL : swk@swk.com | |
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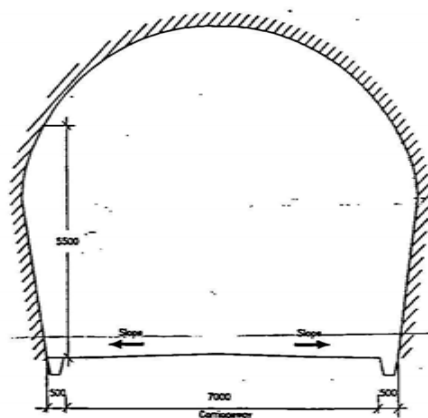
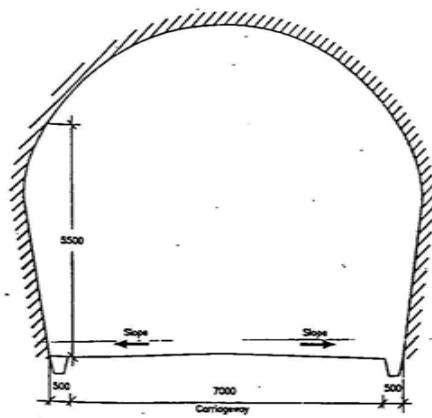
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| Revision Details | REV. NO. | REVISION | DATE |
| | | | |



15



TYPICAL CROSS-SECTION V : VALLEY SIDE WIDENING WITH VIADUCT



TYPICAL CROSS-SECTION VI : TWIN TUNNELS

| Revision | Details | By | Chk | Date | Scale |
|----------|---------|----|-----|------|-------|
| | | | | | |
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| | | | | | |

Project Title
Preparation of Feasibility and Detailed Project Report for Four Lanes of National Highway No.1 & from Samna to Udhampur including Jammu Nagrota and Udhampur Bypass to North South Corridor in the state of J&K

This drawing is the property of the client. It is to be used only for the purpose intended and not to be used in any other manner without the written consent of the engineer. It is to be kept in safe custody and not to be loaned, copied, or otherwise disposed of without the written consent of the engineer.

Original Size: A3
 Plot: 1000000002
 Plot Scale: 1:100

Client
NATIONAL HIGHWAYS AUTHORITY OF INDIA
 (Ministry of Road, Transport & Highways)

Drawing Title
TYPICAL CROSS SECTIONS

Drawing Number : **SNK/05/ED/GENERAL/CS-43**

Scale : 1:100

Drawn By : **A.K. RAJAN** Approved By : **H. PRASAD**

Date : **DECEMBER-05**

Scott, Wilson Kirkpatrick & Co. LLP
 &
 Scott, Wilson Kirkpatrick (I) Pvt. Ltd.

2nd Floor, 20th Cross Street, Anna Salai, Chennai - 600002

FOR ALL INFORMATION CONTACT: **SCOTT WILSON**

FOR ALL INFORMATION CONTACT: **SCOTT WILSON**



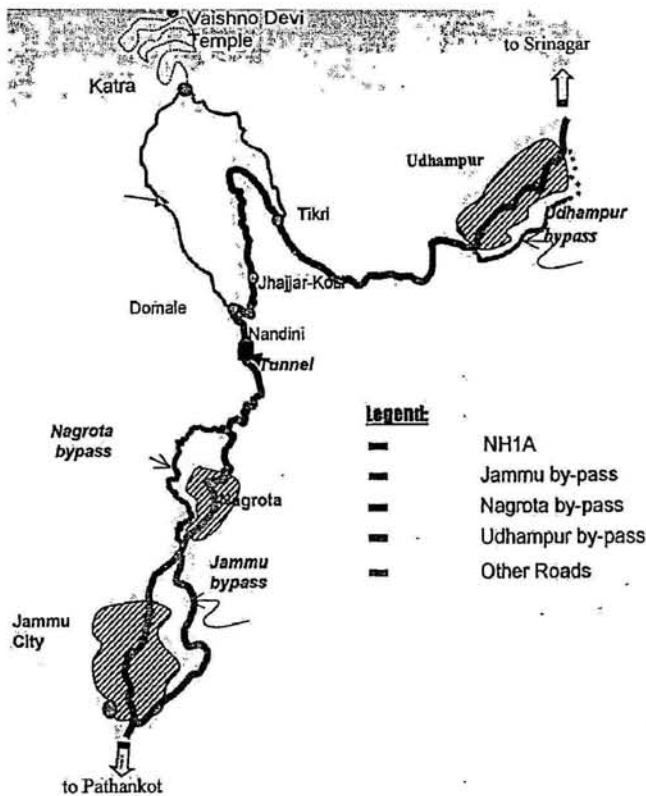
LIST OF DRAWINGS

| DESCRIPTION | DRAWING NO. |
|--|-------------------------------------|
| GENERAL DRAWINGS | |
| LOCATION PLAN | SW/1019/ROAD/GEN-01 |
| KEY PLAN | SW/1019/ROAD/GEN-02 |
| LEGEND | SW/1019/ROAD/GEN-03 |
| TYPICAL CROSS SECTION DRAWINGS | |
| TYPICAL CROSS SECTIONS | SW/1019/ROAD/CS-01 TO CS-14 |
| PLAN AND PROFILE DRAWINGS | |
| PLAN AND PROFILE DRAWINGS | SW/1019/ROAD/PP-01 TO PP-67 |
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| PACKAGE - II | SW/1019/ROAD/PBW-02 |
| PACKAGE - III | SW/1019/ROAD/PBW-03 (SHEET 1 TO 2) |
| PACKAGE - IV | SW/1019/ROAD/PBW-04 (SHEET 1 TO 2) |
| SCHEDULE OF RETAINING WALL DETAIL | |
| PACKAGE - I | SW/1019/ROAD/PRW-01 (SHEET 1 TO 2) |
| PACKAGE - II | SW/1019/ROAD/PRW-02 |
| PACKAGE - III | SW/1019/ROAD/PRW-03 (SHEET 1 TO 2) |
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| MISCELLANEOUS DRAWINGS | |
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| DETAILS OF CHEVRON DIRECTION SIGN, RPM AND ROAD HUMP | SW/1019/ROAD/MISC-02 |
| TYPICAL BUS BAY DRAWINGS | SW/1019/ROAD/MISC-03 |
| HIGH EMBANKMENT & DRAINAGE | SW/1019/ROAD/MISC-04 |
| DRAINAGE DETAILS | SW/1019/ROAD/MISC-05 (SHEET 1 TO 2) |
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| ROAD SIGNS | SW/1019/ROAD/MISC-07 (SHEET 1 TO 3) |
| TYPICAL PIPE CULVERT DETAILS | SW/1019/ROAD/MISC-08 (SHEET 1 TO 2) |
| TYPICAL SLAB CULVERT DETAILS | SW/1019/ROAD/MISC-09 (SHEET 1 TO 2) |
| GUARD-RAIL | SW/1019/ROAD/MISC-10 |
| RIGID PAVEMENT | SW/1019/ROAD/MISC-11 |
| MEDIAN OPENING | SW/1019/ROAD/MISC-12 |
| TYPICAL ROAD SIDE FURNITURE | SW/1019/ROAD/MISC-13 |
| TYPICAL RETAINING AND BREAST WALL DETAILS | SW/1019/ROAD/MISC-14 (SHEET 1 TO 2) |
| DETAILS OF TRAFFIC MARKINGS | SW/1019/ROAD/MISC-15 |
| TYPICAL JUNCTION | SW/1019/ROAD/MISC-16 (SHEET 1 TO 2) |
| TYPICAL DETAILS OF BOX CULVERT | SW/1019/ROAD/MISC-17 (SHEET 1 TO 4) |

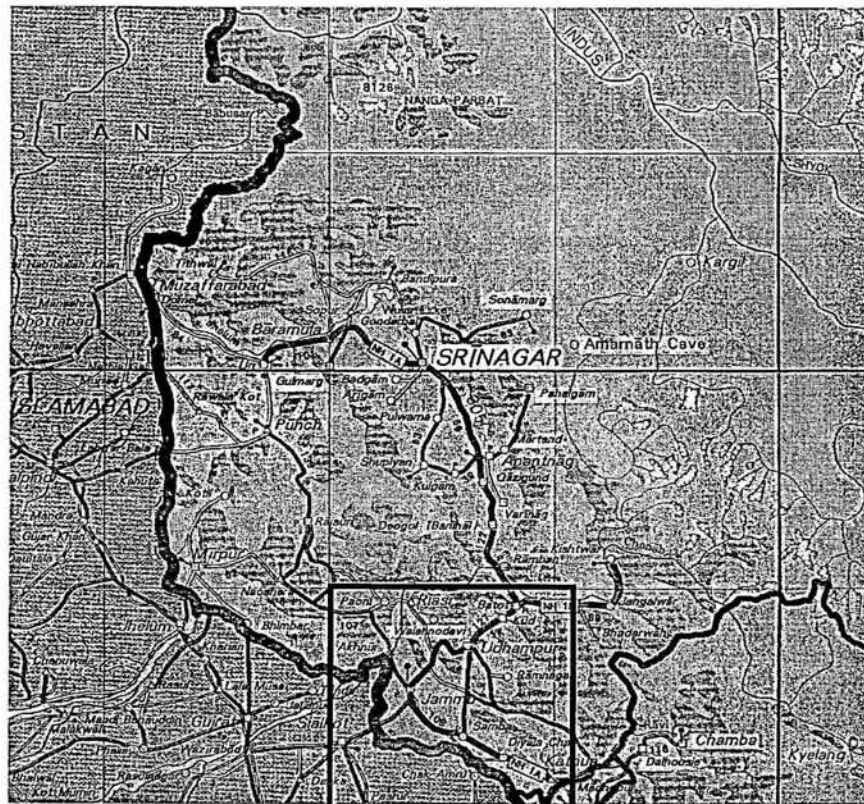


GENERAL DRAWINGS





LOCATION PLAN



INDEX PLAN



| | | | |
|------------------|------|--------|--------|
| 1000 | 1000 | 100000 | 1000 |
| By | Chk | Date | Suffix |
| Revision Details | | | |

Project Title
 Consultancy services for preparation of detailed project report for Rehabilitation and upgrading of existing 3-lane road to 4-lane divided Cartage-way configuration of Jammu - Udhampur section of NH-1 A (low 0.00 to low (6.00) including Jammu/Pur) Nagrota and Udhampur Bypass Phase - III (Provision of North - South - corridor Project, Contract Package C-0718)

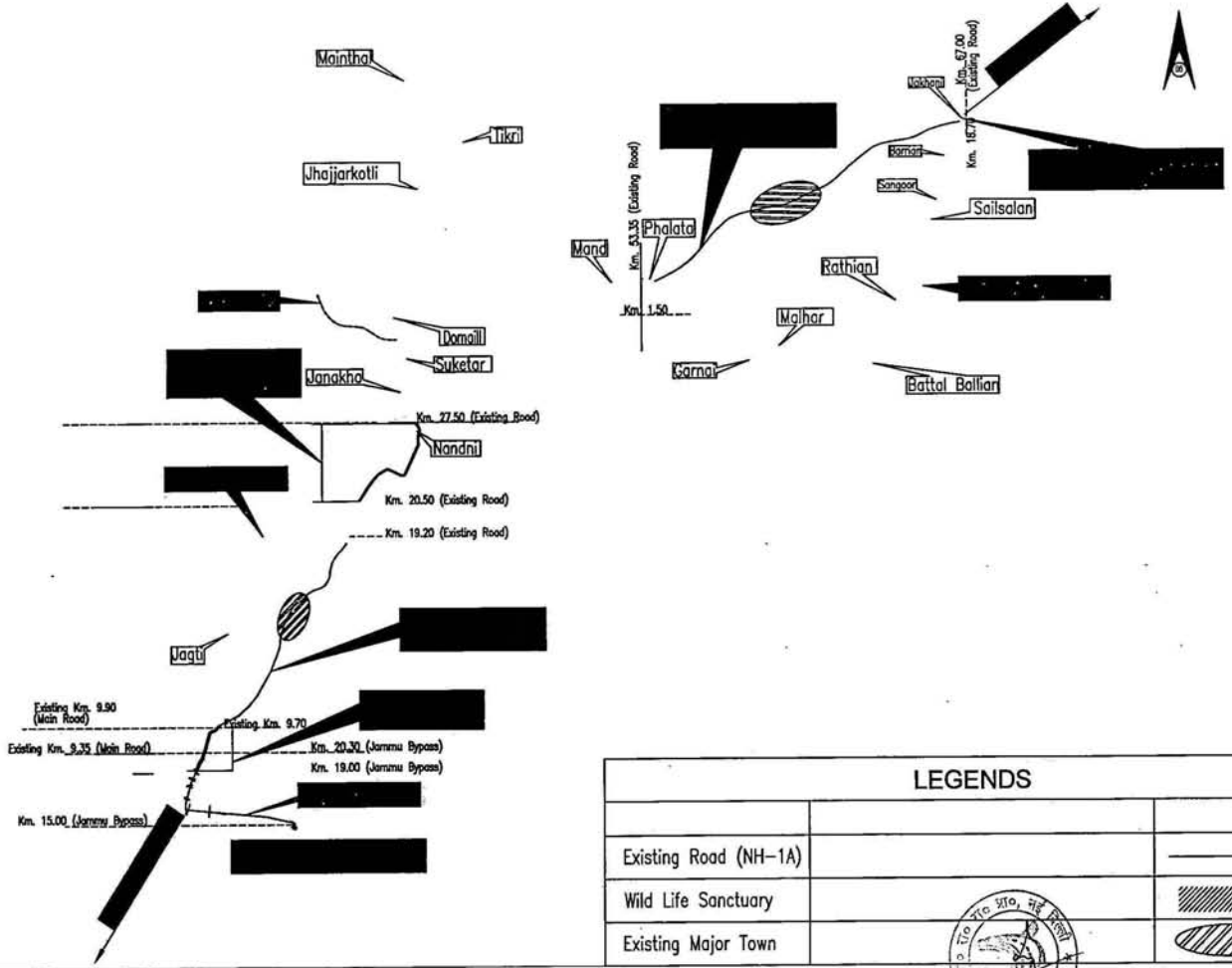
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Original Size: A3
 File Reference: New/JammuUdhampur/consultancy/0718

Client
NATIONAL HIGHWAYS AUTHORITY OF INDIA
 (Ministry of Shipping, Road Transport & Highways)

Drawing Title: LOCATION PLAN
Drawing No.: SWM/1019/ROADS/GEN-01
Scale: 1:—
Dn: Bhardwaj
Dgn: V. V. GULATI
Appd: BRIG.C.D.PURJ
Date: June-2009

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 E-mail: s.windia@gmail.com



| LEGENDS | |
|-----------------------|--|
| Existing Road (NH-1A) | |
| Wild Life Sanctuary | |
| Existing Major Town | |

| | | | | | |
|----------|---------|----|-----|------|------|
| Revision | Details | By | CHK | DATE | SUFF |
| | | | | | |

Project Title
 Consultancy services for preparation of detailed project report for rehabilitation and upgrading of existing 3-lane road to 4-lane divided carriageway and widening of existing 2-lane road to 4-lane divided carriageway and widening of Jammu - Udhampur section of NH-1 A (km 0.00 to km 64.00) including Jammu/POD bypass and Udhampur Bypass Phase - III (Progression of North-South - corridor Project, Contract Package C-0211)

Client
 NATIONAL HIGHWAYS AUTHORITY OF INDIA
 (Ministry of Shipping, Road Transport & Highways)

Drawing Title
 KEY PLAN
 (JAMMU - UDHAMPUR SECTION)

Drawing No.: SW/1019/ROADS/GEN-02

Scale :- N. T. S.

Des. Bhardwaj, Dgn. K.PRASAD, Appd. C.D.PUR, Date JUNE-2009

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LEGEND

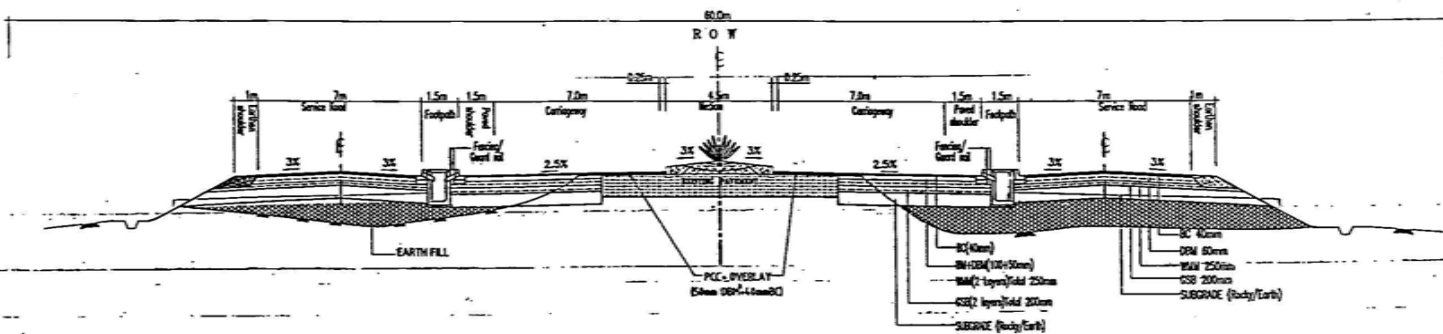
| Sl.No | DESCRIPTION | SYMBOL | Sl.No | DESCRIPTION | SYMBOL |
|-------|--------------------------|----------|-------|--------------------------------|--------|
| 1 | Road | ===== | 23 | Telephone Junction Box | ☐ |
| 2 | Railway Line | +++++ | 24 | Electrical Junction Box | EB |
| 3 | Lamp Post | ↑ | 25 | Gate | ⊘ |
| 4 | Electric Pole | △ | 26 | Transformer | ⊠ |
| 5 | Telephone Pole | ☎ | 27 | Water Pipe Line | — |
| 6 | Square Manhole | ■ | 28 | Fence Line | —+—+— |
| 7 | Kilometer Stone | Ⓜ | 29 | Traffic Signage Board | ↑ |
| 8 | Manhole | ⊙ | 30 | Tap | ⊥ |
| 9 | Drain | ===== | 31 | Neem Tree | ⊙NT |
| 10 | Road signs | Ⓢ | 32 | Other Tree 0.3 TO 0.6 Dia | ⊙OT |
| 11 | Bench_Mark | BM 20.05 | 33 | PalmTree or Coconut tree | ✱ |
| 12 | Building Line | — | 34 | Pepal Tree | ⊙ |
| 13 | Boundary Wall | — | 35 | Bannion Tree | ⊙ |
| 14 | Commercial Building Shop | ☐ | 36 | Asoka Tree | ⊙ |
| 15 | Bridge/Culvert | — | 37 | Mango Tree | ⊙ |
| 16 | Traffic Signal Post | ⊥ | 38 | Bore Hole | ⊥ |
| 17 | Bus Stop | ☐ | 39 | Flag Post | ⊥ |
| 18 | Fencing_Wire | —+—+— | 40 | RESI. BUILDING SINGLE STORIED | ☐ |
| 19 | Fire Hydrant | ⊥ | 41 | RESI. BUILDING DOUBLE STORIED | ☐ |
| 20 | Post Box | ⊥ | 42 | RESI. BUILDING ABOVE 2 STORIES | ☐ |
| 21 | Name Board | Ⓝ | 43 | EucaliptusTree | ⊙UT |
| 22 | OFC | Ⓢ | 44 | Embankment | —+— |



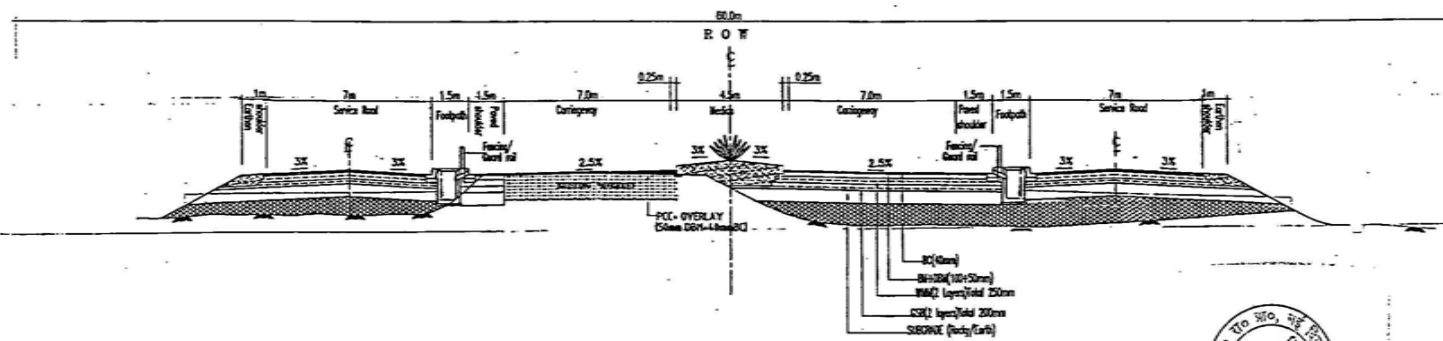
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|---|--|---|--|---|--|---------------------------------|--|---|--|
| Project Title Consultancy services for preparation of detailed project report for Rehabilitation and upgrading of existing 3-lane road to 4-lane divided Carriageway configuration of Jaunpur - Udhampur section of NH-1 A (km 0.00 to km 66.00) including Jaunpur (Part) Nagrota and Udhampur Bypass Phase - III Programme of North-South corridor Project, | | This drawing is the property of National Highways Authority of India and shall not be allowed to be used in any form or by any means without the prior written permission of the Authority. Any person who uses this drawing in any form or by any means without the prior written permission of the Authority shall be liable to prosecution in law. | | Client NATIONAL HIGHWAYS AUTHORITY OF INDIA (Ministry of Shipping, Road Transport & Highways) | | Drawing Title: LEGEND | | Scott Wilson India Private Limited A-26/A, MOHAN CO-OPERATIVE IND. ESTATE MATHURA ROAD NEW DELHI - 110044 Tele: (011) 41679340 - 49 Fax: (011) 41679350 E-mail: scwilinfo@scwil.com | |
| Original Size: A3 Plotting/Printing Size: (Horizontal/Vertical/Longitudinal) | | Drawing No.: SW/1019/ROADS/GEN-03 | | Sheet : | | Scale : -- | | Date : June-2009 | |
| Revision Details By: [] Date: [] Suffix: [] | | Dwn Bhardwaj | | Dgn. V. V. GULATI | | Appd BRIG.C.D.PURI | | June-2009 | |

TYPICAL CROSS SECTION DRAWINGS





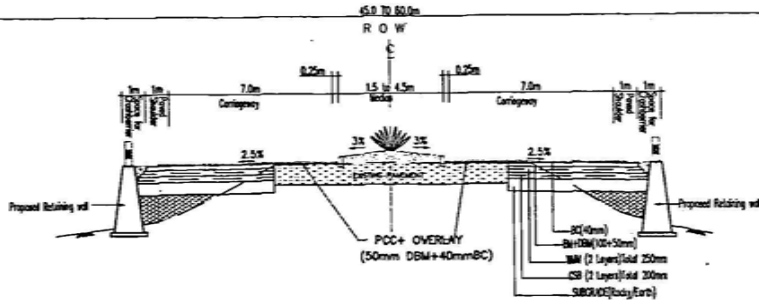
TCS-I
(CONCENTRIC WIDENING-BOTH SIDE SERVICE ROAD)



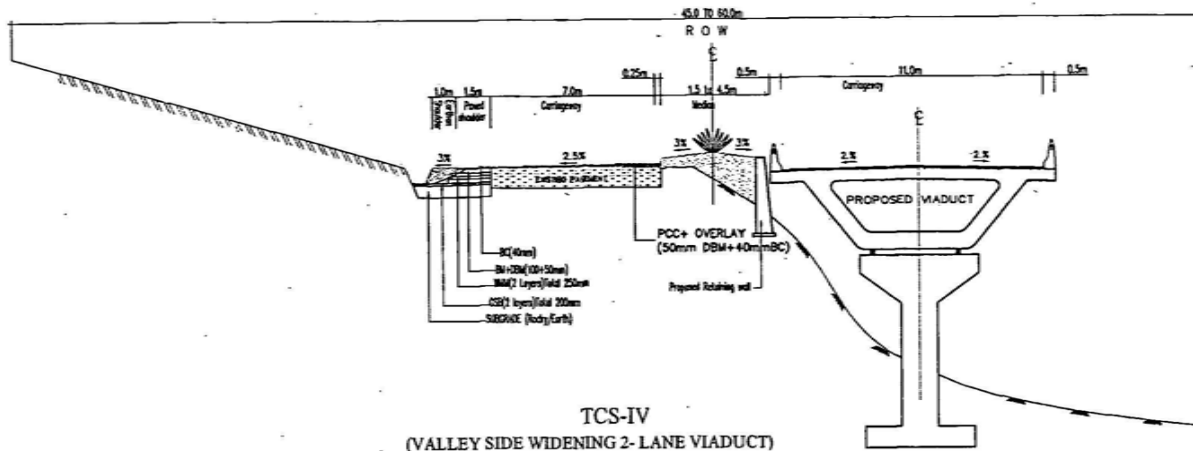
TCS-II
(ECCENTRIC WIDENING- BOTH SIDE SERVICE ROAD)



| | | | | | | | | | |
|--|--|--|--|---|--|--|--|--|--|
| <p>Project Title Consultancy services for preparation of detailed project report for Rehabilitation and upgrading of existing 2-lane road to 4-lane divided Carriageway configuration of Jaipur - Udaipur section of NH-1 A (an 8.00 to km 64.00) including Jaipur/Traj Nagar and Udaipur Bypass Phase - III Programme of North-South corridor Project, Central Package Cell III</p> | | <p>This report is the property of SCOTT WILSON INDIA PRIVATE LIMITED and shall not be allowed to be used in any manner or for any purpose other than that authorized by the Client without the prior written permission of the Client.</p> | | <p>Client NATIONAL HIGHWAYS AUTHORITY OF INDIA (Ministry of Shipping, Road Transport & Highways)</p> | | <p>Drawing Title: TYPICAL CROSS SECTIONS</p> | | <p>Scott Wilson India Private Limited A-25/A, MIDHAN CO-OPERATIVE HO. ESTATE WICKERS ROAD NEW DELHI - 110044 Tel: (011) 41679340 - 49 Fax: (011) 41679350 E-mail: scwils@swindia.com</p> | |
| <p>Original Date: 01/01/2009</p> | | <p>Scale: ---</p> | | <p>Drawing No.: SW/1019/ROADS/CS- 01</p> | | <p>Sheet: (1 of 14)</p> | | <p>Scale: ---</p> | |
| <p>By: D.K. Datta</p> | | <p>Checked: ---</p> | | <p>Date: ---</p> | | <p>Appd: V. V. GUPTA</p> | | <p>Date: 01/01/2009</p> | |



TCS-III
(CONCENTRIC - WIDENING (BOTH SIDE FILLING))



TCS-IV
(VALLEY SIDE WIDENING 2-LANE VIADUCT)



| | | | |
|-----|----|------|--------|
| Rev | By | Date | Reason |
| | | | |
| | | | |
| | | | |

Project Title
 Consultancy services for preparation of detailed project report for Rehabilitation and upgrading of existing 2-lane road to 4-lane divided Carriageway configuration of Jammu - Udhampur section of NH-1A (km 6.00 to km 66.00) including Jammu/Dev) Highway and Udhampur Bypass Phase - III Programme of North - South - corridor Project, Contract Package C-0118

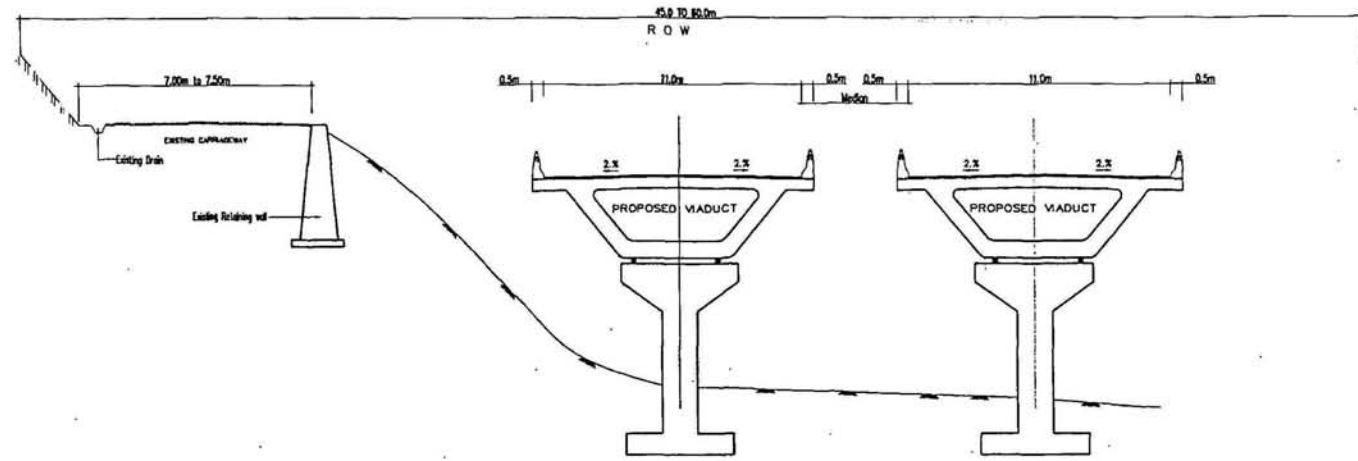
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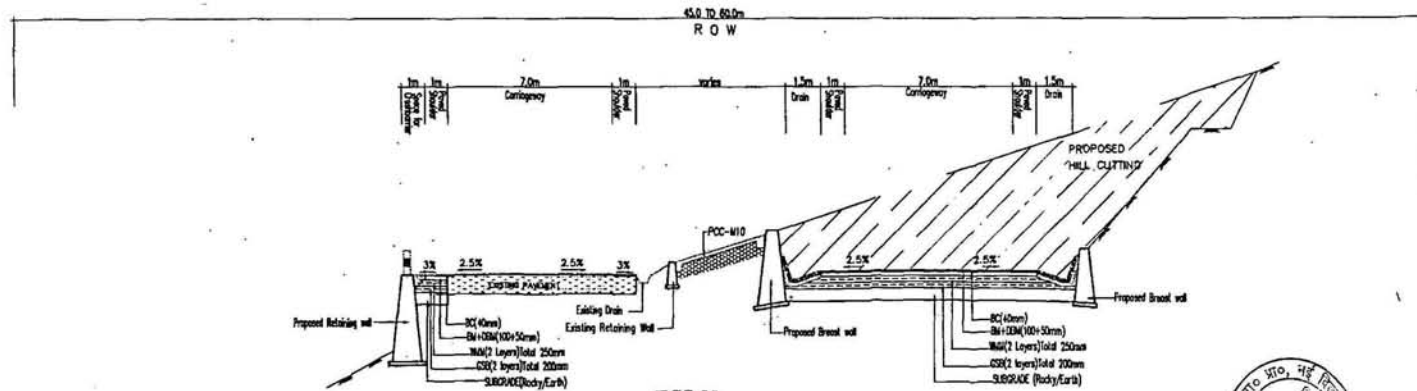
Drawing Title: TYPICAL CROSS SECTIONS
 Drawing No.: SW/1019/ROADS/CS-02
 Scale: --
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TCS-IV-A
(VALLEY SIDE WIDENING 4-LANE VIADUCT-NEW CONSTRUCTION)



TCS-V
(BOX CUTTING IN HILL SECTION IN 2-LANE WIDTH)



| Revision | By | Date | Buffer |
|----------|----|------|--------|
| | | | |
| | | | |
| | | | |

Project Title
 Consultancy services for preparation of detailed project report for Rehabilitation and widening of existing 2-lane road to 4-lane divided Carriageway configuration of Section - Unchharpur section of NH-1 A
 Km 0.00 to Km 46.00 including Junction Over-bridge and Unchharpur Bypass Phase - III Progression of Road - South - cantilever Project, Contract Package C-018

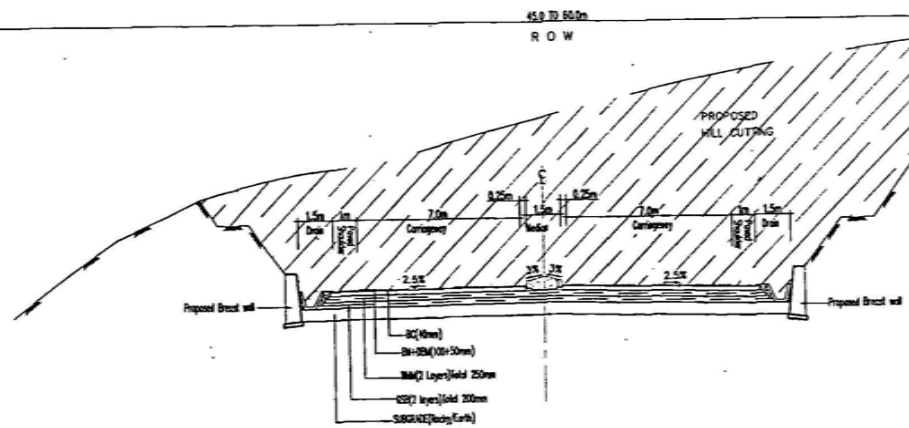
Client
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 (Ministry of Shipping, Road Transport & Highways)

Drawing Title: TYPICAL CROSS SECTIONS
Drawing No.: SW/1010/ROADS/CS-03
Scale: -
Date: A.L.N
Dgn.: V. V. GULATI
Appd: SIVG.C.D./PURI
Date: June-2009

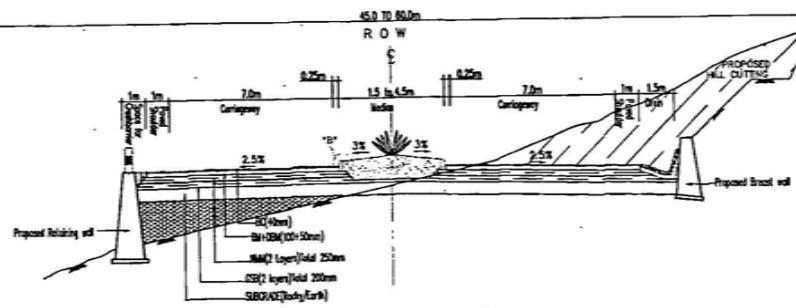
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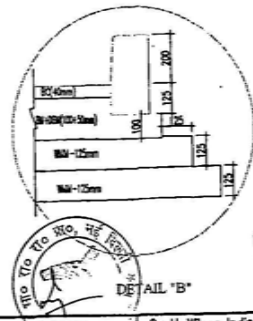




TCS-V-A
 (BOX CUTTING IN HILL SECTION IN 4-LANE WIDTH-NEW CONSTRUCTION)



TCS-VI
 (NEW 4 LANE REALIGNMENT IN HILL SECTION)



NO TO TO NO, WE FROM
 DE
 MAIL "B"

| | |
|------------------|--|
| Project File | Consolidatory services for preparation of detailed project report for Substitution and upgrading of existing 3-lane road to 4-lane divided |
| Revision Details | By: [Signature] Date: [Date] |

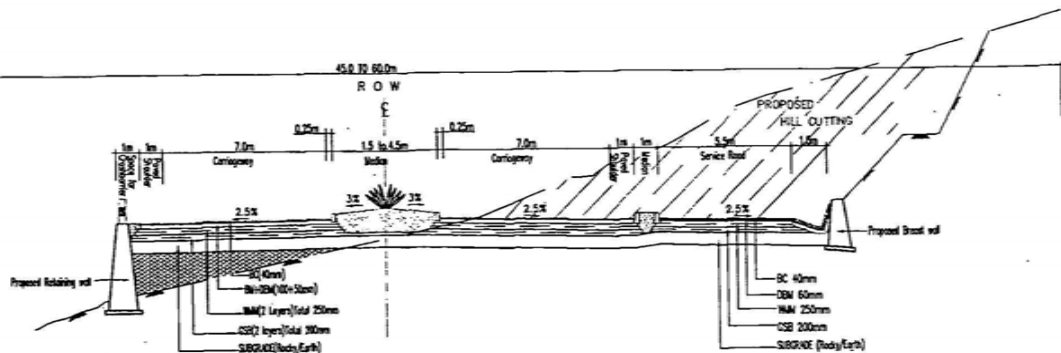
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 (Ministry of Shipping, Road Transport & Highways)

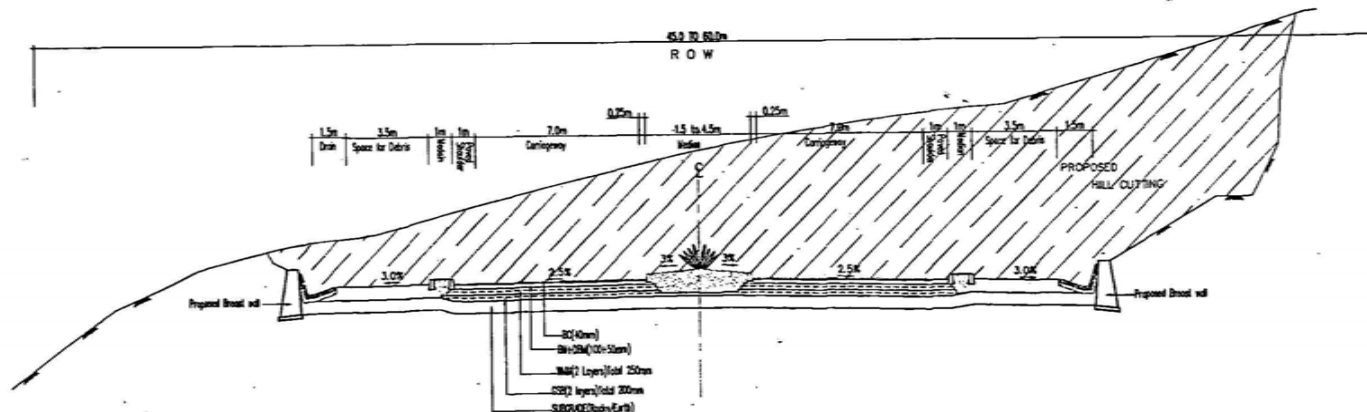
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 Drawing No.: **SW/1019/ROADS/CS-04**
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 Date: June-2009

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TCS-VI-A
(4 LANE REALIGNMENT IN HILL SECTION WITH ONE SIDE SERVICE ROAD)



TCS-VI-B
(4 LANE REALIGNMENT IN HILL SECTION WITH EXTRA SPACE FOR DEBRIS ON BOTH SIDES)



| | | | | |
|----------|---------|----|------|-------|
| Revision | Details | By | Date | Scale |
| | | | | |
| | | | | |
| | | | | |

Project Title
 Consultancy services for preparation of detailed project report for
 Rehabilitation and upgrading of existing 3-lane road to 4-lane divided
 Carriageway configuration of 3-lanes - 1-lane median section of NH-1 A
 (Km 6.00 to Km 66.00) including Interchange (I-1) Nagpur and 1-lane
 Bypass Phase - II Programme of North - South - corridor Project,
 Contract Package C-2011

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 Original Size: A2
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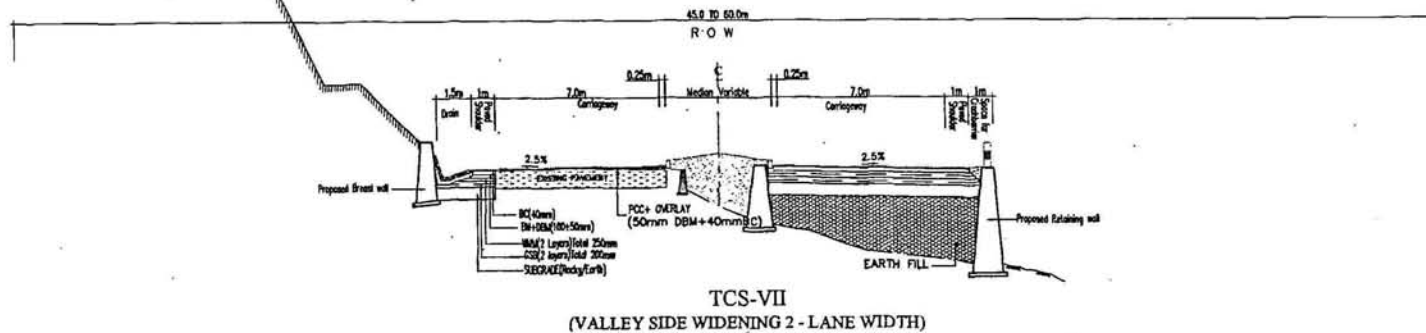
Client
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 AUTHORITY OF INDIA**
 (Ministry of Shipping, Road Transport & Highways)

Drawing Title: **TYPICAL CROSS SECTIONS**
 Drawing No.: **SW/1019/ROADS/CS-05** Sheet :
 Scale : - (5 of 14)
 Dn: K.L.N. Dgn: V. V. QULATI Appd: SRG.C.O.PUR Date: June-2009

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 E-mail: info@swindia.com

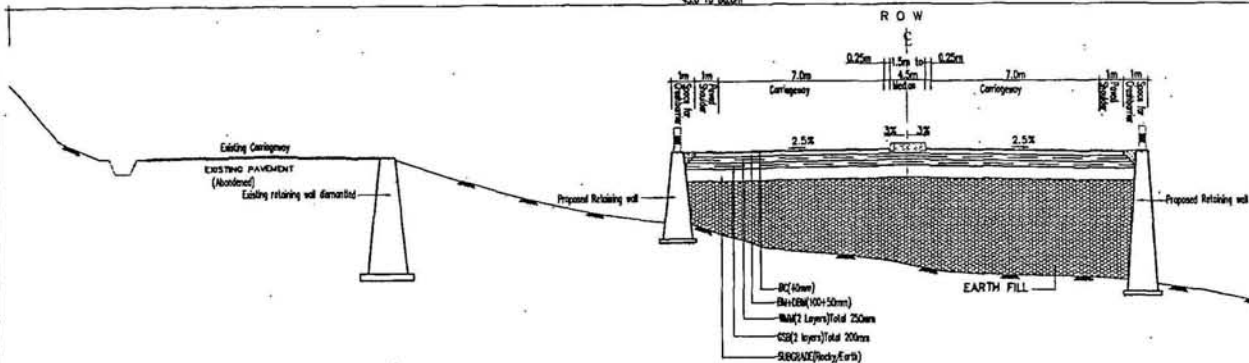


HILL SIDE



TCS-VII
(VALLEY SIDE WIDENING 2 - LANE WIDTH)

45.0 TO 80.0m



TCS-VII-A
(VALLEY SIDE WIDENING 4-LANE WIDTH)



| Rev | By | Date | Surf |
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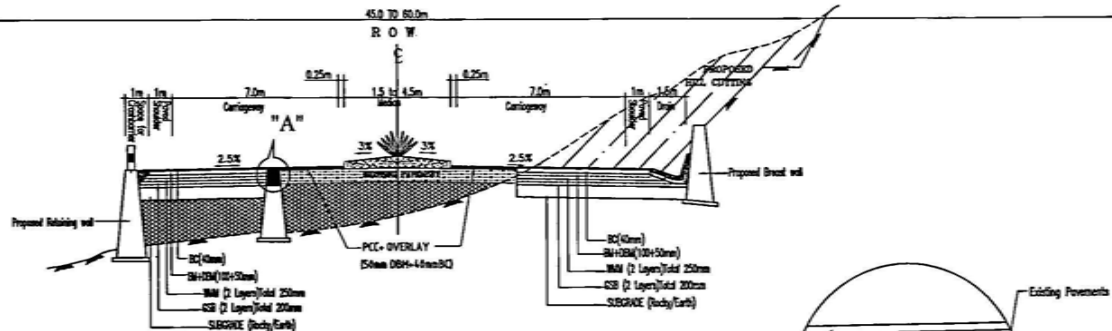
Project Title
 Consultancy services for preparation of detailed project report for Rehabilitation and upgrading of existing 2-lane road to 4-lane divided Carriageway configuration of Jammu - Udhampur section of NH-1 A. (km 0.00 to km 66.00) including Jammu (Part) Bypass and Udhampur Bypass Phase - III Programme of North - South - corridor Project, Contract Package C-02/18

Client
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 (Ministry of Shipping, Road Transport & Highways)

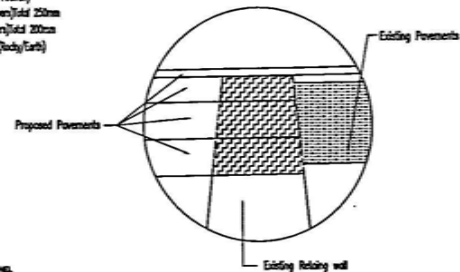
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Date: June-2009

Sheet: (8 OF 14)
Scale: ---
Date: June-2009

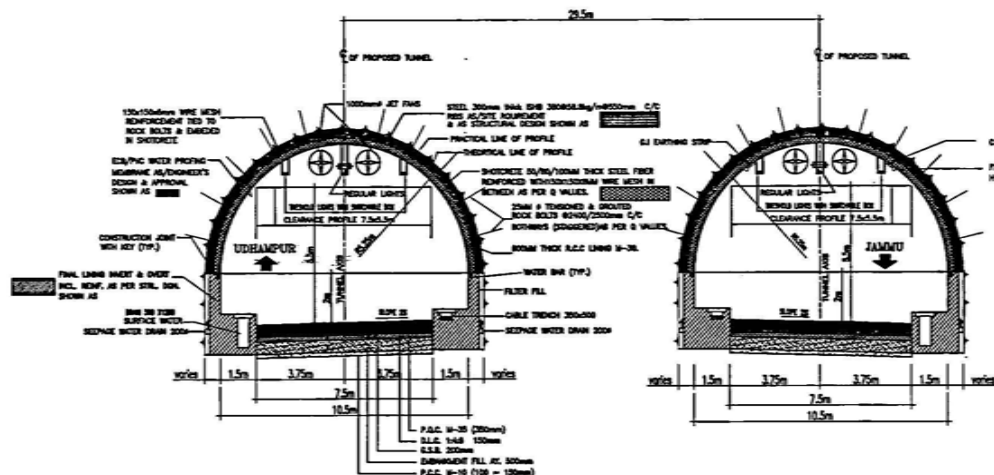
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TCS-VIII
(CONCENTRIC WIDENING CUT & FILL)



DETAIL - A

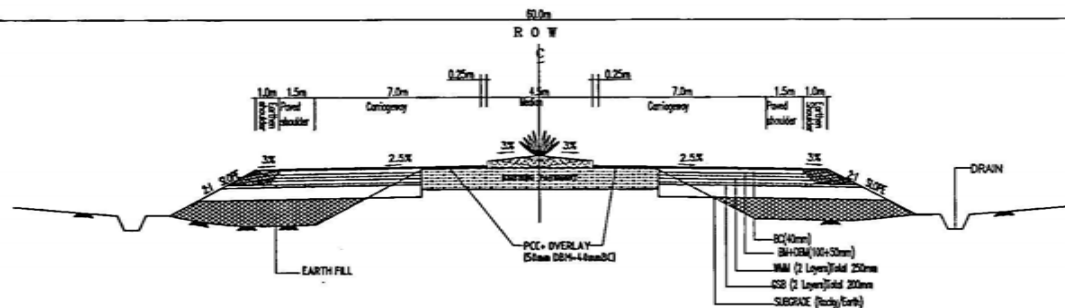


TCS-IX
(TWIN TUNNEL FOR 4-LANE REALIGNMENT)

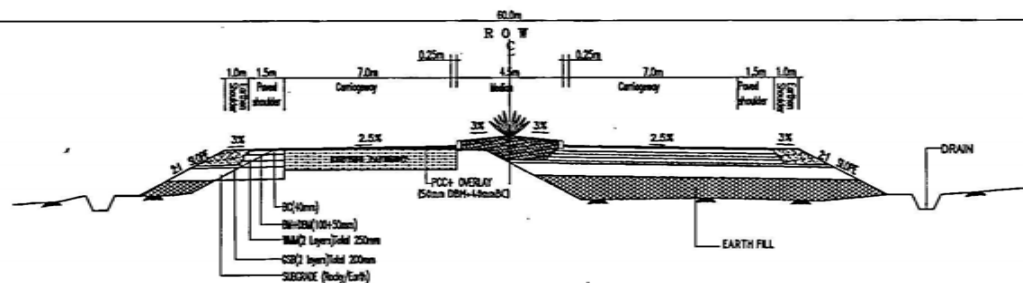
NOTE:-
Hatch area of the Existing Binding soil should be demolished and proposed pavements should laid properly



| | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
|--|------|------|------|------|------|------|------|--|------|------|------|------|------|------|------|------|------|------|------|------|------|------|------|------|------|------|------|------|------|------|------|------|------|------|------|------|------|------|------|------|--|---|--|
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| | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| 1000 | 1000 | 1000 | 1000 | 1000 | 1000 | 1000 | 1000 | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
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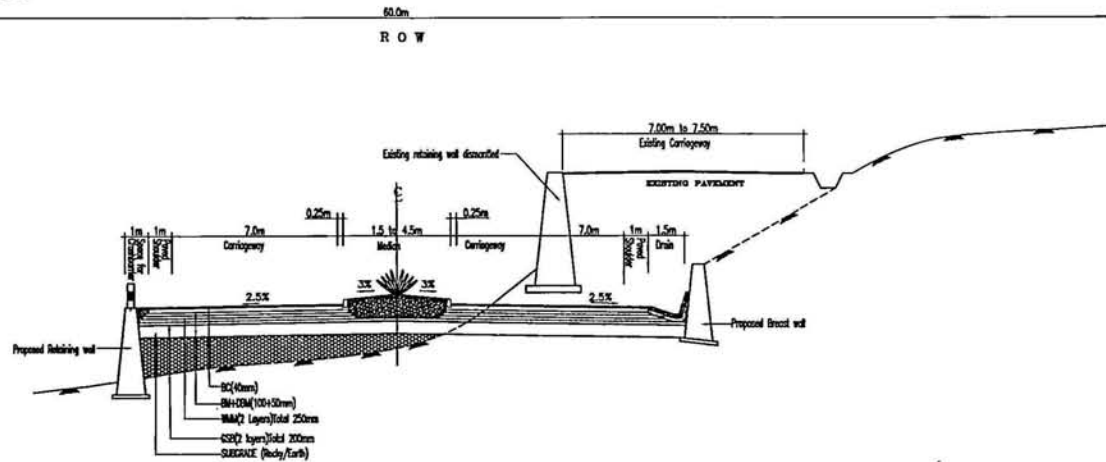
TCS-X
(CONCENTRIC WIDENING IN ROLLING TERRAIN)



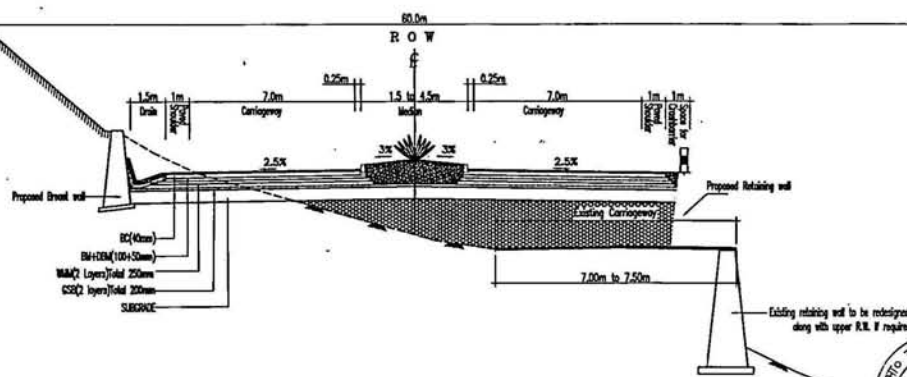
TCS-X-A
(ECCENTRIC WIDENING IN ROLLING TERRAIN)



| | | | | | | | |
|---|--|---|--|--|--|--|--|
| Project Title Consultancy services for preparation of detailed project report for Rehabilitation and upgrading of existing 2-lane road to 4-lane divided Carriageway configuration of Jaunpur - Udhampur section of NH-1 A (km 0+00 to km 66+00) including Jaunpur(2+1) Viaduct and Udhampur Bypass Phase - II Programme of North-South corridor Project, Correct Package C4318 | | Client NATIONAL HIGHWAYS AUTHORITY OF INDIA (Ministry of Shipping, Road Transport & Highways) | | Drawing Title: TYPICAL CROSS SECTIONS Drawing No.: SW/1019/ROADS/CS-08 Scale: NTS Dm: KLN Dgn: V. V. GULATI Appd: BRIG.C.D.PURI Date: June-2009 | | Scott Wilson India Private Limited A-25/A, MOHAN CO-OPERATIVE IND. ESTATE, WATHURA ROAD NEW DELHI - 110044 Tele: (011) 41679340 - 49 Fax: (011) 41679350 E-mail: sabin@scottwilson.com | |
| Revision Details By: CHN Date: SUFF: | | Original Size: A3 File Name: \... | | Sheet: (8 OF 14) | | | |



TCS-XI
(REGRADING BY LOWERING AND WIDENING AT LOWER LEVEL)



TCS-XII
(REGRADING BY FILLING & HILL SIDE WIDENING AT RAISED LEVEL)



| | | | | | |
|----------|---------|----|-----|------|--------|
| Revision | Details | By | Chk | Date | Suffix |
| | | | | | |

Project Title
 Consultancy services for preparation of detailed project report for Rehabilitation and upgrading of existing 2-lane road to 4-lane divided Carriageway configuration of Jammu - Udhampur section of NH-1 A (from 0.00 to less 66.00) including Jammu/Part) Nagrova and Udhampur Bypass Phase - III Programme of North - South - corridors Project, Connect Package C-07/8

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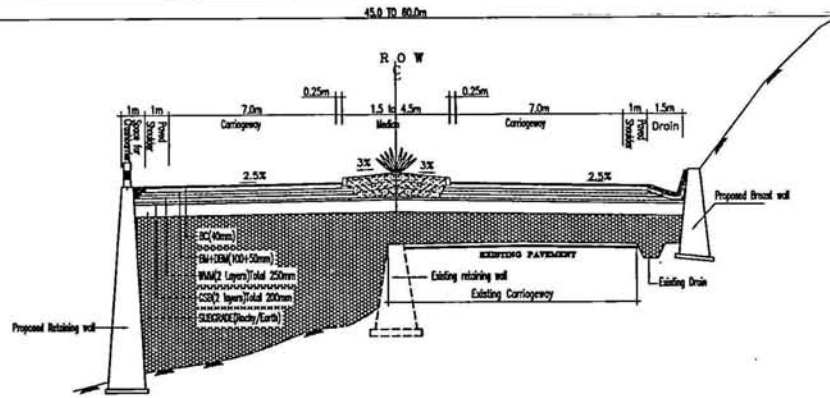
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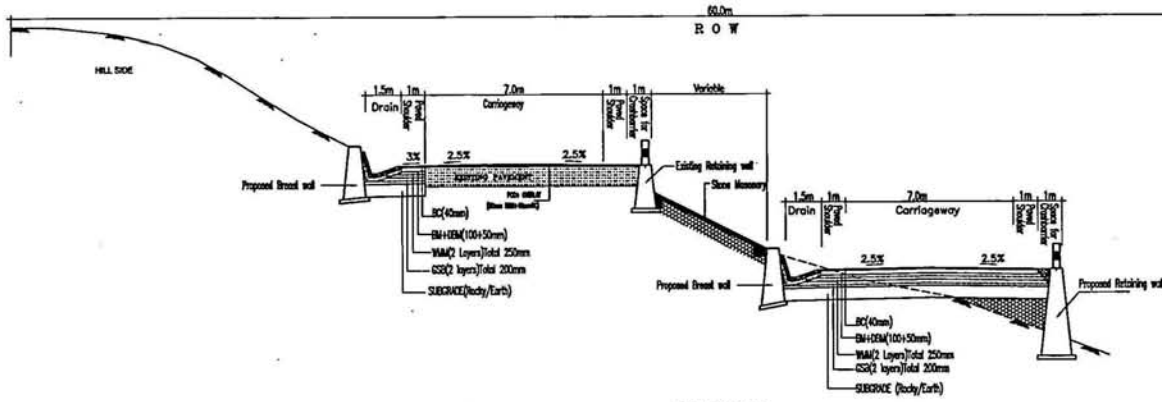
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 Drawing No.: SWM/1019/ROADS/CS-09
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 Dwn: K.L.N. Dgn: V. V. CULATI Appd: BRIG.C.D.PURJ Date: June-2009

Scott Wilson India Private Limited
 A-26/4, MOHAN CO-OPERATIVE IND. ESTATE
 MATHURA ROAD
 NEW DELHI - 110044
 Tele: (011) 41679340 - 49
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 E-mail: scwilson@gmail.com





TCS-XII-A
(REGRADEING BY FILLING & VALLEY SIDE WIDENING AT RAISED LEVEL)

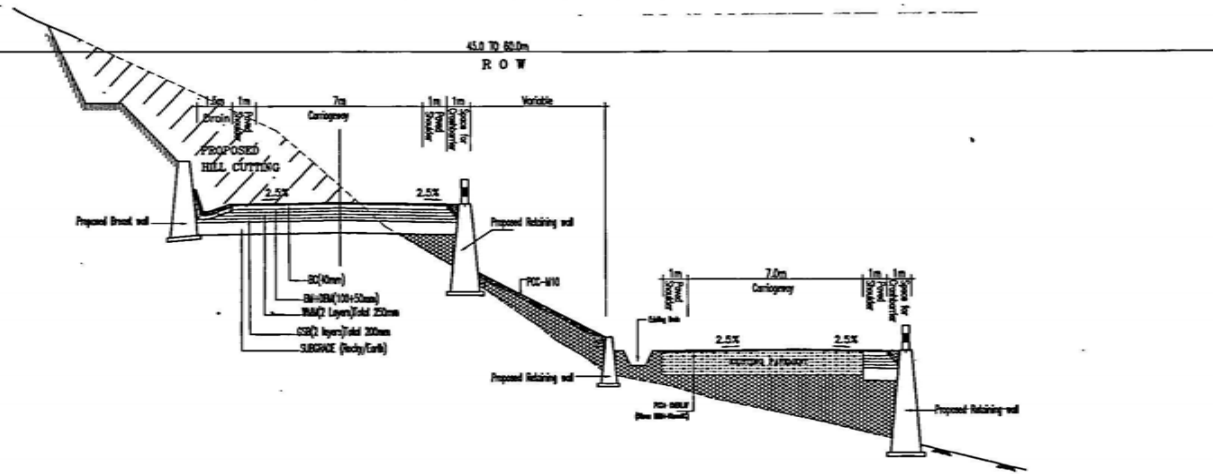


TCS-XIII
(VALLEY SIDE WIDENING WITH 2-LANE LOW LEVEL/SAME LEVEL ROAD)

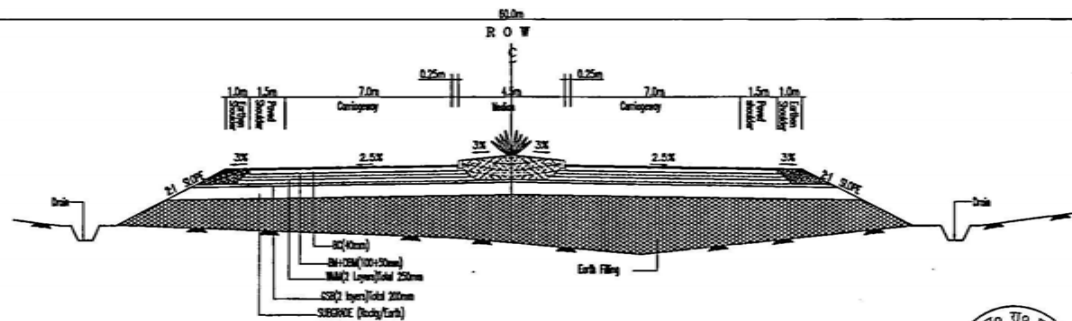


| | | | | | |
|--|-------------------------|--|---|---------------------------|---|
| <p>Project Title Consultancy services for preparation of detailed project report for Rehabilitation and upgrading of existing 2-lane road to 4-lane divided Carriageway configuration of Jaunpur - Uthmanpur section of NH-1 A (km 0.00 to km 66.00) including Jaunpur/Par) Nagpur and Uthmanpur Bypass Phase - II Programme of North - South - corridors Project.</p> <p>Scale :- M:15</p> <p>Contract Package C-0118</p> | <p>Original Size A3</p> | <p>Clients NATIONAL HIGHWAYS AUTHORITY OF INDIA (Ministry of Shipping, Road Transport & Highways)</p> | <p>Drawing Title: TYPICAL CROSS SECTIONS</p> <p>Drawing No.: SW/1019/ROADS/CS- 10</p> <p>Scale :- M:15</p> <p>Dgn. V. V. GULATI</p> <p>Appd. BRIG.C.D.PURJ</p> <p>Date June-2009</p> | <p>Sheet : (10 OF 14)</p> | <p>Scott Wilson India Private Limited A-26/4, MOHAN CO-OPERATIVE IND. ESTATE MATHURA ROAD NEW DELHI -110044 Tel: (011) 41679340 - 49 Fax: (011) 41679350 E-mail: swin@scwpl.com</p>  |
|--|-------------------------|--|---|---------------------------|---|

Revision Details
By: CHM Date: Suffix:



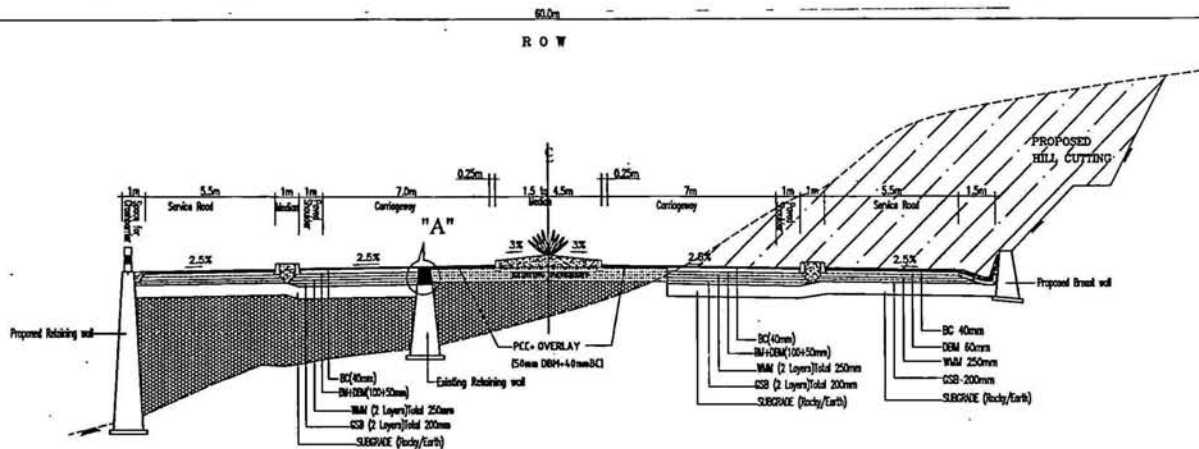
TCS-XIV
(HILL SIDE WIDENING WITH 2-LANE HIGH LEVEL/SAME LEVEL ROAD)



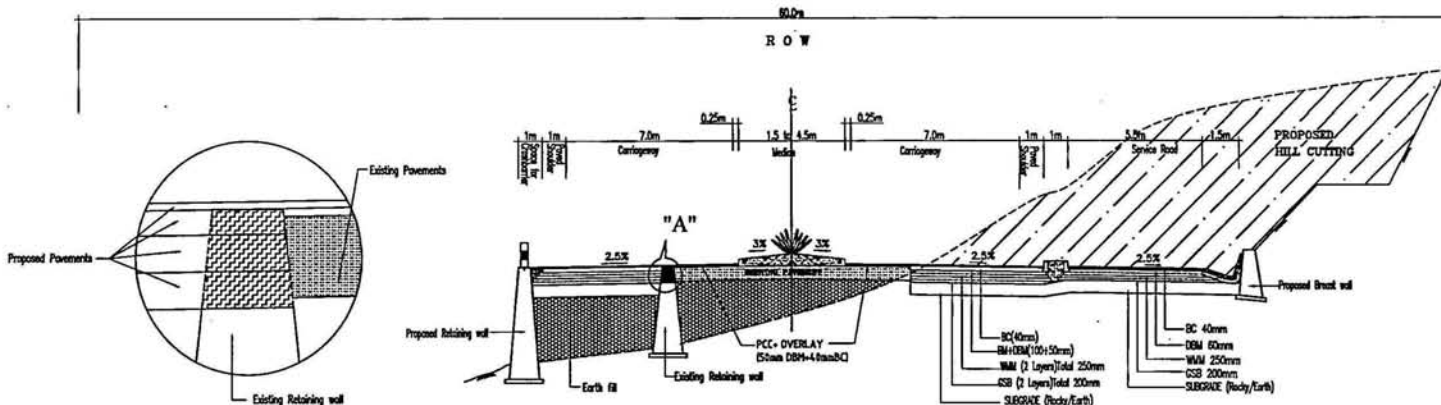
TCS-XV
(NEW 4 LANE REALIGNMENT IN ROLLING TERRAIN)



| <p>Revision Details</p> <table border="1"> <tr> <th>By</th> <th>Chk</th> <th>Date</th> <th>Suffix</th> </tr> <tr> <td> </td> <td> </td> <td> </td> <td> </td> </tr> </table> | By | Chk | Date | Suffix | | | | | <p>Project Title Consultancy services for preparation of detailed project report for Rehabilitation and upgrading of existing 2-lane road to 4-lane divided Carriageway configuration of Jammu - Udhampur section of NH-1 A (from 0+00 to Km 66.00) including Jammu/Paru) Nagrota and Udhampur Bypass Phase - II Programme of North - South - corridors Project, Chennai Package C-03/18</p> | <p>Client NATIONAL HIGHWAYS AUTHORITY OF INDIA (Ministry of Shipping, Road Transport & Highways)</p> | <p>Drawing Title: TYPICAL CROSS SECTIONS Drawing No.: SW/1019/ROADS/CS- 11 Scale: -- NTS Dwn: K.L.N Dgn: V. V. GULATI Appd: BRIG.C.D.PUR Date: June-2009</p> | <p>Sheet: (11 OF 14)</p> | <p>Scott Wilson India Private Limited A-25/4, MISHAN CO-OPERATIVE HO. ESTATE WAZIRPUR ROAD NEW DELHI - 110044 Tele: (011) 41579340 - 49 Fax: (011) 41579350 E-mail: s.wilco@psnl.com</p>  |
|--|-----|------|--------|--------|--|--|--|--|--|---|--|--------------------------|--|
| By | Chk | Date | Suffix | | | | | | | | | | |
| | | | | | | | | | | | | | |



TCS-XVI
(CONCENTRIC WIDENING HILL CUT & FILL WITH BOTH SIDE SERVICE ROAD)

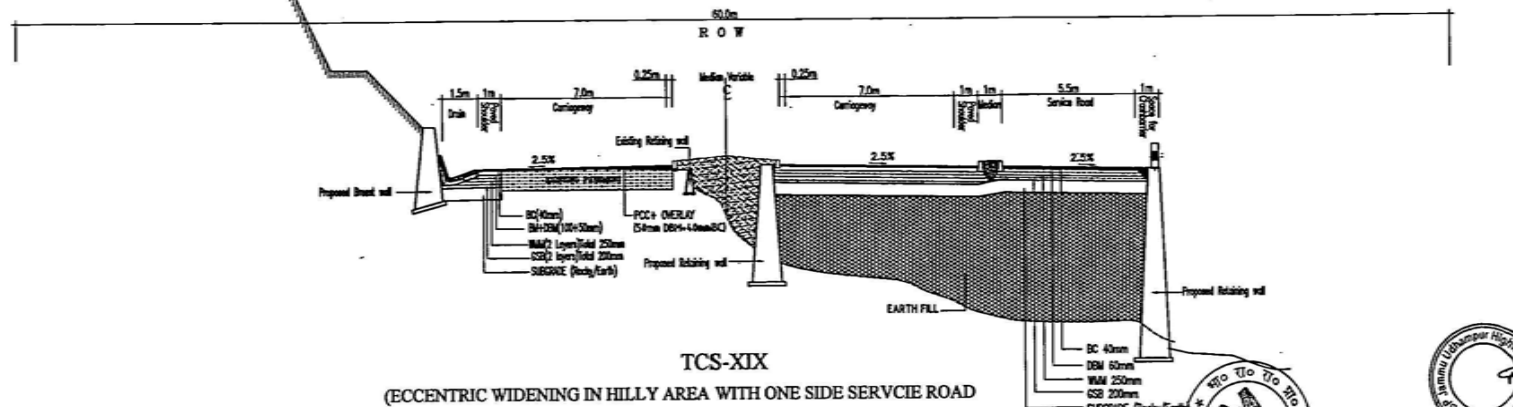
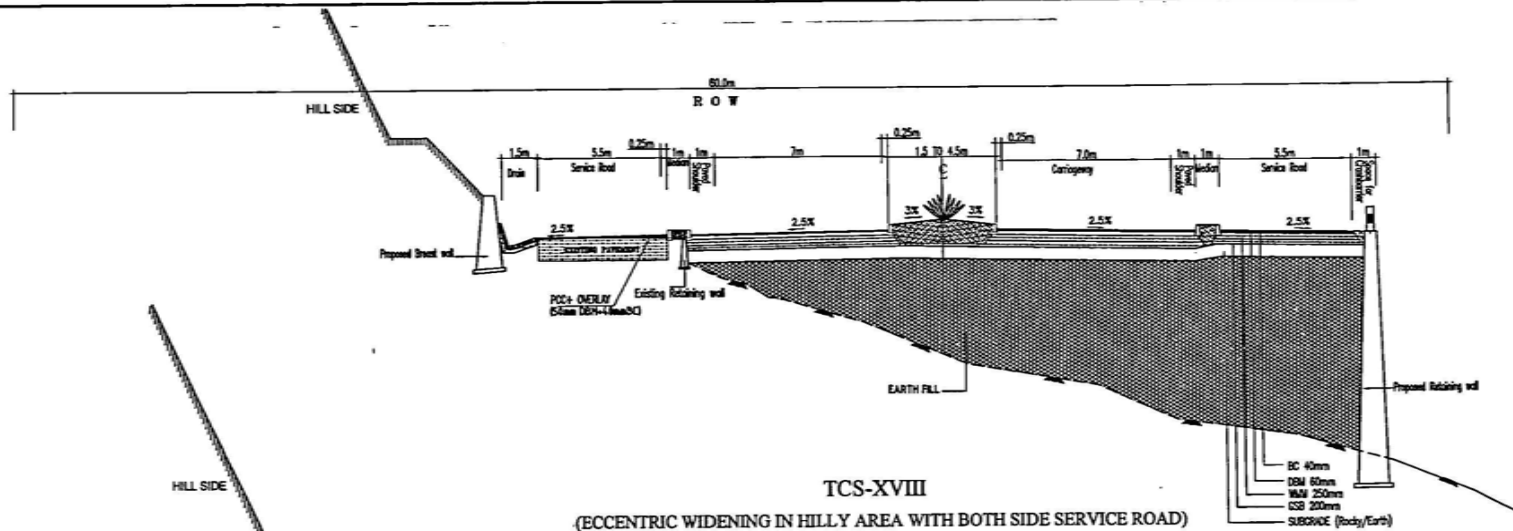


TCS-XVII
(CONCENTRIC WIDENING HILL CUT & FILL WITH ONE SIDE SERVICE ROAD)

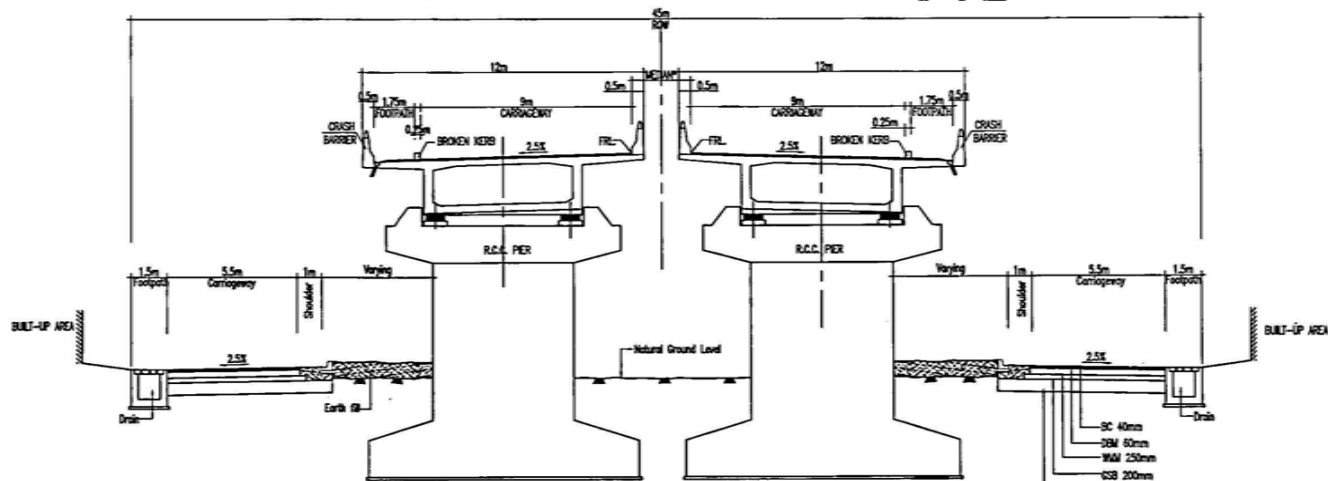
NOTE-
Hatch area of the Existing Retaining wall should be demarcated and proposed pavements should laid properly



| | | | | | |
|---|---|--|--|---|---|
| Revision Details By: [] Chk: [] Date: [] Suffix: [] | Project Title Consultancy services for preparation of detailed project report for Rehabilitation and upgrading of existing 2-lane road to 4-lane divided Carriageway configuration of Jammu - Udhampur section of NH-1A (Km 0.00 to Km 66.00) including Jammu(Pur) Bypass and Udhampur Bypass Phase - III Programme of North - South - corridor Project, Contract Package C-0318 | Client NATIONAL HIGHWAYS AUTHORITY OF INDIA (Ministry of Shipping, Road Transport & Highways) | Drawing Title: TYPICAL CROSS SECTIONS | Drawing No.: SW/1019/ROADS/CS- 12 Scale :- NTS Dm: K.L.N Dgn: V. V. GULATI Appd: BRIG.C.D.PURJ Date: June-2009 | Sheet : (12 OF 14) Scott Wilson India Private Limited A-28/A, MOHAN CO-OPERATIVE IND. ESTATE WATHRUA ROAD NEW DELHI - 110044 Tele: (011) 41679340 - 49 Fax: (011) 41679350 E-mail : scwinfo@scw.com |
| | Original Size: A3 File Name: [] | Scott Wilson | | | |

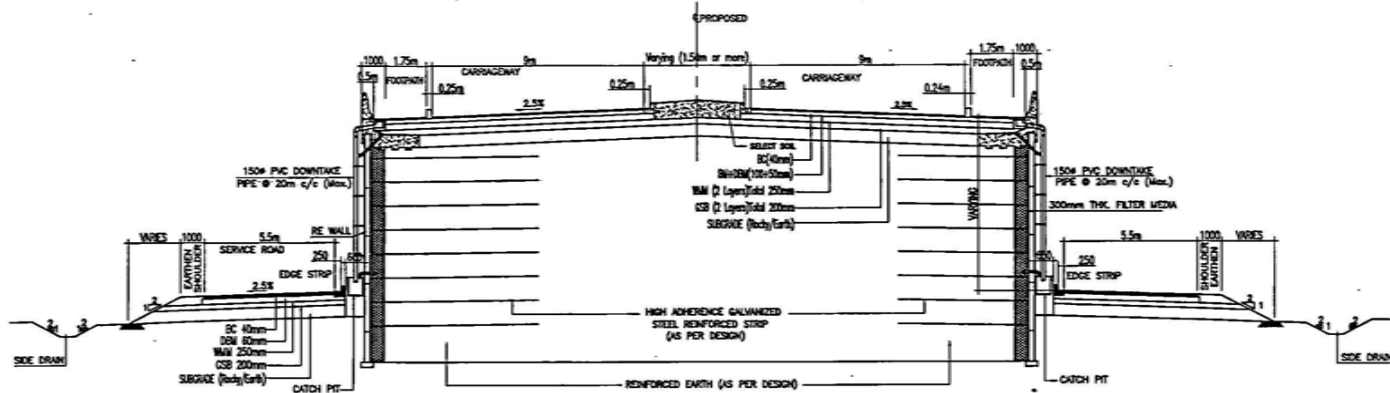


| | | | | |
|--|--|--|---|--|
| Revision Details No. of Date By Date Suffix | Project Title Consultancy services for preparation of detailed project report for Rehabilitation and upgrading of existing 2-lane road to 4-lane divided Carriageway configuration of Jammu - Udhampur section of NS-1 A. (km 0.00 to km 66.00) including Jammu/Puri/Nagarn and Udhampur Bypass Phase - III Programme of Work - South - corridor Project, Contract Package C-0918 | Client NATIONAL HIGHWAYS AUTHORITY OF INDIA (Ministry of Shipping, Road Transport & Highways) | Drawing Title: TYPICAL CROSS SECTIONS | Client Scott Wilson India Private Limited A-26/4, MCHW CO-OPERATIVE IND. ESTATE MATURA ROAD NEW DELHI - 110044 Tele: (011) 41679340 - 49 Fax: (011) 41679350 E-mail: - swi@scw.com |
| | Drawing No.: SW/1019/ROADS/CS-13 Scale: - KTS Date: K.L.N. Design: V. V. GULATI Appd: BRG.C.D.PUR | Sheet: (13 OF 14) Date: June-2009 | Original Size: A2 Path: \\scw\projects\1019\ROADS\CS-13\Drawings\TCS-XVIII.dwg | Logo of Scott Wilson |



TCS-IV B

(4 LANE BRIDGE WITH BOTH SIDE SERVICE ROAD)



TCS-XX

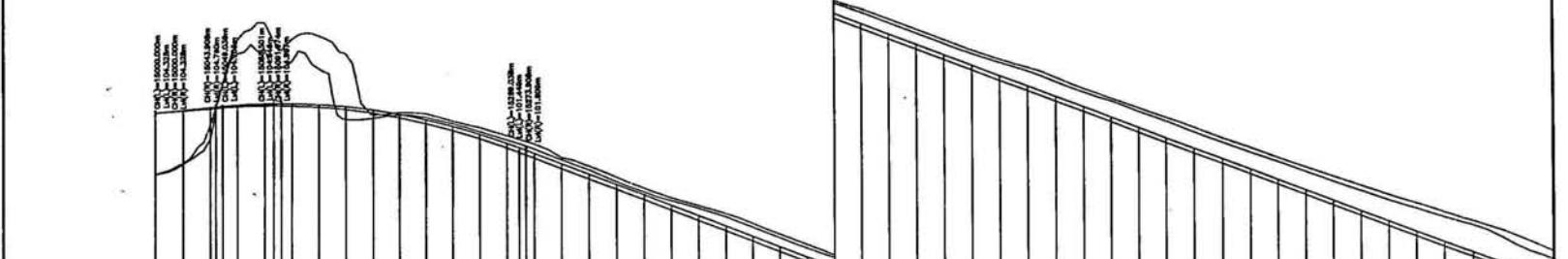
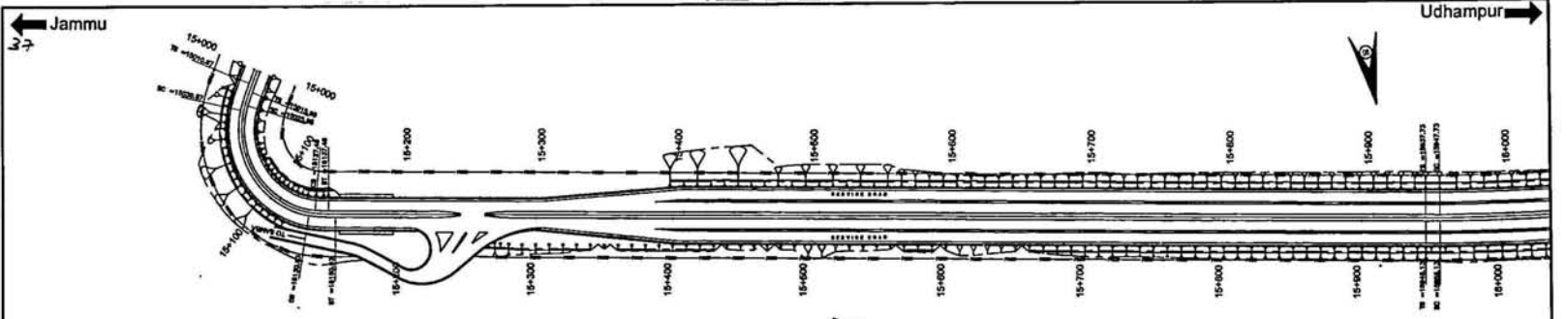
(4 LANE APPROACH WITH RE WALL ON BOTH SIDE SERVICE ROAD)

| | | | | | | | |
|---|--|---|--|--|--|---|--|
| <p>Project Title Consultancy services for preparation of detailed project report for Rehabilitation and upgrading of existing 2-lane road to 4-lane divided Carriageway configuration of Jammu - Udhampur section of NH-1 A (from 10.00 to km 66.00) including Jammu(Pur) Bypass and Udhampur Bypass Phase - III Progression of North - South - corridor Project, Contract Package C-0313</p> | | <p>Client NATIONAL HIGHWAYS AUTHORITY OF INDIA (Ministry of Shipping, Road Transport & Highways)</p> | | <p>Drawing Title: TYPICAL CROSS SECTIONS Drawing No: SW/1019/ROADS/CS-14 Scale: - N.T.S. Date: June-2009</p> | | <p>Scott Wilson India Private Limited A-26/4, MOHAN CO-OPERATIVE IND. ESTATE MATHURA ROAD NEW DELHI - 110044 Tele: (011) 41679340 - 49 Fax: (011) 41679350 E-mail: scwilson@gmail.com</p> | |
| <p>Revision Details</p> | | <p>Original Scale: A2</p> | | <p>Drawn: K.L.W. Dgn: V. V. GULATI Appd: BRG.C.D.PUR Date: June-2009</p> | | <p>(14 of 14)</p> | |



PLAN AND PROFILE DRAWINGS

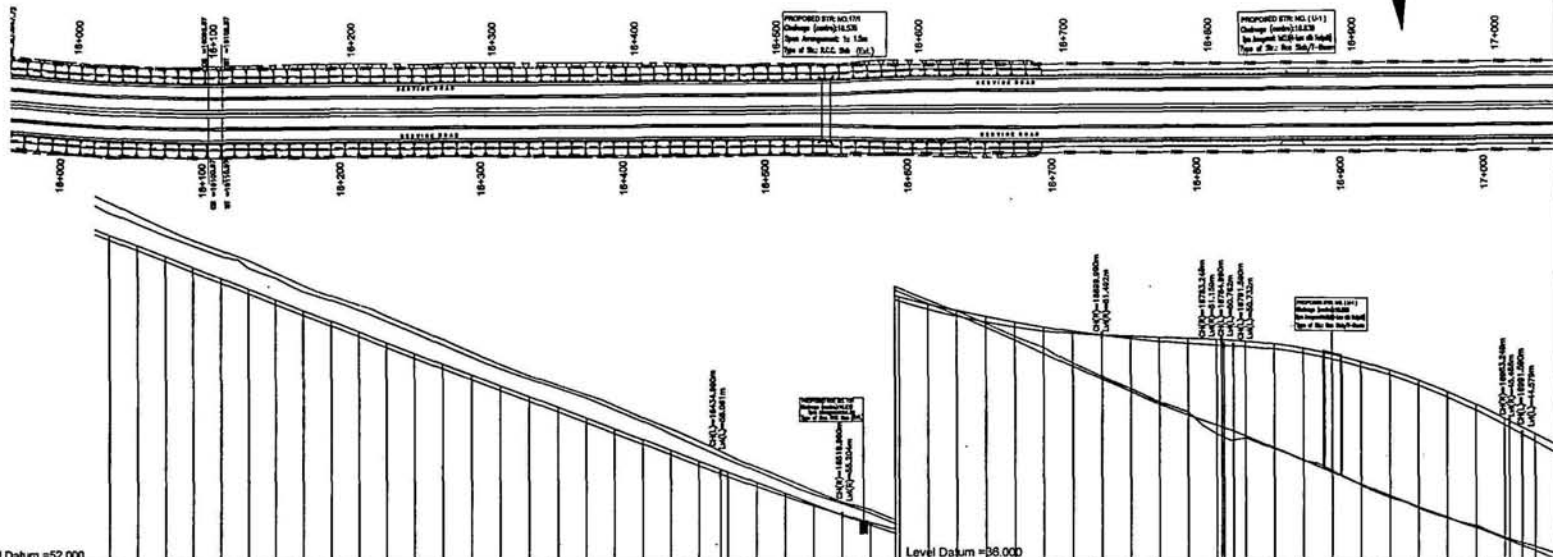




| | | | |
|--|---|--|---|
| Level Datum = 82.000 | | Level Datum = 74.000 | |
| Proposed Level along Left Median Edge (m) | 15+000: 150.0000 15+100: 150.0000 15+200: 150.0000 15+300: 150.0000 15+400: 150.0000 15+500: 150.0000 15+600: 150.0000 15+700: 150.0000 15+800: 150.0000 15+900: 150.0000 15+1000: 150.0000 | Existing Level Below Left Median Edge (m) | 15+000: 150.0000 15+100: 150.0000 15+200: 150.0000 15+300: 150.0000 15+400: 150.0000 15+500: 150.0000 15+600: 150.0000 15+700: 150.0000 15+800: 150.0000 15+900: 150.0000 15+1000: 150.0000 |
| Proposed Level along Right Median Edge (m) | 15+000: 150.0000 15+100: 150.0000 15+200: 150.0000 15+300: 150.0000 15+400: 150.0000 15+500: 150.0000 15+600: 150.0000 15+700: 150.0000 15+800: 150.0000 15+900: 150.0000 15+1000: 150.0000 | Existing Level Below Right Median Edge (m) | 15+000: 150.0000 15+100: 150.0000 15+200: 150.0000 15+300: 150.0000 15+400: 150.0000 15+500: 150.0000 15+600: 150.0000 15+700: 150.0000 15+800: 150.0000 15+900: 150.0000 15+1000: 150.0000 |
| Horizontal Alignment Left | Horizontal Alignment Right | Vertical Alignment Left | Vertical Alignment Right |
| LHS Super-elevation | RHS Super-elevation | Left Profile Chainage | Right Profile Chainage |

| | | | | | |
|--|---|--|---|--|--|
| 1. Existing 2-lane 2. Proposed 4-lane 3. Proposed 2-lane 4. Proposed 1-lane 5. Proposed 0.5-lane 6. Proposed 0.25-lane 7. Proposed 0.125-lane 8. Proposed 0.0625-lane 9. Proposed 0.03125-lane 10. Proposed 0.015625-lane 11. Proposed 0.0078125-lane 12. Proposed 0.00390625-lane 13. Proposed 0.001953125-lane 14. Proposed 0.0009765625-lane 15. Proposed 0.00048828125-lane 16. Proposed 0.000244140625-lane 17. Proposed 0.0001220703125-lane 18. Proposed 0.00006103515625-lane 19. Proposed 0.000030517578125-lane 20. Proposed 0.0000152587890625-lane 21. Proposed 0.00000762939453125-lane 22. Proposed 0.000003814697265625-lane 23. Proposed 0.0000019073486328125-lane 24. Proposed 0.00000095367431640625-lane 25. Proposed 0.000000476837158203125-lane 26. Proposed 0.0000002384185791015625-lane 27. Proposed 0.00000011920928955078125-lane 28. Proposed 0.000000059604644775390625-lane 29. Proposed 0.0000000298023223876953125-lane 30. Proposed 0.00000001490116119384765625-lane 31. Proposed 0.000000007450580596923828125-lane 32. Proposed 0.0000000037252902984619140625-lane 33. Proposed 0.000000001862645149230571015625-lane 34. Proposed 0.0000000009313225746152855078125-lane 35. Proposed 0.00000000046566128730764275390625-lane 36. Proposed 0.000000000232830643653821376953125-lane 37. Proposed 0.0000000001164153218269109571015625-lane 38. Proposed 0.000000000058207660913455475390625-lane 39. Proposed 0.0000000000291038304567272876953125-lane 40. Proposed 0.000000000014551915228363643821376953125-lane 41. Proposed 0.0000000000072759576141818219109571015625-lane 42. Proposed 0.0000000000036379788070909105475390625-lane 43. Proposed 0.000000000001818989403545452272876953125-lane 44. Proposed 0.00000000000090949470177272876953125-lane 45. Proposed 0.0000000000004547473508863643821376953125-lane 46. Proposed 0.0000000000002273736754431821376953125-lane 47. Proposed 0.00000000000011368683772219109571015625-lane 48. Proposed 0.00000000000005684341855955475390625-lane 49. 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Proposed 0.0000000000000000000000000001009741594409571015625-lane 98. Proposed 0.00000000000000000000000000005048707972876953125-lane 99. Proposed 0.0000000000000000000000000000252435398643821376953125-lane 100. Proposed 0.00000000000000000000000000001262176993219109571015625-lane | Project Title Consultancy services for preparation of detailed project report for Rehabilitation and upgrading of existing 2-lane road to 4-lane divided Category-II configuration of Jammu - Udhampur section of NH-44 A (Km 0.00 to Km 16.00) including Jammu(Ur) Hgrs and Udhampur Bypass Phase - III Programme of North - South corridor Project, Contract Package C-4013 | This drawing is the property of SCOTT WILSON INDIA PRIVATE LIMITED and must not be used for any other project or any other purpose without the written consent of SCOTT WILSON INDIA PRIVATE LIMITED. The drawing is to be used only for the purposes for which it is intended. The drawing is not to be used for any other purpose without the written consent of SCOTT WILSON INDIA PRIVATE LIMITED. | Client NATIONAL HIGHWAYS AUTHORITY OF INDIA (Ministry of Shipping, Road Transport & Highways) | Drawing Title: PLAN & PROFILE Km. 15+000 to 16+000 Drawing No.: SW/1019/ROADS/PP-07 Scale: 1:12000 Date: JUNE-2009 Drawn: K.L.N. Checked: K.PRASAD Appr'd: BRIG.C.D.PURI Date: | Client: Scott Wilson India Private Limited A-26/A, MOHAN CO-OPERATIVE IND. ESTATE MATURA ROAD NEW DELHI - 110044 Tel: (011) 41679340 - 49 Fax: (011) 41679350 E-mail: scwilson@scwilson.com |
|--|---|--|---|--|--|





| Station | Proposed Level along Left Median Edge (m) | Existing Level Below Left Median Edge (m) | Proposed Level along Right Median Edge (m) | Existing Level Below Right Median Edge (m) |
|---------|---|---|--|--|
| 18+000 | 78.782 | 78.674 | 79.178 | 79.070 |
| 18+050 | 78.877 | 78.769 | 79.273 | 79.165 |
| 18+100 | 78.972 | 78.864 | 79.368 | 79.260 |
| 18+150 | 79.067 | 78.959 | 79.463 | 79.355 |
| 18+200 | 79.162 | 79.054 | 79.558 | 79.450 |
| 18+250 | 79.257 | 79.149 | 79.653 | 79.545 |
| 18+300 | 79.352 | 79.244 | 79.748 | 79.640 |
| 18+350 | 79.447 | 79.339 | 79.843 | 79.735 |
| 18+400 | 79.542 | 79.434 | 79.938 | 79.830 |
| 18+450 | 79.637 | 79.529 | 80.033 | 79.925 |
| 18+500 | 79.732 | 79.624 | 80.128 | 80.020 |
| 18+550 | 79.827 | 79.719 | 80.223 | 80.115 |
| 18+600 | 79.922 | 79.814 | 80.318 | 80.210 |
| 18+650 | 80.017 | 79.909 | 80.413 | 80.305 |
| 18+700 | 80.112 | 80.004 | 80.508 | 80.400 |
| 18+750 | 80.207 | 80.099 | 80.603 | 80.495 |
| 18+800 | 80.302 | 80.194 | 80.698 | 80.590 |
| 18+850 | 80.397 | 80.289 | 80.793 | 80.685 |
| 18+900 | 80.492 | 80.384 | 80.888 | 80.780 |
| 18+950 | 80.587 | 80.479 | 80.983 | 80.875 |
| 19+000 | 80.682 | 80.574 | 81.078 | 80.970 |
| 19+050 | 80.777 | 80.669 | 81.173 | 81.065 |
| 19+100 | 80.872 | 80.764 | 81.268 | 81.160 |
| 19+150 | 80.967 | 80.859 | 81.363 | 81.255 |
| 19+200 | 81.062 | 80.954 | 81.458 | 81.350 |
| 19+250 | 81.157 | 81.049 | 81.553 | 81.445 |
| 19+300 | 81.252 | 81.144 | 81.648 | 81.540 |
| 19+350 | 81.347 | 81.239 | 81.743 | 81.635 |
| 19+400 | 81.442 | 81.334 | 81.838 | 81.730 |
| 19+450 | 81.537 | 81.429 | 81.933 | 81.825 |
| 19+500 | 81.632 | 81.524 | 82.028 | 81.920 |
| 19+550 | 81.727 | 81.619 | 82.123 | 82.015 |
| 19+600 | 81.822 | 81.714 | 82.218 | 82.110 |
| 19+650 | 81.917 | 81.809 | 82.313 | 82.205 |
| 19+700 | 82.012 | 81.904 | 82.408 | 82.300 |
| 19+750 | 82.107 | 82.000 | 82.503 | 82.395 |
| 19+800 | 82.202 | 82.095 | 82.598 | 82.490 |
| 19+850 | 82.297 | 82.190 | 82.693 | 82.585 |
| 19+900 | 82.392 | 82.285 | 82.788 | 82.680 |
| 19+950 | 82.487 | 82.380 | 82.883 | 82.775 |
| 20+000 | 82.582 | 82.475 | 82.978 | 82.870 |

| Part | Description |
|------|-------------------------|
| 1 | 1. Proposed Level |
| 2 | 2. Existing Level |
| 3 | 3. Proposed Level Below |
| 4 | 4. Existing Level Below |
| 5 | 5. Proposed Level Above |
| 6 | 6. Existing Level Above |

Project Title
 Consultancy services for preparation of detailed project report for
 Rehabilitation and upgrading of existing 3 lanes road to 4-lane divided
 Category 2 road between Jammu - Udhampur section of NH-1A
 Km 16+00 to Km 17+000 (including Jammu Bypass) and Udhampur
 Bypass Phase - III Programme of North - South - corridor Project,
 Contract Package C-2013

Client
**NATIONAL HIGHWAYS
 AUTHORITY OF INDIA**
 (Ministry of Shipping, Road Transport & Highways)

Drawing Title: **PLAN & PROFILE**
 Km. 16+000 to 17+000 (JAMMU BYPASS)

Drawing No.: SWW/1019/ROADS/PP-02

Scale: --- HOR: 1:2000 VER: 1:200

Drn: K.L.N. Appd: K.PRASAD Date: JUN-2009

Scale: --- HOR: 1:2000 VER: 1:200

Drn: K.L.N. Appd: K.PRASAD Date: JUN-2009

Scale: --- HOR: 1:2000 VER: 1:200

Drn: K.L.N. Appd: K.PRASAD Date: JUN-2009

Scale: --- HOR: 1:2000 VER: 1:200

Drn: K.L.N. Appd: K.PRASAD Date: JUN-2009

Scale: --- HOR: 1:2000 VER: 1:200

Drn: K.L.N. Appd: K.PRASAD Date: JUN-2009



Scale: --- HOR: 1:2000 VER: 1:200

Drn: K.L.N. Appd: K.PRASAD Date: JUN-2009

Scale: --- HOR: 1:2000 VER: 1:200

Drn: K.L.N. Appd: K.PRASAD Date: JUN-2009

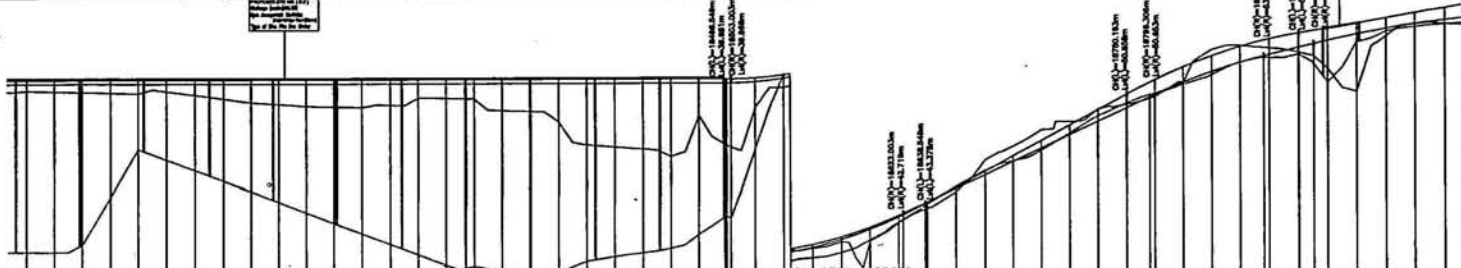
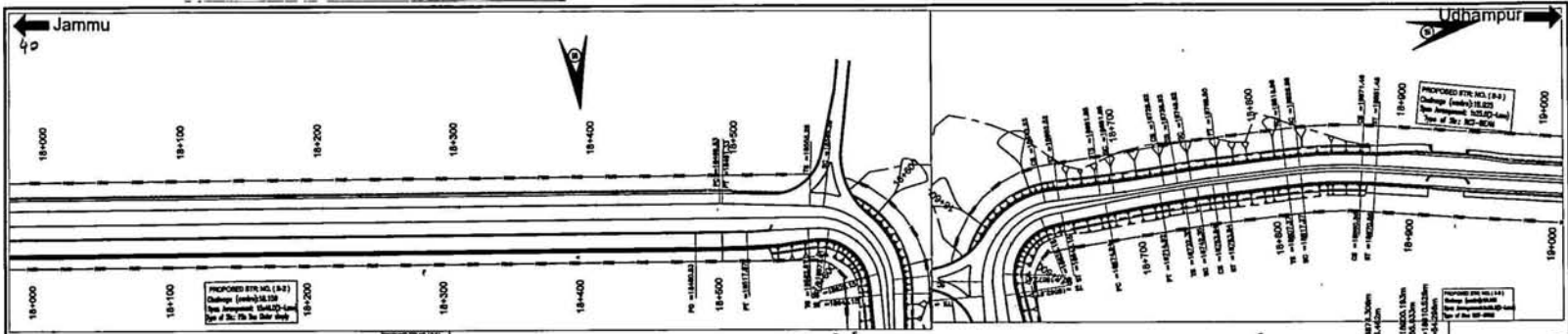
Scale: --- HOR: 1:2000 VER: 1:200

Drn: K.L.N. Appd: K.PRASAD Date: JUN-2009

Scale: --- HOR: 1:2000 VER: 1:200

Drn: K.L.N. Appd: K.PRASAD Date: JUN-2009





| Station | Proposed Level along Left Median Edge (m) | Existing Level Below Left Median Edge (m) | Proposed Level along Right Median Edge (m) | Existing Level Below Right Median Edge (m) |
|---------|---|---|--|--|
| 18+000 | 38.17 | 38.13 | 38.14 | 38.10 |
| 18+100 | 38.15 | 38.11 | 38.16 | 38.12 |
| 18+200 | 38.17 | 38.13 | 38.18 | 38.14 |
| 18+300 | 38.20 | 38.16 | 38.21 | 38.17 |
| 18+400 | 38.25 | 38.21 | 38.26 | 38.22 |
| 18+500 | 38.32 | 38.28 | 38.33 | 38.29 |
| 18+600 | 38.40 | 38.36 | 38.41 | 38.37 |
| 18+700 | 38.48 | 38.44 | 38.49 | 38.45 |
| 18+800 | 38.55 | 38.51 | 38.56 | 38.52 |
| 18+900 | 38.62 | 38.58 | 38.63 | 38.59 |
| 19+000 | 38.68 | 38.64 | 38.69 | 38.65 |

| Station | Horizontal Alignment Left | Horizontal Alignment Right |
|---------|---------------------------|----------------------------|
| 18+000 | D=433.390 | D=433.390 |
| 18+100 | D=433.390 | D=433.390 |
| 18+200 | D=433.390 | D=433.390 |
| 18+300 | D=433.390 | D=433.390 |
| 18+400 | D=433.390 | D=433.390 |
| 18+500 | D=433.390 | D=433.390 |
| 18+600 | D=433.390 | D=433.390 |
| 18+700 | D=433.390 | D=433.390 |
| 18+800 | D=433.390 | D=433.390 |
| 18+900 | D=433.390 | D=433.390 |
| 19+000 | D=433.390 | D=433.390 |

| Station | Vertical Alignment Left | Vertical Alignment Right |
|---------|-------------------------|--------------------------|
| 18+000 | L=728.423m | L=728.423m |
| 18+100 | L=728.423m | L=728.423m |
| 18+200 | L=728.423m | L=728.423m |
| 18+300 | L=728.423m | L=728.423m |
| 18+400 | L=728.423m | L=728.423m |
| 18+500 | L=728.423m | L=728.423m |
| 18+600 | L=728.423m | L=728.423m |
| 18+700 | L=728.423m | L=728.423m |
| 18+800 | L=728.423m | L=728.423m |
| 18+900 | L=728.423m | L=728.423m |
| 19+000 | L=728.423m | L=728.423m |

| Station | LHS Super-elevation | RHS Super-elevation |
|---------|---------------------|---------------------|
| 18+000 | 0=3.800 | 0=3.800 |
| 18+100 | 0=3.800 | 0=3.800 |
| 18+200 | 0=3.800 | 0=3.800 |
| 18+300 | 0=3.800 | 0=3.800 |
| 18+400 | 0=3.800 | 0=3.800 |
| 18+500 | 0=3.800 | 0=3.800 |
| 18+600 | 0=3.800 | 0=3.800 |
| 18+700 | 0=3.800 | 0=3.800 |
| 18+800 | 0=3.800 | 0=3.800 |
| 18+900 | 0=3.800 | 0=3.800 |
| 19+000 | 0=3.800 | 0=3.800 |

| Station | Left Profile Chainage | Right Profile Chainage |
|---------|-----------------------|------------------------|
| 18+000 | 18000.000 | 18000.000 |
| 18+100 | 18100.000 | 18100.000 |
| 18+200 | 18200.000 | 18200.000 |
| 18+300 | 18300.000 | 18300.000 |
| 18+400 | 18400.000 | 18400.000 |
| 18+500 | 18500.000 | 18500.000 |
| 18+600 | 18600.000 | 18600.000 |
| 18+700 | 18700.000 | 18700.000 |
| 18+800 | 18800.000 | 18800.000 |
| 18+900 | 18900.000 | 18900.000 |
| 19+000 | 19000.000 | 19000.000 |

| Rev | Description |
|-----|----------------|
| 1 | As per drawing |
| 2 | As per drawing |
| 3 | As per drawing |
| 4 | As per drawing |
| 5 | As per drawing |
| 6 | As per drawing |
| 7 | As per drawing |
| 8 | As per drawing |
| 9 | As per drawing |
| 10 | As per drawing |

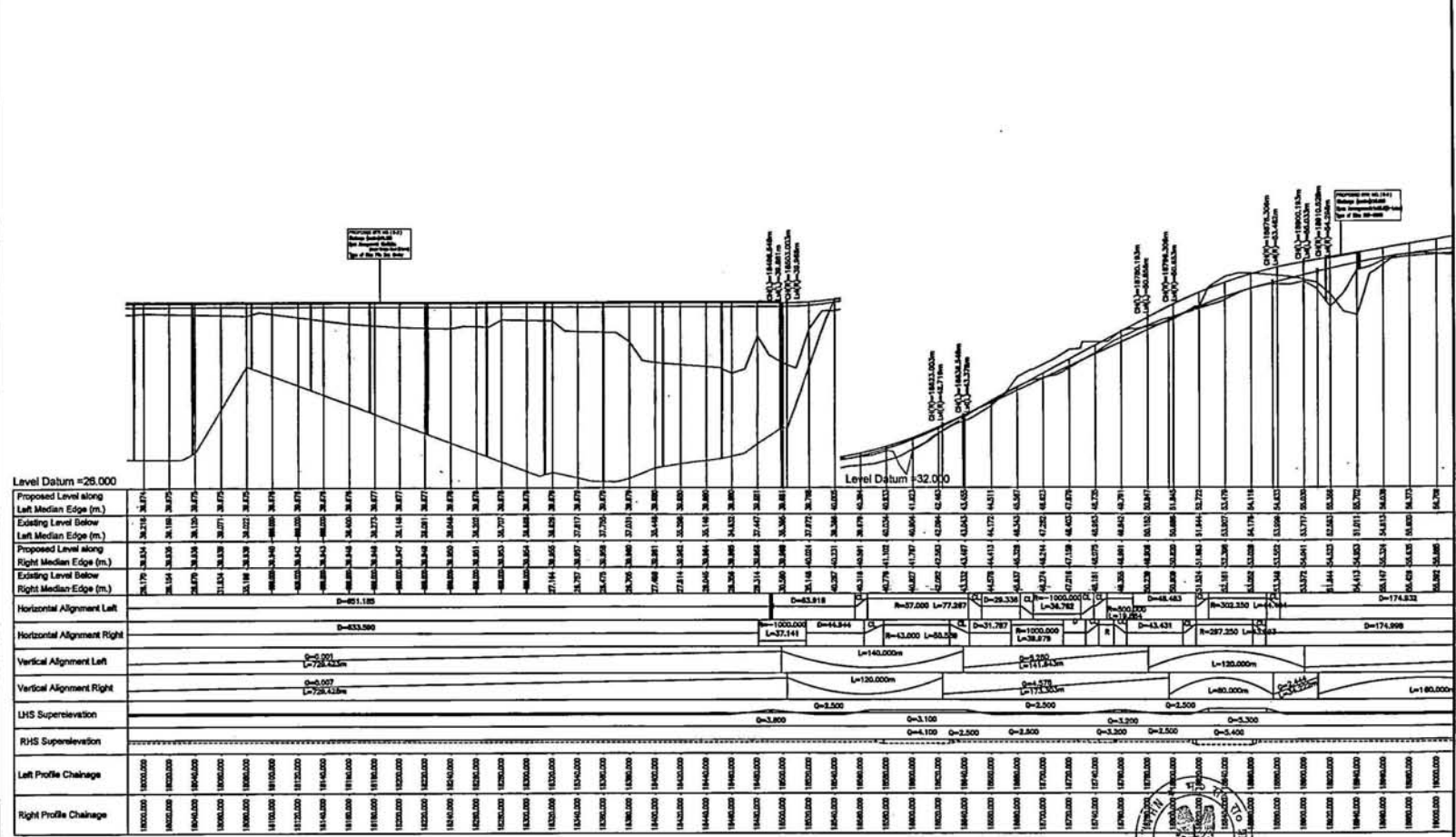
Project Title
 Consulting services for preparation of detailed project report for Rehabilitation and upgrading of existing 3-lane road to 4-lane divided Category configuration of Jammu - Udhampur section of NH-44 A (Km 0.00 to Km 64.00) including Jammu-Udhampur and Udhampur-Bypass Phase - III Programme of North-South corridor Project, Contract Package C-0511

Client
NATIONAL HIGHWAYS AUTHORITY OF INDIA
 (Ministry of Shipping, Road Transport & Highways)

Drawing Title: PLAN & PROFILE
 Km. 19+000 to 19+000 (JAMMU - UDHAMPUR)
Drawing No.: SW/1019/ROADS/PP-04
 Scale: --- HOR:-1:2000 VER:-1:200
 Dgm: K.PRASAD
 Appd: BRIG.C.D.PURI
 Date: JUNE-2009

Scale: --- HOR:-1:2000 VER:-1:200
 Dgm: K.PRASAD
 Appd: BRIG.C.D.PURI
 Date: JUNE-2009
 Project No: SW/1019/ROADS/PP-04
 Drawing No: 04
 Scale: --- HOR:-1:2000 VER:-1:200
 Dgm: K.PRASAD
 Appd: BRIG.C.D.PURI
 Date: JUNE-2009





(ISSUED)

| For | Prepared | Checked | Verified | Approved |
|-------------|----------|---------|----------|----------|
| Design | | | | |
| Estimation | | | | |
| Contract | | | | |
| Supervision | | | | |
| Final | | | | |

Project Title
 Consultancy services for preparation of detailed project report for Rehabilitation and upgrading of existing 2-lane road to 4-lane divided Contiguity configuration of Jammu - Udhampur section of NH-1 A (Km 0.00 to Km 64.00) including Jammu(Ver2) Bypass and Udhampur Bypass Phase - III Programme of North-South corridor Project, Contract Package C-0513

NATIONAL HIGHWAYS AUTHORITY OF INDIA
 (Ministry of Shipping, Road Transport & Highways)

Drawing Title: PLAN & PROFILE
 Km. 18+000 to 19+000 (JAMMU BYPASS)

Drawing No.: SW/1019/ROADS/PP-04

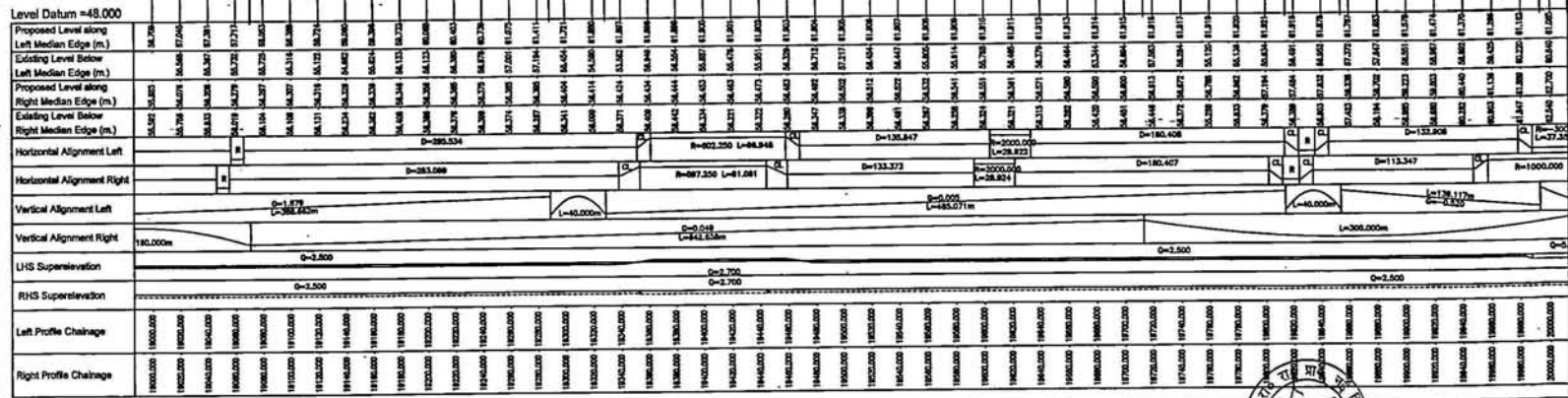
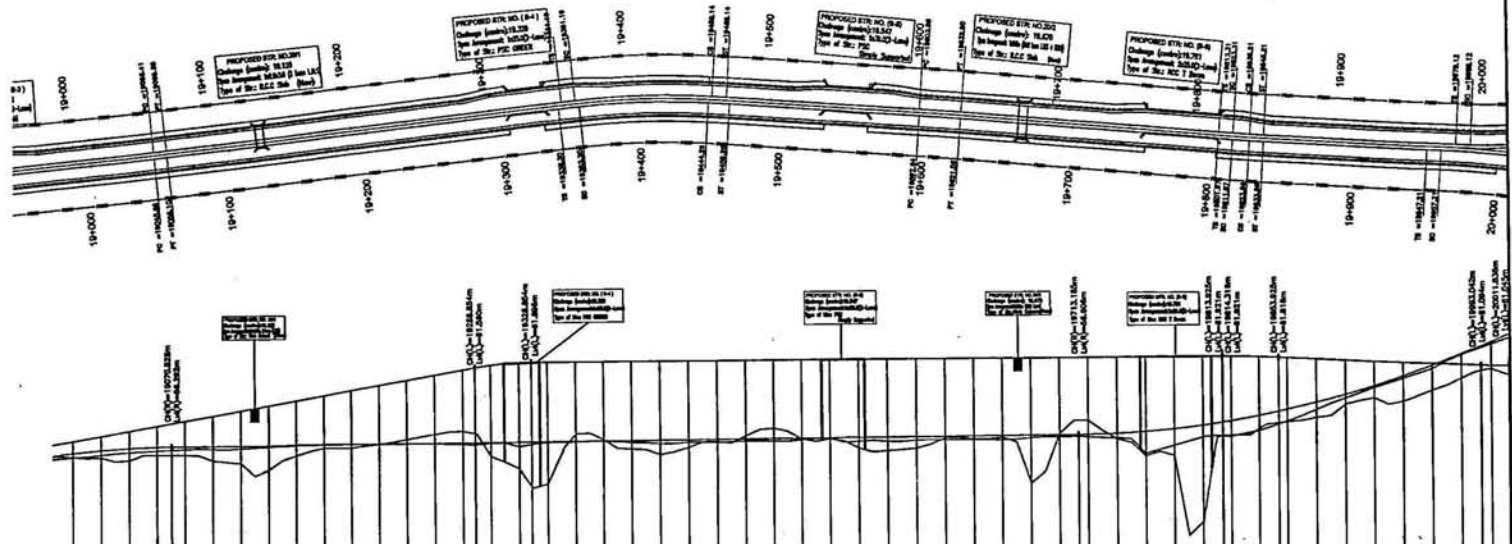
Scale: HOR:-1:2000 VER:-1:200

Date: JUNE-2009

Appd: BRG.C.D.PURI

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 28/4, MOHAN CO-OPERATIVE IND. ESTATE
 ANTRA ROAD
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 Tel: (011) 41679340 - 49
 Fax: (011) 41679350
 E-mail: swindia@gmail.com





REVISIONS

| No. | Description | Date |
|-----|-------------------------|------|
| 1 | As per approved Plan | |
| 2 | As per approved Profile | |
| 3 | As per approved EIR | |
| 4 | As per approved EIR | |
| 5 | As per approved EIR | |
| 6 | As per approved EIR | |
| 7 | As per approved EIR | |
| 8 | As per approved EIR | |
| 9 | As per approved EIR | |
| 10 | As per approved EIR | |

Project Title
 Consulting services for preparation of detailed project report for
 Rehabilitation and upgrading of existing 3-lane road to 4-lane divided
 Category and upgrading of Jammu - Udhampur section of JH-1 A
 (On 0.00 to Km 66.00) including Jammu (7+00) to Udhampur and Udhampur
 Bypass Phase - III Programme of North - South - corridor Project,
 Contract Package C-0918

Client
**NATIONAL HIGHWAYS
 AUTHORITY OF INDIA**
 (Ministry of Shipping, Road Transport & Highways)

Drawing Title: PLAN & PROFILE
 Km. 19+000 to 20+000 (JAMMU BYPASS)

Drawing No.: SW/1019/ROADS/PP-05

Scale: HOR:- 1:2000 VER:- 1:200

Drn: K.L.N **Dgn:** K.PRASAD **Appd:** BRG.C.D.PURU **Date:** JUNE-2009

Client: Wilson India Private Limited
 Mohan Co-Operative Ind. Estate
 NEW DELHI - 110044
 (011) 41679340 - 49
 (011) 41679350
 E-mail: wivindia@rediffmail.com



1. Existing Road Profile
 2. Proposed Road Profile
 3. Proposed Road Profile
 4. Proposed Road Profile
 5. Proposed Road Profile
 6. Proposed Road Profile
 7. Proposed Road Profile
 8. Proposed Road Profile
 9. Proposed Road Profile
 10. Proposed Road Profile

Project Title
 Construction of 2-lane 2-way road with 2.5m wide shoulders and 10m wide median for 1.5 km from Km 20+000 to Km 20+300 on NH-100, Jammu & Kashmir.

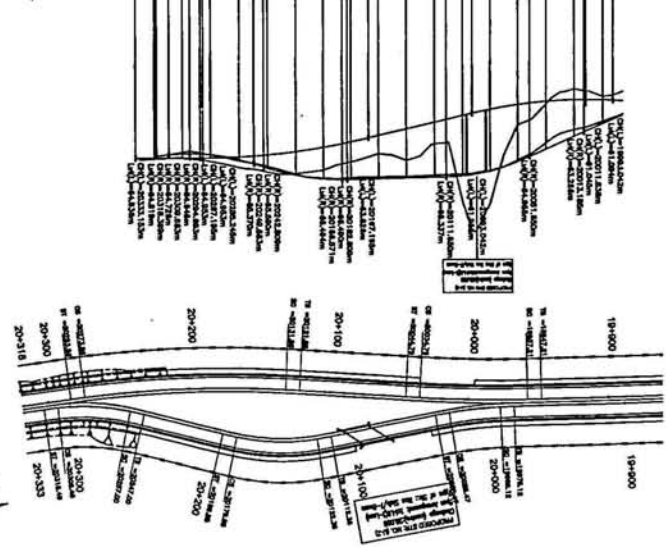
Client
 NATIONAL HIGHWAYS AUTHORITY OF INDIA
 (Ministry of Shipping, Road Transport & Highways)

Drawing Title
 PLAN & PROFILE
 Km 20+000 to 20+300 (Jammu & Kashmir)

Drawing No.: SW/1018/ROADS/PP-06
 Scale: 1:1000
 Date: 15/06/2008



| Right Profile Challenge | Left Profile Challenge | RHS Super-elevation | LHS Super-elevation | Vertical Alignment Right | Horizontal Alignment Right | Horizontal Alignment Left | Right Level Edge (m) | Existing Level Edge (m) | Proposed Level Edge (m) | Existing Level Edge (m) | Proposed Level Edge (m) | Left Level Edge (m) | Existing Level Edge (m) | Proposed Level Edge (m) | Level Datum = 52.000 |
|-------------------------|------------------------|---------------------|---------------------|--------------------------|----------------------------|---------------------------|----------------------|-------------------------|-------------------------|-------------------------|-------------------------|---------------------|-------------------------|-------------------------|----------------------|
| 20000.00 | 20000.00 | | | | | | 42.90 | 42.90 | 42.90 | 42.90 | 42.90 | 42.90 | 42.90 | 42.90 | |
| 20010.00 | 20010.00 | | | | | | 42.80 | 42.80 | 42.80 | 42.80 | 42.80 | 42.80 | 42.80 | 42.80 | |
| 20020.00 | 20020.00 | | | | | | 42.70 | 42.70 | 42.70 | 42.70 | 42.70 | 42.70 | 42.70 | 42.70 | |
| 20030.00 | 20030.00 | | | | | | 42.60 | 42.60 | 42.60 | 42.60 | 42.60 | 42.60 | 42.60 | 42.60 | |
| 20040.00 | 20040.00 | | | | | | 42.50 | 42.50 | 42.50 | 42.50 | 42.50 | 42.50 | 42.50 | 42.50 | |
| 20050.00 | 20050.00 | | | | | | 42.40 | 42.40 | 42.40 | 42.40 | 42.40 | 42.40 | 42.40 | 42.40 | |
| 20060.00 | 20060.00 | | | | | | 42.30 | 42.30 | 42.30 | 42.30 | 42.30 | 42.30 | 42.30 | 42.30 | |
| 20070.00 | 20070.00 | | | | | | 42.20 | 42.20 | 42.20 | 42.20 | 42.20 | 42.20 | 42.20 | 42.20 | |
| 20080.00 | 20080.00 | | | | | | 42.10 | 42.10 | 42.10 | 42.10 | 42.10 | 42.10 | 42.10 | 42.10 | |
| 20090.00 | 20090.00 | | | | | | 42.00 | 42.00 | 42.00 | 42.00 | 42.00 | 42.00 | 42.00 | 42.00 | |
| 20100.00 | 20100.00 | | | | | | 41.90 | 41.90 | 41.90 | 41.90 | 41.90 | 41.90 | 41.90 | 41.90 | |
| 20110.00 | 20110.00 | | | | | | 41.80 | 41.80 | 41.80 | 41.80 | 41.80 | 41.80 | 41.80 | 41.80 | |
| 20120.00 | 20120.00 | | | | | | 41.70 | 41.70 | 41.70 | 41.70 | 41.70 | 41.70 | 41.70 | 41.70 | |
| 20130.00 | 20130.00 | | | | | | 41.60 | 41.60 | 41.60 | 41.60 | 41.60 | 41.60 | 41.60 | 41.60 | |
| 20140.00 | 20140.00 | | | | | | 41.50 | 41.50 | 41.50 | 41.50 | 41.50 | 41.50 | 41.50 | 41.50 | |
| 20150.00 | 20150.00 | | | | | | 41.40 | 41.40 | 41.40 | 41.40 | 41.40 | 41.40 | 41.40 | 41.40 | |
| 20160.00 | 20160.00 | | | | | | 41.30 | 41.30 | 41.30 | 41.30 | 41.30 | 41.30 | 41.30 | 41.30 | |
| 20170.00 | 20170.00 | | | | | | 41.20 | 41.20 | 41.20 | 41.20 | 41.20 | 41.20 | 41.20 | 41.20 | |
| 20180.00 | 20180.00 | | | | | | 41.10 | 41.10 | 41.10 | 41.10 | 41.10 | 41.10 | 41.10 | 41.10 | |
| 20190.00 | 20190.00 | | | | | | 41.00 | 41.00 | 41.00 | 41.00 | 41.00 | 41.00 | 41.00 | 41.00 | |
| 20200.00 | 20200.00 | | | | | | 40.90 | 40.90 | 40.90 | 40.90 | 40.90 | 40.90 | 40.90 | 40.90 | |
| 20210.00 | 20210.00 | | | | | | 40.80 | 40.80 | 40.80 | 40.80 | 40.80 | 40.80 | 40.80 | 40.80 | |
| 20220.00 | 20220.00 | | | | | | 40.70 | 40.70 | 40.70 | 40.70 | 40.70 | 40.70 | 40.70 | 40.70 | |
| 20230.00 | 20230.00 | | | | | | 40.60 | 40.60 | 40.60 | 40.60 | 40.60 | 40.60 | 40.60 | 40.60 | |
| 20240.00 | 20240.00 | | | | | | 40.50 | 40.50 | 40.50 | 40.50 | 40.50 | 40.50 | 40.50 | 40.50 | |
| 20250.00 | 20250.00 | | | | | | 40.40 | 40.40 | 40.40 | 40.40 | 40.40 | 40.40 | 40.40 | 40.40 | |
| 20260.00 | 20260.00 | | | | | | 40.30 | 40.30 | 40.30 | 40.30 | 40.30 | 40.30 | 40.30 | 40.30 | |
| 20270.00 | 20270.00 | | | | | | 40.20 | 40.20 | 40.20 | 40.20 | 40.20 | 40.20 | 40.20 | 40.20 | |
| 20280.00 | 20280.00 | | | | | | 40.10 | 40.10 | 40.10 | 40.10 | 40.10 | 40.10 | 40.10 | 40.10 | |
| 20290.00 | 20290.00 | | | | | | 40.00 | 40.00 | 40.00 | 40.00 | 40.00 | 40.00 | 40.00 | 40.00 | |
| 20300.00 | 20300.00 | | | | | | 39.90 | 39.90 | 39.90 | 39.90 | 39.90 | 39.90 | 39.90 | 39.90 | |

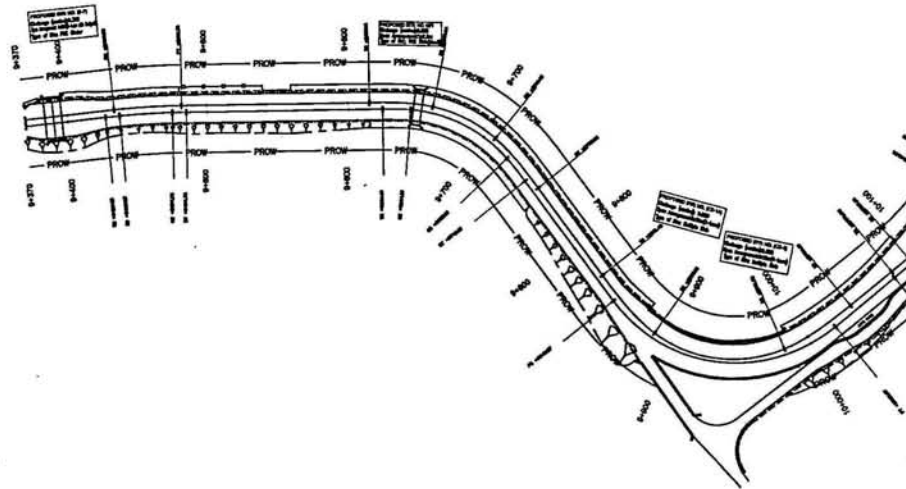


Udhampur ←

→ Jammu

Jammu ←

Udhampur →



Level Datum = 199.000

| |
|---|
| Proposed Level along Left Median Edge (m.) |
| Existing Level Below Left Median Edge (m.) |
| Proposed Level along Right Median Edge (m.) |
| Existing Level Below Right Median Edge (m.) |
| Horizontal Alignment Left |
| Horizontal Alignment Right |
| Vertical Alignment Left |
| Vertical Alignment Right |
| LHS Superelevation |
| RHS Superelevation |
| Left Profile Chainage |
| Right Profile Chainage |

| REVISIONS | |
|-----------------------------|--|
| 1. For Proposed Four Lanes | |
| 2. For Proposed Four Lanes | |
| 3. For Proposed Four Lanes | |
| 4. For Proposed Four Lanes | |
| 5. For Proposed Four Lanes | |
| 6. For Proposed Four Lanes | |
| 7. For Proposed Four Lanes | |
| 8. For Proposed Four Lanes | |
| 9. For Proposed Four Lanes | |
| 10. For Proposed Four Lanes | |

Project Title
 Consultancy services for preparation of detailed project report for
 Rehabilitation and upgrading of existing 3-lane road to 4-lane divided
 highway on alignment of Jammu - Udhampur section at NH-1A
 Km 9.00 to Km 10.00 including Jammu (NH) Upgrade and Udhampur
 Bypass Phase - II Programme of North - South corridor Project,
 Contract Package C-0214

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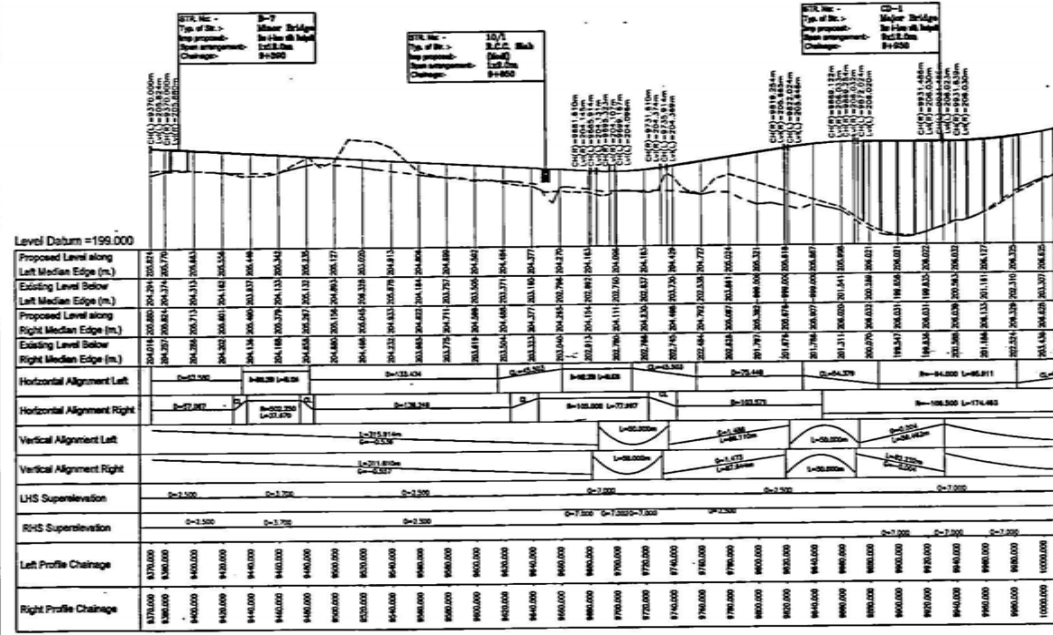
Client
NATIONAL HIGHWAYS
AUTHORITY OF INDIA
 (Ministry of Shipping, Road Transport & Highways)

Drawing Title: PLAN & PROFILE
 Km. 9+370 to 10+000 (MAIN ROAD)
Drawing No.: SW/M019/ROADS/PP-07
Scale: 1:2000
Date: JUNE-2009
Appd: BRIG.C.D.PURI
Dgn: K.PRASAD
Drn: K.L.N.



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 Tele: (011) 41678340 - 49
 Fax: (011) 41678350
 E-mail: scw@scw.com





STL No. - B-7
Type of Str. - Minor Bridge
Span arrangement - By 10m & 10m
Clearance - 5+300

STL No. - 10/A
Type of Str. - R.C.C. Arch
Span arrangement - By 20m
Clearance - 5+500

STL No. - CD-1
Type of Str. - Major Bridge
Span arrangement - By 10m & 10m
Clearance - 5+500

1. Title 2. of contract
2. Proposed and existing level
3. Proposed and existing level
4. Proposed and existing level
5. Proposed and existing level
6. Proposed and existing level
7. Proposed and existing level
8. Proposed and existing level
9. Proposed and existing level
10. Proposed and existing level

Project Title
Consistency services for preparation of detailed project report for
rehabilitation and upgrading of existing 3-lane road to 4-lane divided
concrete configuration of Jammu - Udhampur section of NH-44 A
Km 9+370 to Km 10+000 including Jammu-Udhampur and Udhampur-
Rajouri Phase - II Programme of North - South corridor Project
Contract Package C-2013

Client
**NATIONAL HIGHWAYS
AUTHORITY OF INDIA**
(Ministry of Shipping, Road Transport & Highways)

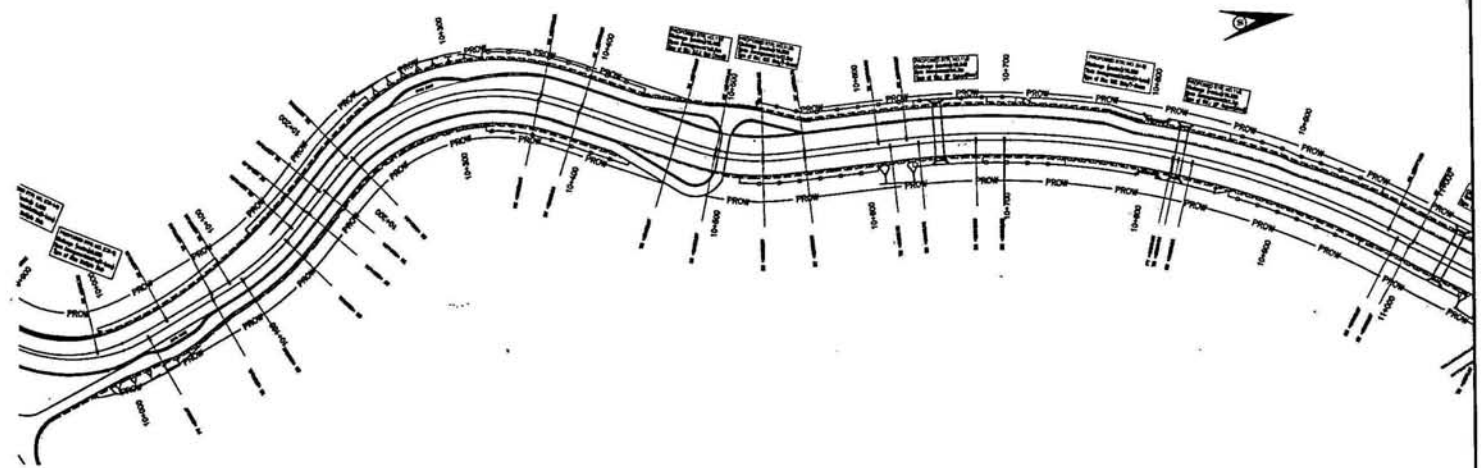
Drawing Title: **PLAN & PROFILE**
Km. 9+370 to 10+000 (MAIN ROAD)
Drawing No.: **SW/1019/ROADS/PP-07**
Scale: -
Dgn. K.L.N.
Appd. K.PRASAD
Date: JUNE-2009

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NEW DELHI - 110044
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Fax: (011) 41679350
E-mail: swi@scwipl.com



← Jammu
46

Udhampur →



Level Datum = 202.000

| |
|---|
| Proposed Level along Left Median Edge (m.) |
| Existing Level Below Left Median Edge (m.) |
| Proposed Level along Right Median Edge (m.) |
| Existing Level Below Right Median Edge (m.) |
| Horizontal Alignment Left |
| Horizontal Alignment Right |
| Vertical Alignment Left |
| Vertical Alignment Right |
| LHS Super-elevation |
| RHS Super-elevation |
| Left Profile Chaisage |
| Right Profile Chaisage |

| LEGEND | FOR PROPOSED ROAD LAYOUT |
|----------------------------------|----------------------------------|
| 1. Right of Way | 1. Right of Way |
| 2. Proposed Road | 2. Proposed Road |
| 3. Proposed Left Side Foot Path | 3. Proposed Left Side Foot Path |
| 4. Proposed Right Side Foot Path | 4. Proposed Right Side Foot Path |
| 5. Proposed Level | 5. Proposed Level |
| 6. Existing Level | 6. Existing Level |
| 7. Existing Right Side Foot Path | 7. Existing Right Side Foot Path |
| 8. Existing Left Side Foot Path | 8. Existing Left Side Foot Path |
| 9. Right of Way | 9. Right of Way |

Project Title
 Consultancy services for preparation of detailed project report for Rehabilitation and upgrading of existing 2-lane road to 4-lane divided carriageway along Jammu - Udhampur section of NH-1A
 Km 10.00 to Km 10.70 including Nagrota (P.O.) Nagrota and Udhampur Bypass Phase - III Programme of North - South - Corridor Project, Contract Package C-02/14

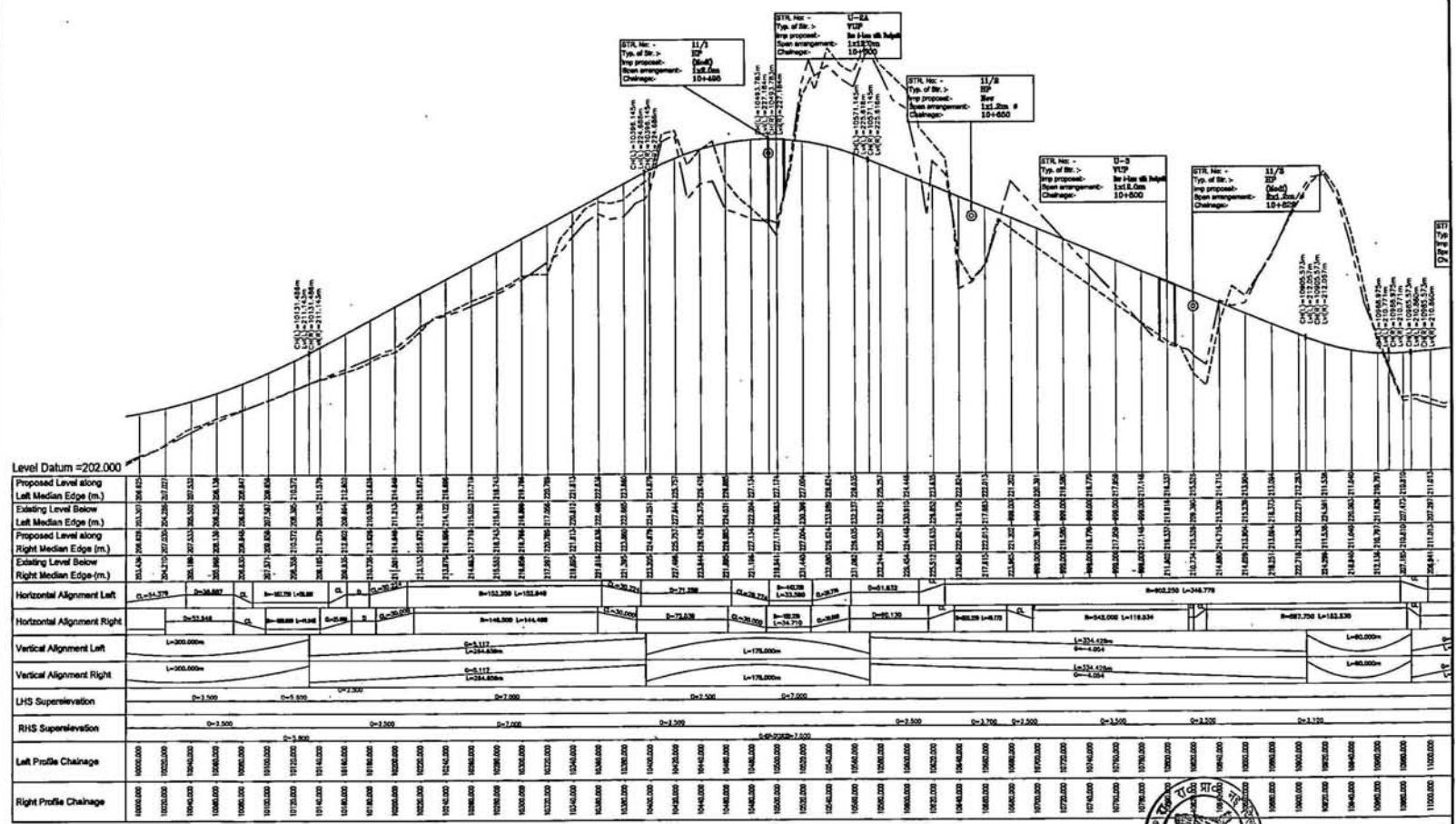
Site - Located in the proximity of Nagrota (P.O.) Nagrota and Udhampur Bypass Phase - III Programme of North - South - Corridor Project, Contract Package C-02/14
Scale - 1:2000
Date - JUNE-2009

Client
NATIONAL HIGHWAYS AUTHORITY OF INDIA
 (Ministry of Shipping, Road Transport & Highways)

Drawing Title: PLAN & PROFILE
 Km. 10+000 to 11+000 (NAGROTA BYPASS)
Drawing No.: SW/1019/ROADS/PP - 08
Scale: HOR-1:2000 VER-1:200
Appd: BIRG.C.D.PURI
Date: JUNE-2009

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 NEW DELHI - 110044
 Tele: (011) 41679340 - 49
 Fax: (011) 41679350
 E-mail: swindia@swi.com





Scale: 1:1000

Vertical Scale: 1:100

Horizontal Scale: 1:1000

Project Title
 Consultancy services for preparation of detailed project report for Rehabilitation and upgrading of existing 2-lane road to 4-lane divided carriageway configuration of Jammu - Udhampur section of NH-1 A (Km. 0.00 to Km. 66.00) including Jammu(Ur) Nagar and Udhampur Bypass Phase - III Programme of North - South - corridor Project, Contract Package C-2013

Client
 NATIONAL HIGHWAYS AUTHORITY OF INDIA
 (Ministry of Shipping, Road Transport & Highways)

Drawing Title: PLAN & PROFILE
 Km. 10+000 to 11+000 (NAGRA) - Bypass Phase - III

Drawing No.: SW/1019/ROADS/PP-08

Scale: - HDR-1:2000 VDR-1:200

Date: JUNE-2009

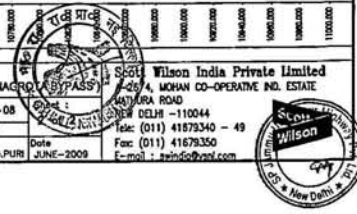
Dgn.: K.PRASAD
Appd.: BRIG.C.D.PURI
Date: JUNE-2009

Client: S661 Wilson India Private Limited
 4/4, MOHAN CO-OPERATIVE IND. ESTATE
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 Tel: (011) 41879340 - 49
 Fax: (011) 41879350
 E-mail: arjun@wilsonindia.com

Scale: 1:1000

Vertical Scale: 1:100

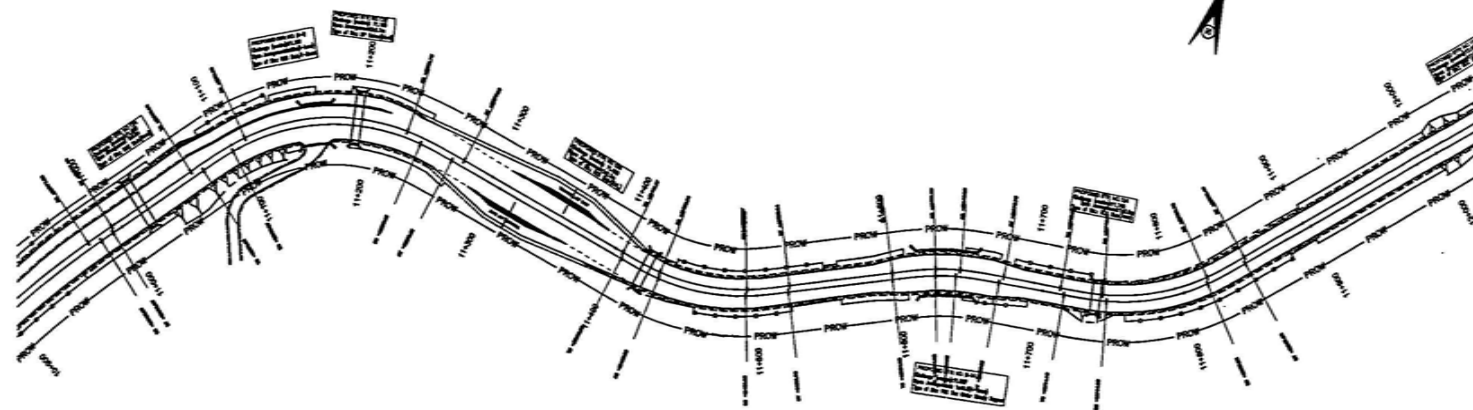
Horizontal Scale: 1:1000



Jammu

4B

Udhampur



Level Datum = 204.000

| |
|---|
| Proposed Level along Left Median Edge (m.) |
| Existing Level Below Left Median Edge (m.) |
| Proposed Level along Right Median Edge (m.) |
| Existing Level Below Right Median Edge (m.) |
| Horizontal Alignment Left |
| Horizontal Alignment Right |
| Vertical Alignment Left |
| Vertical Alignment Right |
| LHS Superelevation |
| RHS Superelevation |
| Left Profile Chainage |
| Right Profile Chainage |

| REVISION | |
|----------|-------------|
| No. | Description |
| | |
| | |
| | |
| | |
| | |
| | |
| | |
| | |
| | |
| | |

Project Title
 Consultancy services for preparation of detailed project report for rehabilitation and upgrading of existing 2-lane road to 4-lane divided carriageway configuration of Jammu - Udhampur section of NH-1A
 Km 5.00 to Km 14.00 including Jammu/Ch/ Nagrota and Udhampur Bypass Phase - II Programme of North - South - corridor Project
 Contract Package C-2018

Site Drawing
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Client
NATIONAL HIGHWAYS AUTHORITY OF INDIA
 (Ministry of Shipping, Road Transport & Highways)

Drawing Title
 PLAN & PROFILE
 Km. 11+000 to 12+000 (NAGROTA BYPASS)

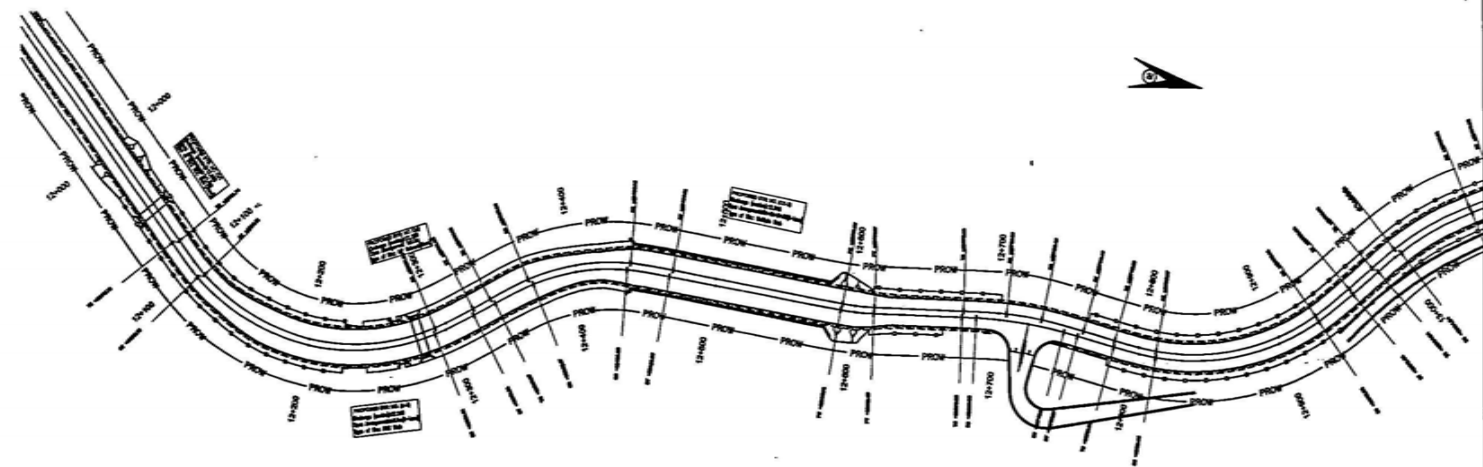
Drawing No.: SW/1019/ROADS/PP-06
Scale: HOR-1:2000 VER-1:200
Dim: K.L.N
Dgn: K.PRASAD
Appd: BING.C.D.PURJ
Date: JUNE-2008

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 Tele: (011) 41679340 - 49
 Fax: (011) 41679350
 E-mail: s.wi@scwip.com



Jammu ← So

Udhampur →



| |
|---|
| Level Datum =+206.000 |
| Proposed Level along Left Median Edge (m.) |
| Existing Level Below Left Median Edge (m.) |
| Proposed Level along Right Median Edge (m.) |
| Existing Level Below Right Median Edge (m.) |
| Horizontal Alignment Left |
| Horizontal Alignment Right |
| Vertical Alignment Left |
| Vertical Alignment Right |
| LHS Superelevation |
| RHS Superelevation |
| Left Profile Chainage |
| Right Profile Chainage |



| Legend | |
|--|--|
| Proposed Road Limits | Existing Road Limits |
| Proposed Level | Existing Level |
| Proposed Level along Left Median Edge | Existing Level along Left Median Edge |
| Proposed Level along Right Median Edge | Existing Level along Right Median Edge |
| Proposed Vertical Alignment | Existing Vertical Alignment |
| Proposed LHS Superelevation | Existing LHS Superelevation |
| Proposed RHS Superelevation | Existing RHS Superelevation |
| Proposed Profile Chainage | Existing Profile Chainage |

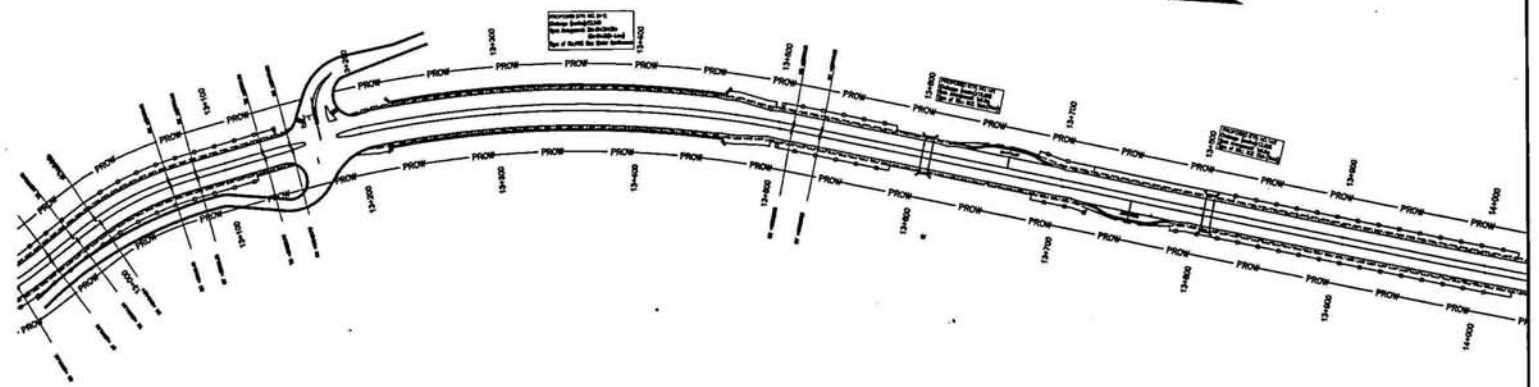
Project Title
 Consultancy services for preparation of detailed project report for rehabilitation and upgrading of existing 3-lane road to 4-lane divided carriageway and provision of Access - Collector system of NH 151 A (Km 8.50 to Km 46.95) including Interchange/Overbridges and Underpasses
 Project Phase - II Progression of Work - South - westward Project
 Contract Package - C0213

Client
 NATIONAL HIGHWAYS AUTHORITY OF INDIA
 (Ministry of Shipping, Road Transport & Highways)

Drawing Title: PLAN & PROFILE
 Km. 12+000 to 13+000 (NAGROTA BYPASS)

Drawing No.: SW/1019/ROADS/PP-10
Scale: HOR-1:2000 VER-1:200
Sheet: 1 of 2
Date: JUNE-2009
 Dm: K.L.N Dgn: K.PRASAD Appr: SSG.C.D.PUR

Scott Wilson India Private Limited
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 NEW DELHI - 110044
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 Fax: (011) 41679350
 E-mail: info@swindia.com



Level Datum =214.000

| |
|---|
| Proposed Level along Left Median Edge (m.) |
| Existing Level Below Left Median Edge (m.) |
| Proposed Level along Right Median Edge (m.) |
| Existing Level Below Right Median Edge (m.) |
| Horizontal Alignment Left |
| Horizontal Alignment Right |
| Vertical Alignment Left |
| Vertical Alignment Right |
| LHS Super-elevation |
| RHS Super-elevation |
| Left Profile Chainage |
| Right Profile Chainage |

| REVISION | |
|----------|-----------------------------|
| No. | Description |
| 1 | As per approved Plan Layout |
| 2 | As per approved Plan Layout |
| 3 | As per approved Plan Layout |
| 4 | As per approved Plan Layout |
| 5 | As per approved Plan Layout |
| 6 | As per approved Plan Layout |
| 7 | As per approved Plan Layout |
| 8 | As per approved Plan Layout |
| 9 | As per approved Plan Layout |
| 10 | As per approved Plan Layout |

Project Title
 Consultancy services for preparation of detailed project report for Rehabilitation and upgrading of existing 2 lane road to 4 lane divided carriageway configuration of Jammu - Udhampur section of NH-1A
 Km 0.00 to km 66.00 including Jammu/PrD/Nagrota and Udhampur Bypass Phase - III Programme of North - South - corridor Project.
 Contract Package C-0711

Notes:
 1. The drawings to the property of SCOTT WILSON INDIA PRIVATE LIMITED and shall not be used for any other purpose without the written consent of the Engineer in Charge. The drawings shall not be altered in any way without the written consent of the Engineer in Charge.
 2. The drawings shall be used for the purpose of the project only and shall not be used for any other purpose.
 3. The drawings shall be used for the purpose of the project only and shall not be used for any other purpose.
 4. The drawings shall be used for the purpose of the project only and shall not be used for any other purpose.

Client
NATIONAL HIGHWAYS AUTHORITY OF INDIA
 (Ministry of Shipping, Road Transport & Highways)

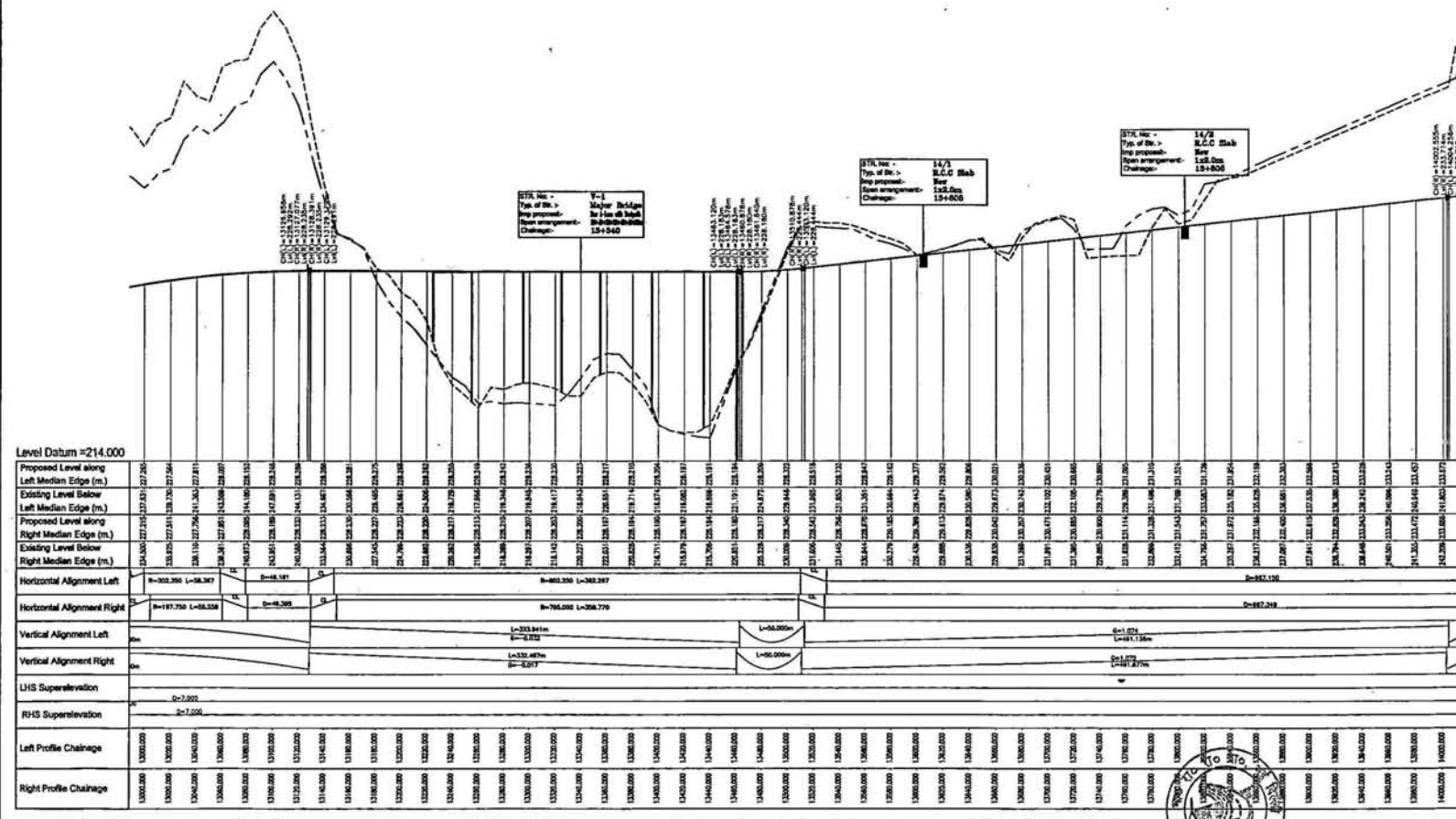
Drawing Title: PLAN & PROFILE
 Km. 13+000 to 14+000 (NAGROTA BYPASS)
Drawing No.: SW/1018/ROADS/PP - 11
Scale: HOR=1:2000 VER=1:200
Sheet: 1 of 2
Drn: K.L.N
Dgn: K.PRASAD
Appd: BRIG.C.D.PURI
Date: JUNE-2009

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Jammu
S3

Udhampur



| REV. | DATE | BY | DESCRIPTION |
|------|------|----|----------------|
| 1 | | | As per drawing |
| 2 | | | As per drawing |
| 3 | | | As per drawing |
| 4 | | | As per drawing |
| 5 | | | As per drawing |
| 6 | | | As per drawing |
| 7 | | | As per drawing |
| 8 | | | As per drawing |
| 9 | | | As per drawing |
| 10 | | | As per drawing |

Project Title
 Consultancy services for preparation of detailed project report for Rehabilitation and upgrading of existing 2-lane road to 4-lane divided. Contiguity configuration of Jammu - Udhampur section of NH-1 A (On 0.00 to Km 64.00) including Jammu/PTQ Nagrota and Udhampur Bypass Phase - III Programme of North - South corridor Project, Contract Package C-0211

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Client
NATIONAL HIGHWAYS AUTHORITY OF INDIA
 (Ministry of Shipping, Road Transport & Highways)

Drawing Title: **PLAN & PROFILE**
 Km. 13+000 to 14+000 (NAGROTA BYPASS)
 Drawing No.: **SW/1019/ROADS/PP-11**
 Scale: -- HOR--1:2000 VER--1:200
 Dn: K.L.N. Appd: K.PRASAD Date: JUNE-2009

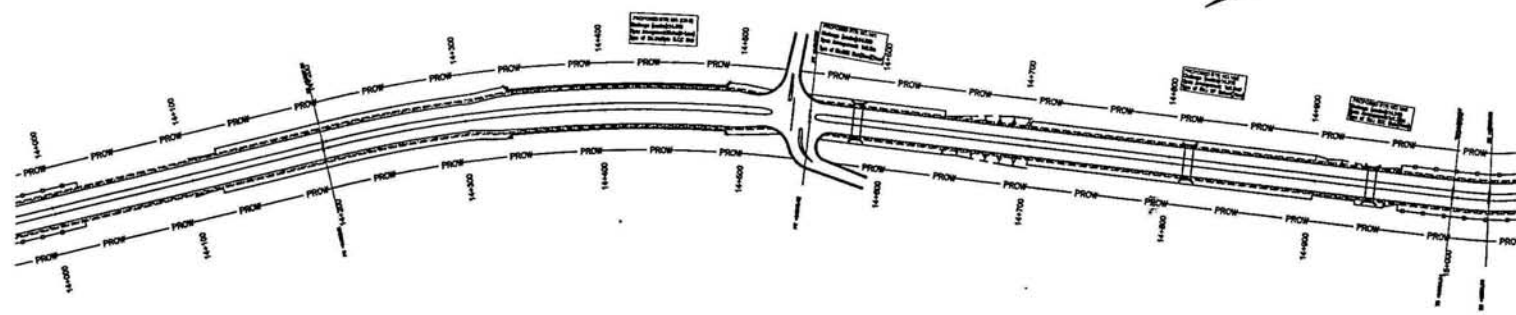
Scott Wilson India Private Limited
 22/4, MOHAN CO-OPERATIVE IND. ESTATE
 WAZIRPURA ROAD
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 Tel: (011) 41679340 - 49
 Fax: (011) 41679350
 E-mail: s.windia@gmail.com



← Jammu

Udhampur →

S4



Level Datum = 226.000

| |
|---|
| Proposed Level along Left Median Edge (m.) |
| Existing Level Below Left Median Edge (m.) |
| Proposed Level along Right Median Edge (m.) |
| Existing Level Below Right Median Edge (m.) |
| Horizontal Alignment Left |
| Horizontal Alignment Right |
| Vertical Alignment Left |
| Vertical Alignment Right |
| LHS Superelevation |
| RHS Superelevation |
| Left Profile Chainage |
| Right Profile Chainage |

| LEGEND: | |
|--|--|
| For Proposed Four Lanes | |
| 1. Existing 2-lane | |
| 2. Proposed 4-lane | |
| 3. Proposed Left side Left Median Edge | |
| 4. Proposed Right side Right Median Edge | |
| 5. Proposed 2-lane | |
| 6. Existing 2-lane | |
| 7. Existing Left side Left Median Edge | |
| 8. Existing Right side Right Median Edge | |
| 9. Existing 2-lane | |
| 10. Existing 4-lane | |
| 11. Existing 2-lane | |
| 12. Existing 4-lane | |

Project Title
 Consultancy services for preparation of detailed project report for Rehabilitation and upgrading of existing 2-lane road to 4-lane divided carriageway configurations of Jammu - Udhampur section of JH-1 A (Km 0.00 to Km 66.00) including Jammu/Chowk/Meghwal and Udhampur Bypass Phase - III Programme of North-South corridor Project, Contract Package C-2018

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Scale: 1. Vertical scale 1:1 (Level and Elevation) (2:4.00)
 2. The horizontal scale 1:1 (to be used for layout and alignment)
 3. The drawing shall be used for reference only and shall not be used for any other purpose without the written permission of the Director, SCRRV.

Client
NATIONAL HIGHWAYS AUTHORITY OF INDIA
 (Ministry of Shipping, Road Transport & Highways)

Drawing Title: PLAN & PROFILE
 Km. 14+000 to 15+000

Drawing No.: SW/1019/ROADS/PP-12

Scale: 1:—
 HOR—1:2000 VER—1:300

Date: JUNE-2009

Appd: K.PRASAD
BRG.C.D.PUR

Drawn: K.L.N.

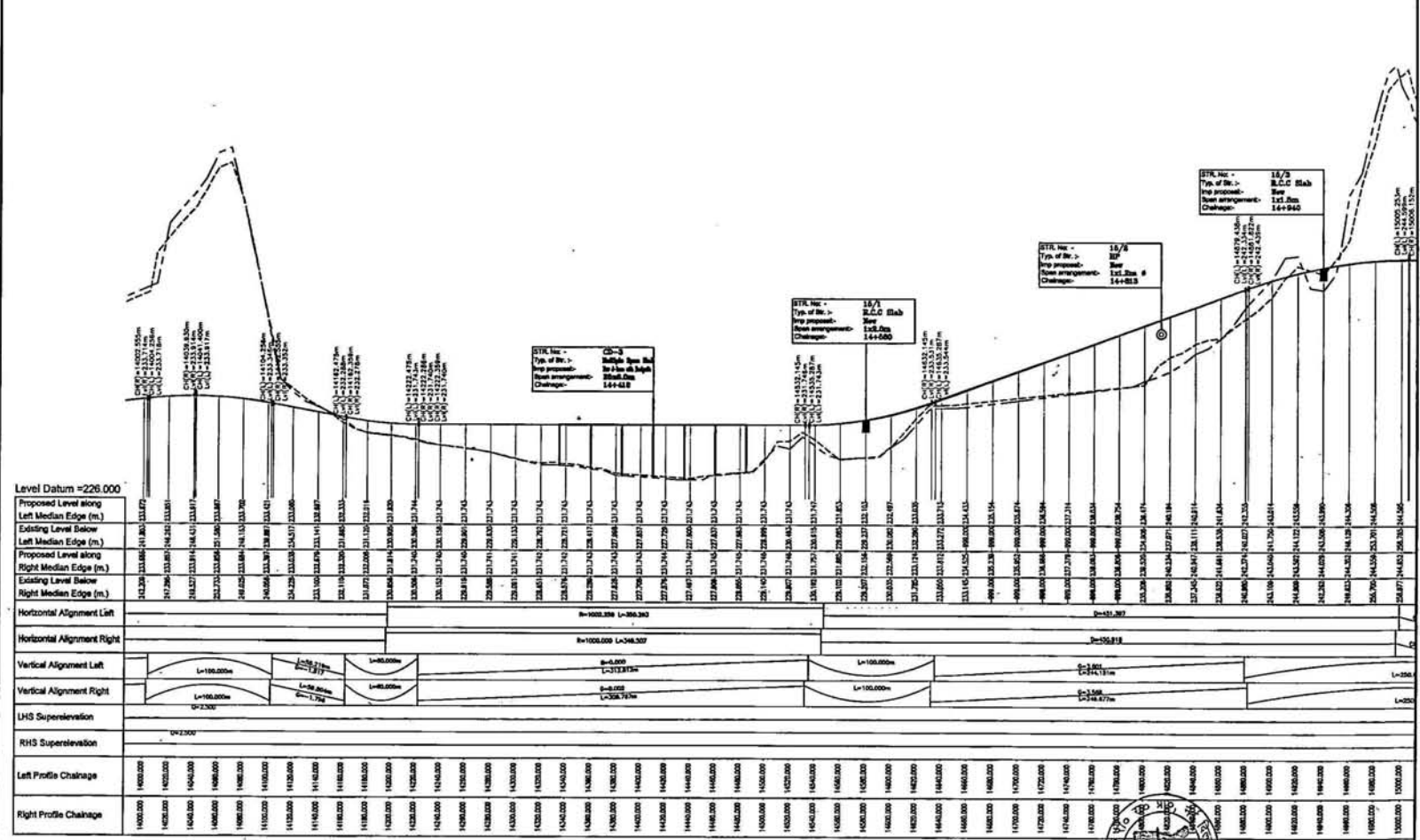
Client: **Scott Wilson India Private Limited**
 A-236/4, MOHAN CO-OPERATIVE IND. ESTATE
 GURUKRPA ROAD
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 E-mail: info@swi.com

Wilson



Jammu
SS

Udhampur



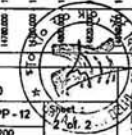
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|--------------------------|--------------------------|--------------------------|--------------------------|--------------------------|--------------------------|--------------------------|--------------------------|--------------------------|--------------------------|--------------------------|--------------------------|--------------------------|--------------------------|--------------------------|--------------------------|--------------------------|--------------------------|--------------------------|--------------------------|
| <input type="checkbox"/> | <input type="checkbox"/> | <input type="checkbox"/> | <input type="checkbox"/> | <input type="checkbox"/> | <input type="checkbox"/> | <input type="checkbox"/> | <input type="checkbox"/> | <input type="checkbox"/> | <input type="checkbox"/> | <input type="checkbox"/> | <input type="checkbox"/> | <input type="checkbox"/> | <input type="checkbox"/> | <input type="checkbox"/> | <input type="checkbox"/> | <input type="checkbox"/> | <input type="checkbox"/> | <input type="checkbox"/> | <input type="checkbox"/> |

Project Title
 Consultancy services for preparation of detailed project report for Rehabilitation and upgrading of existing 3-lane road to 4-lane divided Cartageway and integration of Jammu - Udhampur section of NH-1A (Km 0.00 to Km 66.00) including lanes of 2.10 meters and 1.80 meters Bypass Phase - III Progression of North-South - outside Project, Contract Package C-02/18

Client
NATIONAL HIGHWAYS AUTHORITY OF INDIA
 (Ministry of Shipping, Road Transport & Highways)

Drawing Title: PLAN & PROFILE
 Km. 14+000 to 15+000
Drawing No.: SW/1019/ROADS/PP-12
Scale: 1:2000
Date: JUNE-2009

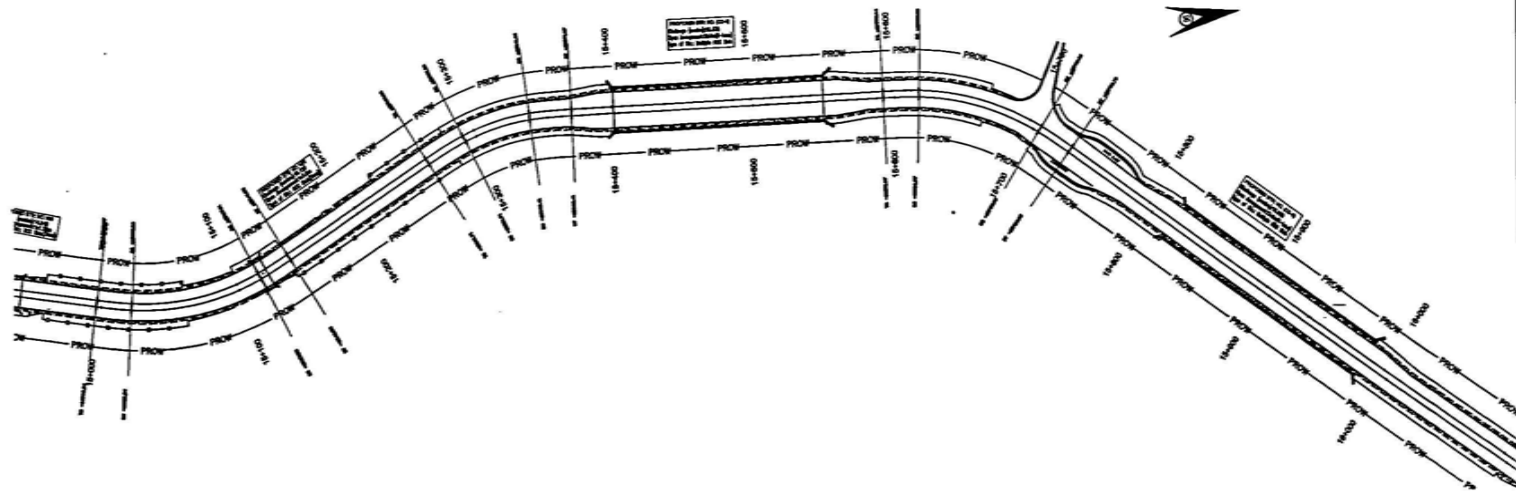
Client: **Scott Wilson India Private Limited**
 A-26/4, MOHAN CO-OPERATIVE RD. ESTATE
 WATHURA ROAD
 NEW DELHI - 110044
 Tel: (011) 41879343 - 49
 Fax: (011) 41879350
 E-mail: swi@swi.com



← Jammu

Udhampur →

S6



Level Datum = 232.000

| |
|---|
| Proposed Level along Left Median Edge (m.) |
| Existing Level Below Left Median Edge (m.) |
| Proposed Level along Right Median Edge (m.) |
| Existing Level Below Right Median Edge (m.) |
| Horizontal Alignment Left |
| Horizontal Alignment Right |
| Vertical Alignment Left |
| Vertical Alignment Right |
| LHS Superelevation |
| RHS Superelevation |
| Left Profile Chainage |
| Right Profile Chainage |

| | |
|---|----------|
| LEGEND: | |
| For Proposed Four Lanes | As Shown |
| 1. Proposed left side full width slip | ----- |
| 2. Proposed right side full width slip | ----- |
| 3. Proposed left side full width slip | ----- |
| 4. Proposed right side full width slip | ----- |
| 5. Proposed left side full width slip | ----- |
| 6. Proposed right side full width slip | ----- |
| 7. Proposed left side full width slip | ----- |
| 8. Proposed right side full width slip | ----- |
| 9. Proposed left side full width slip | ----- |
| 10. Proposed right side full width slip | ----- |

Project Title
 Consultancy services for preparation of detailed project report for Rehabilitation and upgrading of existing 2-lane road to 4-lane divided Category and upgrade of Jammu - Udhampur section of NH-1A (Km. 0+00 to Km. 64+00) including Jammu (NH) Bypass and Udhampur Bypass Phase - II Progress of North - South - corridor Project, Contract Package C-08/11

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Scale:
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 2. The drawing shall be in accordance with the scale indicated on the drawing.
 3. The drawing shall be in accordance with the scale indicated on the drawing.
 4. The drawing shall be in accordance with the scale indicated on the drawing.

Client
NATIONAL HIGHWAYS AUTHORITY OF INDIA
 (Ministry of Shipping, Road Transport & Highways)

Drawing Title: PLAN & PROFILE
 Km. 15+000 to 16+000

Drawing No.: SW/1019/ROADS/PP - 13

Scale: HOR - 1:2000 VER - 1:200

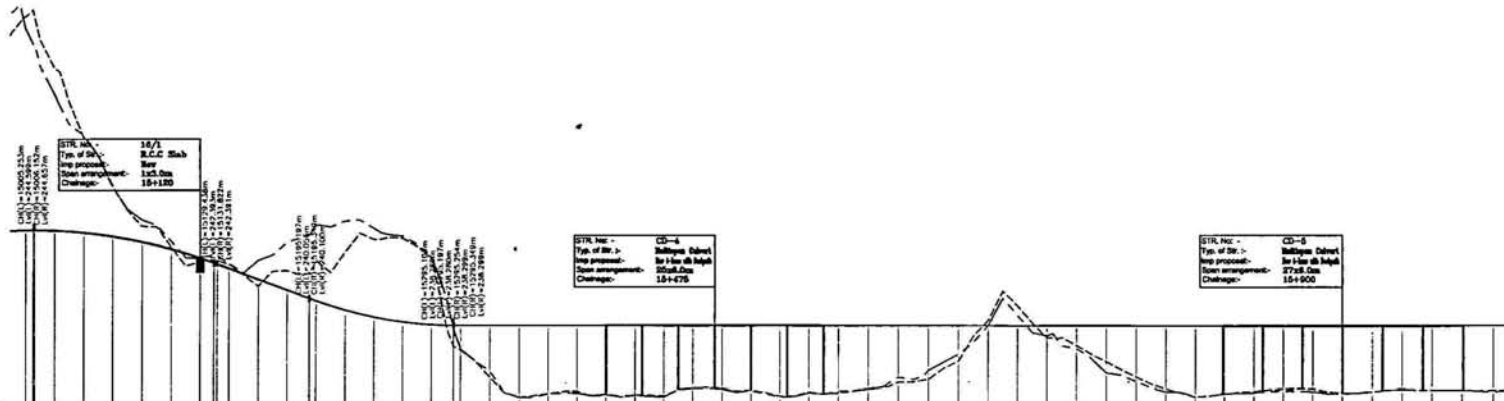
Sheet No.: 1 of 2

Date: JUNE-2009

Appd: ERIC.C.D.PURJ

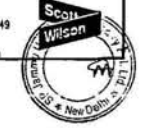
Scott Wilson India Private Limited
 A-25/4, MOPAN CO-OPERATIVE IND. ESTATE
 MATILDA ROAD
 NEW DELHI - 110044
 Tele: (011) 41679340 - 49
 Fax: (011) 41679350
 E-mail: swindia@swi.com

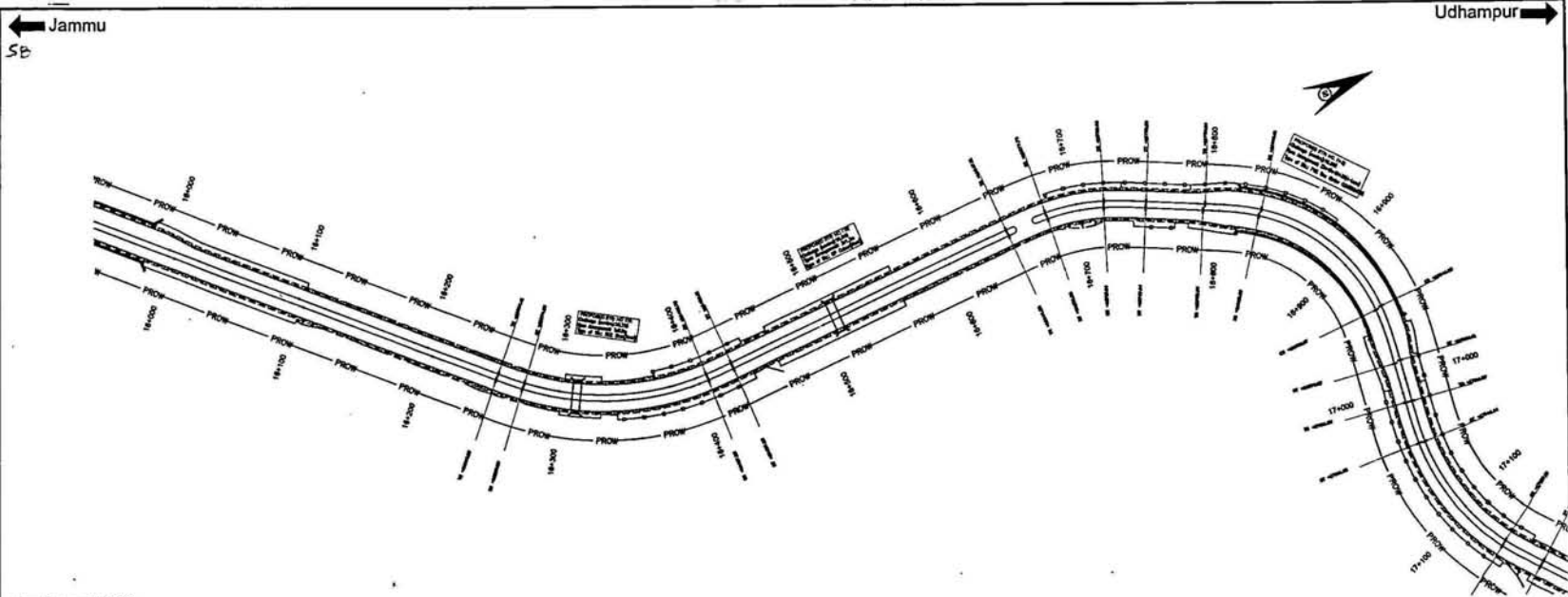




| | | | | | | | | |
|----------------------------|--|--------------------|--|--------------------|---|--------------------|---|--------------------|
| Level Datum =232.000 | Proposed Level along Left Median Edge (m.) | | Existing Level Below Left Median Edge (m.) | | Proposed Level along Right Median Edge (m.) | | Existing Level Below Right Median Edge (m.) | |
| Horizontal Alignment Left | R=117.500 L=43.214 | R=117.500 L=43.214 | R=117.500 L=43.214 | R=117.500 L=43.214 | R=117.500 L=43.214 | R=117.500 L=43.214 | R=117.500 L=43.214 | R=117.500 L=43.214 |
| Horizontal Alignment Right | R=172.250 L=86.219 | R=172.250 L=86.219 | R=172.250 L=86.219 | R=172.250 L=86.219 | R=172.250 L=86.219 | R=172.250 L=86.219 | R=172.250 L=86.219 | R=172.250 L=86.219 |
| Vertical Alignment Left | L=200.000m | L=200.000m | L=200.000m | L=200.000m | L=200.000m | L=200.000m | L=200.000m | L=200.000m |
| Vertical Alignment Right | L=200.000m | L=200.000m | L=200.000m | L=200.000m | L=200.000m | L=200.000m | L=200.000m | L=200.000m |
| LHS Super-elevation | 0.7500 | 0.7500 | 0.7500 | 0.7500 | 0.7500 | 0.7500 | 0.7500 | 0.7500 |
| RHS Super-elevation | 0.7500 | 0.7500 | 0.7500 | 0.7500 | 0.7500 | 0.7500 | 0.7500 | 0.7500 |
| Left Profile Chainage | 15000.000 | 15000.000 | 15000.000 | 15000.000 | 15000.000 | 15000.000 | 15000.000 | 15000.000 |
| Right Profile Chainage | 15000.000 | 15000.000 | 15000.000 | 15000.000 | 15000.000 | 15000.000 | 15000.000 | 15000.000 |

| | | | | |
|---|---|--|--|---|
| <p>REVISIONS</p> <p>No. Description of Revision</p> <p>1. Issued for tender</p> <p>2. Issued for tender</p> <p>3. Issued for tender</p> <p>4. Issued for tender</p> <p>5. Issued for tender</p> <p>6. Issued for tender</p> <p>7. Issued for tender</p> <p>8. Issued for tender</p> <p>9. Issued for tender</p> <p>10. Issued for tender</p> | <p>Project Title</p> <p>Consultancy services for preparation of detailed project report for Rehabilitation and upgrading of existing 3-lane road to 4-lane divided carriageway configuration of Jammu - Udhampur section of NH-1A.</p> <p>Over 0.00 to km 64.00 including Jammu(Near) Nagru and Udhampur Bypass Phase - III Programme of North-South corridor Project, Contract Package-C(2)13</p> | <p>Client</p> <p>NATIONAL HIGHWAYS AUTHORITY OF INDIA</p> <p>(Ministry of Shipping, Road Transport & Highways)</p> | <p>Drawing Title: PLAN & PROFILE</p> <p>Km. 15+000 to 16+000</p> <p>Drawing No.: SW/1019/ROADS/PP-13</p> <p>Scale: 1:— HOR:—1:2000 VER:—1:200</p> <p>Drn: K.L.N. Dgn: K.PRASAD Appd: BRIG.C.D.PURI Date: JUNE-2008</p> | <p>Scott Wilson India Private Limited</p> <p>"A-26/4" MOHAN CO-OPERATIVE IND. ESTATE</p> <p>MATRIJARA ROAD</p> <p>"NEW DELHI -110044</p> <p>Tel: (011) 41679340 - 49</p> <p>Fax: (011) 41679350</p> <p>E-mail: scwilson@rediffmail.com</p> |
|---|---|--|--|---|





Level Datum = 232.000

| |
|---|
| Proposed Level along Left Median Edge (m.) |
| Existing Level Below Left Median Edge (m.) |
| Proposed Level along Right Median Edge (m.) |
| Existing Level Below Right Median Edge (m.) |
| Horizontal Alignment Left |
| Horizontal Alignment Right |
| Vertical Alignment Left |
| Vertical Alignment Right |
| LHS Superlevation |
| RHS Superlevation |
| Left Profile Chainage |
| Right Profile Chainage |

| Legend | For Proposed Four Lanes |
|---|---|
| 1. Existing R.O.W. | 1. Existing R.O.W. |
| 2. Existing L.O.W. | 2. Existing L.O.W. |
| 3. Proposed L.O.W. along Left Median Edge | 3. Proposed L.O.W. along Left Median Edge |
| 4. Proposed L.O.W. along Right Median Edge | 4. Proposed L.O.W. along Right Median Edge |
| 5. Existing L.O.W. along Left Median Edge | 5. Existing L.O.W. along Left Median Edge |
| 6. Existing L.O.W. along Right Median Edge | 6. Existing L.O.W. along Right Median Edge |
| 7. Proposed L.O.W. along Left Median Edge | 7. Proposed L.O.W. along Left Median Edge |
| 8. Proposed L.O.W. along Right Median Edge | 8. Proposed L.O.W. along Right Median Edge |
| 9. Existing L.O.W. along Left Median Edge | 9. Existing L.O.W. along Left Median Edge |
| 10. Existing L.O.W. along Right Median Edge | 10. Existing L.O.W. along Right Median Edge |

Project Title
 Consultancy services for preparation of detailed project report for rehabilitation and upgrading of existing 2-lanes road to 4-lanes divided carriageway configuration of Jammu - Udhampur section of NH-1 A (Km 0.00 to Km 66.00) including Jammu(Para) Bypass and Udhampur Bypass Phase-III Programme of North-South corridor Project, Contract Package C-0118

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NOTE: 1. Engineer and his Licensed Surveyors (MIS) Certificate (17/200) 2. The drawings shall be in 1:1 scale and shall be in 1:1 scale. 3. The drawings shall be in 1:1 scale and shall be in 1:1 scale. 4. The drawings shall be in 1:1 scale and shall be in 1:1 scale. 5. The drawings shall be in 1:1 scale and shall be in 1:1 scale. 6. The drawings shall be in 1:1 scale and shall be in 1:1 scale. 7. The drawings shall be in 1:1 scale and shall be in 1:1 scale. 8. The drawings shall be in 1:1 scale and shall be in 1:1 scale. 9. The drawings shall be in 1:1 scale and shall be in 1:1 scale. 10. The drawings shall be in 1:1 scale and shall be in 1:1 scale.

Client
NATIONAL HIGHWAYS AUTHORITY OF INDIA
 (Ministry of Shipping, Road Transport & Highways)

Drawing Title: **PLAN & PROFILE**
 Km. 16+000 to 17+000
 Drawing No.: **SW/1019/ROADS/PP-14** Sheet 1 of 2
 Scale: --- HOR:-1:2000 VER:-1:200
 Dwn: K.L.N. Dgn: K.PRASAD Appd: BRIG.C.D.PUR Date: JUNE-2009

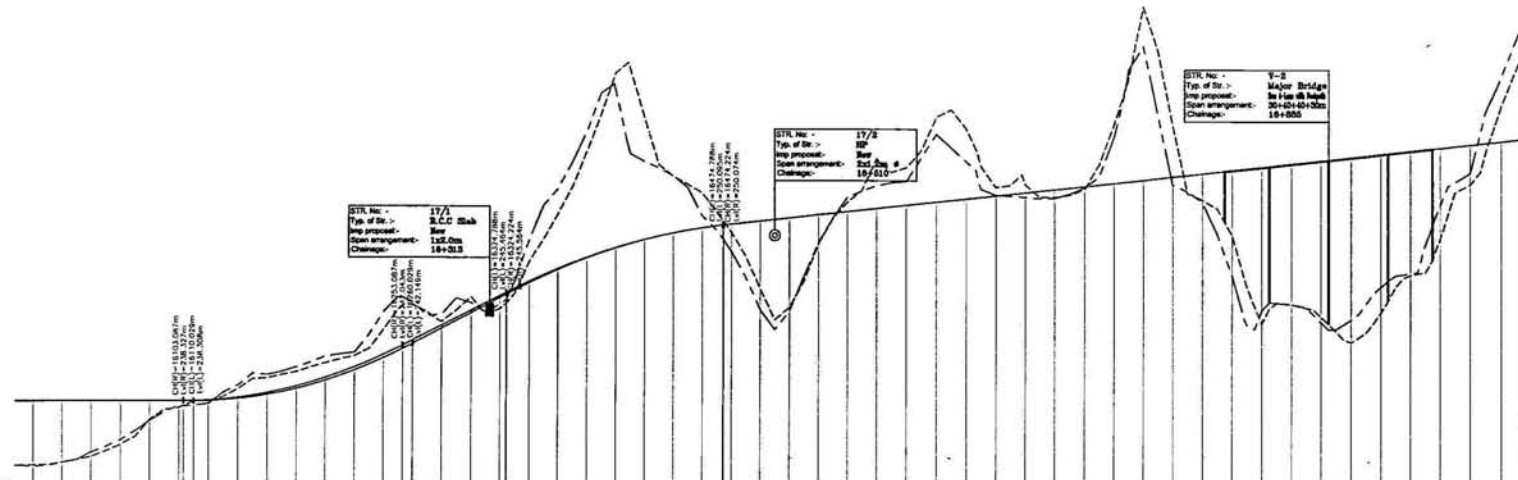
Scott Wilson India Private Limited
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 E-mail: scwinfo@swi.com



Jammu

Udhampur

S9



| | | | | |
|---------------------------|--|--|---|---|
| Level Datum = 232.000 | Proposed Level along Left Median Edge (m.) | Existing Level Below Left Median Edge (m.) | Proposed Level along Right Median Edge (m.) | Existing Level Below Right Median Edge (m.) |
| Horizontal Alignment Left | Horizontal Alignment Right | Vertical Alignment Left | Vertical Alignment Right | LHS Superelevation |
| RHS Superelevation | Left Profile Chainage | Right Profile Chainage | | |

| LEGEND | Project Title |
|--|--|
| <ul style="list-style-type: none"> 1. Proposed Four Lanes 2. Existing 2-lane 3. Proposed Left Hand Side 4. Proposed Right Hand Side 5. Existing Left Hand Side 6. Existing Right Hand Side 7. Existing 2-lane 8. Existing 4-lane | Consultancy services for preparation of detailed project report for Rehabilitation and upgrading of existing 2-lane road to 4-lane divided carriageway configuration of Jammu - Udhampur section of NH-1A (Km 0.00 to Km 66.00) including Jammu(Ut) Nagra and Udhampur Bypass Phase - (II) Programme of North-South corridor Project, Contract Package C-01/11 |

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Scale: 1:1000

Date: JUNE-2009

NATIONAL HIGHWAYS AUTHORITY OF INDIA
(Ministry of Shipping, Road Transport & Highways)

Client: NATIONAL HIGHWAYS AUTHORITY OF INDIA

Drawing Title: **PLAN & PROFILE**
Km. 16+000 to 17+000

Drawing No.: **SW/1019/ROADS/PP - 14**

Scale: **HOR:-1:2000 VER:-1:200**

Sheet: **2 of 2**

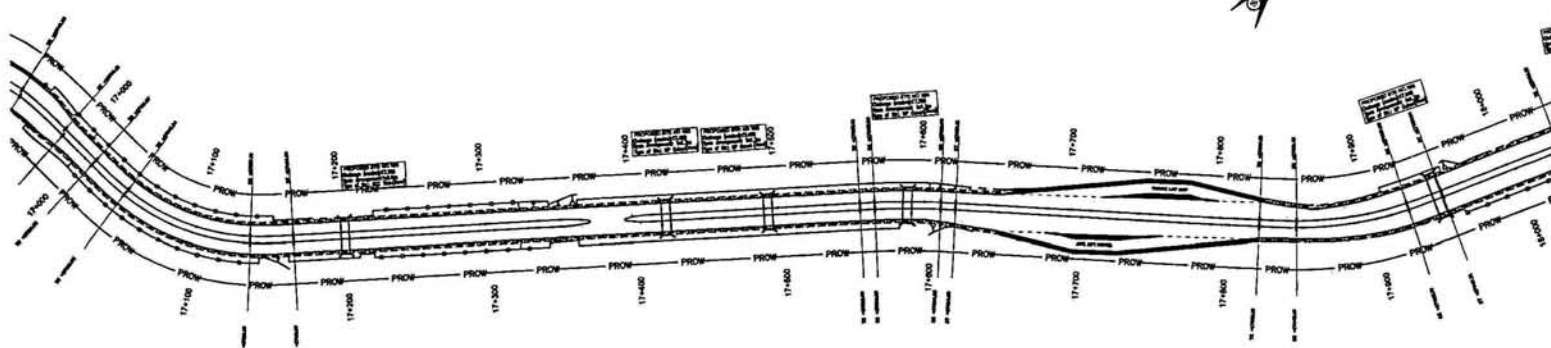
Date: **JUNE-2009**

Scott Wilson India Private Limited
A-25/4, MOHAN CO-OPERATIVE IND. ESTATE
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NEW DELHI - 110044
Tel: (011) 41679340 - 49
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E-mail: scw@scw.com



← Jammu
60

Udhampur →



Level Datum = 248.000

| |
|---|
| Proposed Level along Left Median Edge (m.) |
| Existing Level Below Left Median Edge (m.) |
| Proposed Level along Right Median Edge (m.) |
| Existing Level Below Right Median Edge (m.) |
| Horizontal Alignment Left |
| Horizontal Alignment Right |
| Vertical Alignment Left |
| Vertical Alignment Right |
| LHS Superelevation |
| RHS Superelevation |
| Left Profile Chainage |
| Right Profile Chainage |

| NO. | DESCRIPTION | DATE |
|-----|-------------|------|
| 1. | Issue | |
| 2. | Revised | |
| 3. | Final | |
| 4. | As per | |
| 5. | As per | |
| 6. | As per | |
| 7. | As per | |
| 8. | As per | |
| 9. | As per | |
| 10. | As per | |

Project Title
 Consultancy services for preparation of detailed project report for Rehabilitation and upgrading of existing 2-lane road to 4-lane divided carriageway configuration of Jammu - Udhampur section of NH-1A (km 0.00 to km 64.00) including Jammu(Ur) Bypass and Udhampur Bypass Phase - III Programme of North - South - corridor Project, Contract Package C-02718

Client
NATIONAL HIGHWAYS AUTHORITY OF INDIA
 (Ministry of Shipping, Road Transport & Highways)

Client
NATIONAL HIGHWAYS AUTHORITY OF INDIA
 (Ministry of Shipping, Road Transport & Highways)

Drawing Title: PLAN & PROFILE
 Km. 17+000 to 18+000

Drawing No.: SWW1019/ROADS/PP-15
Scale: HOR:-1:2000 VER:-1:200

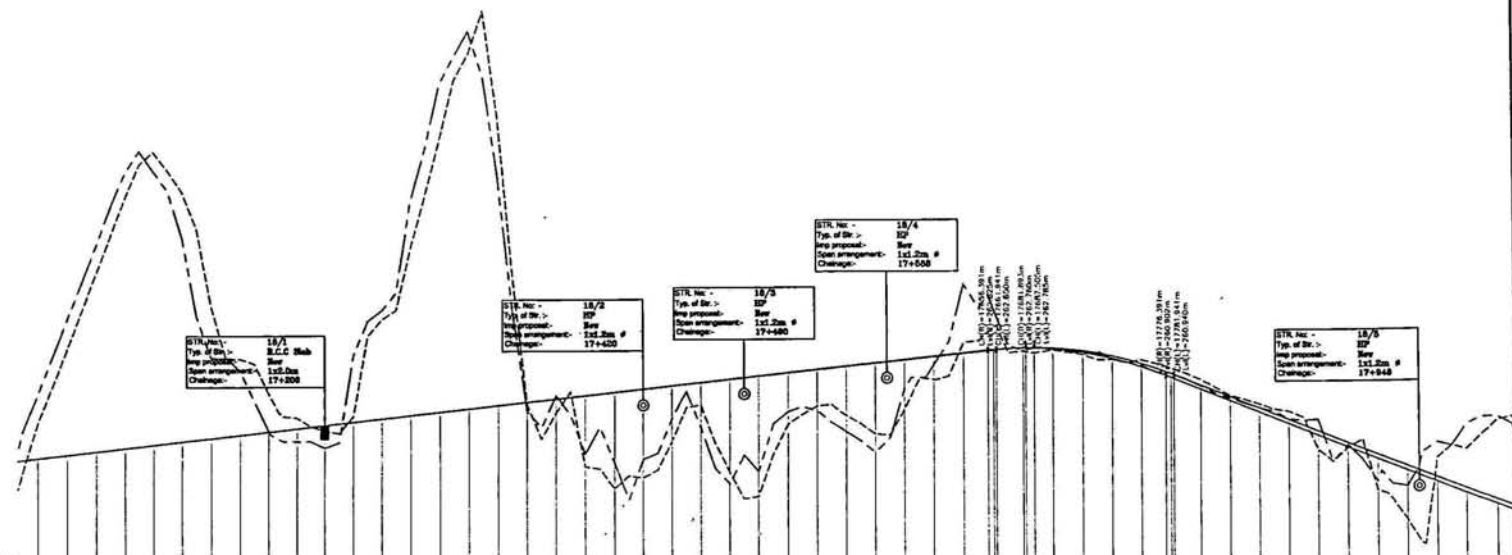
Sheet: 1 of 2

Date: JUNE-2009

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 E-mail: scwilson@swi.com



51



| | |
|---|---|
| Level Datum = 248.000 | |
| Proposed Level along Left Median Edge (m.) | 248.000 255.654 252.240 255.646 |
| Existing Level Below Left Median Edge (m.) | 254.867 255.650 252.207 255.651 |
| Proposed Level along Right Median Edge (m.) | 254.867 255.650 252.207 255.651 |
| Existing Level Below Right Median Edge (m.) | 254.867 255.650 252.207 255.651 |
| Horizontal Alignment Left | D=29.858 Q=22.272 R=142.750 L=60.786 CL=28.774 C=308.208 |
| Horizontal Alignment Right | R=132.258 L=64.333 CL=35.000 C=308.208 |
| Vertical Alignment Left | C=1.058 L=112.134m |
| Vertical Alignment Right | R=1.058 L=112.134m |
| LHS Super-elevation | 0% 2.000 0% 2.000 |
| RHS Super-elevation | 0% 2.000 0% 2.000 |
| Left Profile Chainage | 17002.000 17003.000 17004.000 17005.000 17006.000 17007.000 17008.000 17009.000 17010.000 17011.000 17012.000 17013.000 17014.000 17015.000 17016.000 17017.000 17018.000 17019.000 17020.000 17021.000 17022.000 17023.000 17024.000 17025.000 17026.000 17027.000 17028.000 17029.000 17030.000 17031.000 17032.000 17033.000 17034.000 17035.000 17036.000 17037.000 17038.000 17039.000 17040.000 17041.000 17042.000 17043.000 17044.000 17045.000 17046.000 17047.000 17048.000 17049.000 17050.000 17051.000 17052.000 17053.000 17054.000 17055.000 17056.000 17057.000 17058.000 17059.000 17060.000 17061.000 17062.000 17063.000 17064.000 17065.000 17066.000 17067.000 17068.000 17069.000 17070.000 17071.000 17072.000 17073.000 17074.000 17075.000 17076.000 17077.000 17078.000 17079.000 17080.000 17081.000 17082.000 17083.000 17084.000 17085.000 17086.000 17087.000 17088.000 17089.000 17090.000 17091.000 17092.000 17093.000 17094.000 17095.000 17096.000 17097.000 17098.000 17099.000 17100.000 |
| Right Profile Chainage | 17002.000 17003.000 17004.000 17005.000 17006.000 17007.000 17008.000 17009.000 17010.000 17011.000 17012.000 17013.000 17014.000 17015.000 17016.000 17017.000 17018.000 17019.000 17020.000 17021.000 17022.000 17023.000 17024.000 17025.000 17026.000 17027.000 17028.000 17029.000 17030.000 17031.000 17032.000 17033.000 17034.000 17035.000 17036.000 17037.000 17038.000 17039.000 17040.000 17041.000 17042.000 17043.000 17044.000 17045.000 17046.000 17047.000 17048.000 17049.000 17050.000 17051.000 17052.000 17053.000 17054.000 17055.000 17056.000 17057.000 17058.000 17059.000 17060.000 17061.000 17062.000 17063.000 17064.000 17065.000 17066.000 17067.000 17068.000 17069.000 17070.000 17071.000 17072.000 17073.000 17074.000 17075.000 17076.000 17077.000 17078.000 17079.000 17080.000 17081.000 17082.000 17083.000 17084.000 17085.000 17086.000 17087.000 17088.000 17089.000 17090.000 17091.000 17092.000 17093.000 17094.000 17095.000 17096.000 17097.000 17098.000 17099.000 17100.000 |

| REVISION | |
|----------|-------------------------|
| No. | Description |
| 1 | As per approved drawing |
| 2 | As per approved drawing |
| 3 | As per approved drawing |
| 4 | As per approved drawing |
| 5 | As per approved drawing |
| 6 | As per approved drawing |
| 7 | As per approved drawing |
| 8 | As per approved drawing |
| 9 | As per approved drawing |
| 10 | As per approved drawing |

Project Title
 Rehabilitation and widening of existing 2-lane road to 4-lane divided carriageway and operations of Jammu - Udhampur section of NH-1A (km 0.00 to km 66.00) including Jammu/Parit Nagar and Udhampur Bypass Phase - III Programme of North-South corridor Project, Contract Package C-08/11

Client
 NATIONAL HIGHWAYS AUTHORITY OF INDIA (Ministry of Shipping, Road Transport & Highways)

Drawing Title: PLAN & PROFILE
 Km. 17+000 to 18+000

Drawing No.: SWW/1019/ROADS/PP - 15

Scale: HOR:-1:2000 VER:-1:200

Drawn: K.L.N

Appd: BRIG.C.D.PURI

Date: JUNE-2009

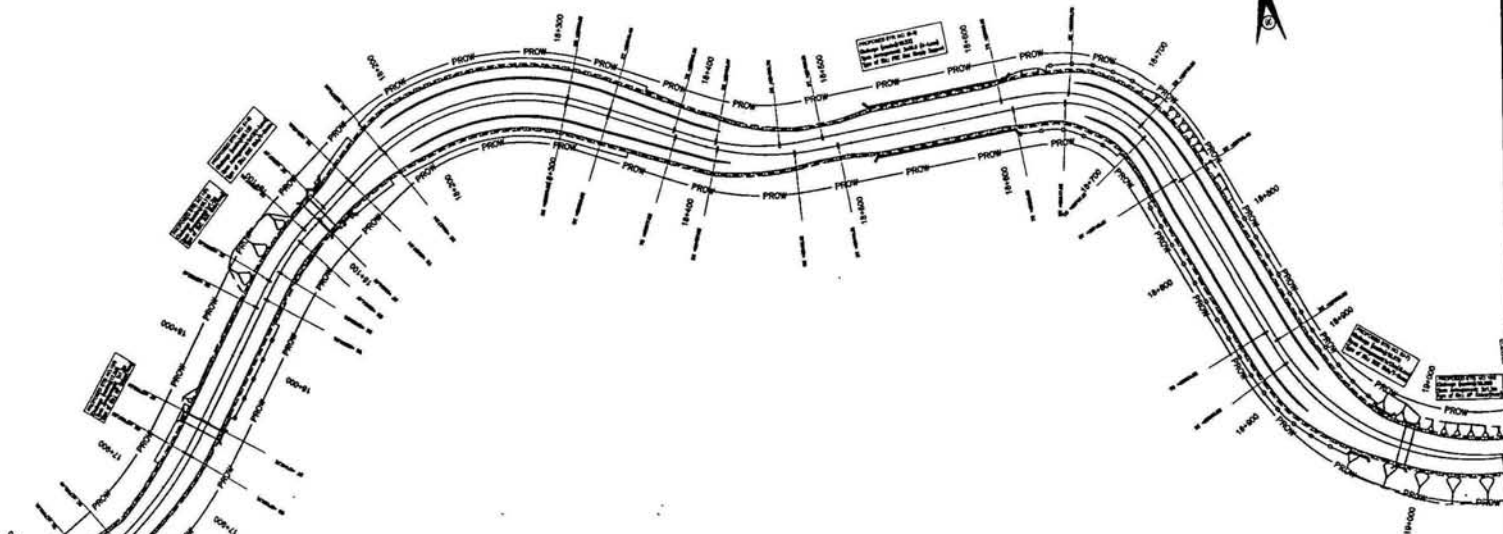
Scott Wilson India Private Limited
 A-26/4, MOHAN CO-OPERATIVE IND. ESTATE
 BATHINDA ROAD
 NEW DELHI - 110044
 Tele: (011) 41679340 - 49
 Fax: (011) 41679350
 E-mail: scotwilson@gmail.com

Scott Wilson

← Jammu

62

Udhampur →



Level Datum = 225.000

| | |
|---|---|
| Proposed Level along Left Median Edge (m.) | Existing Level Below Left Median Edge (m.) |
| Proposed Level along Right Median Edge (m.) | Existing Level Below Right Median Edge (m.) |
| Horizontal Alignment Left | Horizontal Alignment Right |
| Vertical Alignment Left | Vertical Alignment Right |
| LHS Super-elevation | RHS Super-elevation |
| Left Profile Chainage | Right Profile Chainage |

| Sl. No. | Particulars | Remarks |
|---------|---|---------|
| 1. | 1. Existing 2-lane road to be widened to 4-lane divided | |
| 2. | 2. Existing 2-lane road to be widened to 4-lane divided | |
| 3. | 3. Existing 2-lane road to be widened to 4-lane divided | |
| 4. | 4. Existing 2-lane road to be widened to 4-lane divided | |
| 5. | 5. Existing 2-lane road to be widened to 4-lane divided | |
| 6. | 6. Existing 2-lane road to be widened to 4-lane divided | |
| 7. | 7. Existing 2-lane road to be widened to 4-lane divided | |
| 8. | 8. Existing 2-lane road to be widened to 4-lane divided | |

Project Title
 Consultancy services for preparation of detailed project report for Rehabilitation and upgrading of existing 2-lane road to 4-lane divided
 Corridorway configuration of Jammu - Udhampur section of NH-1 A
 Km 0.00 to Km 64.00 including Jammu (Part) Bypass and Udhampur Bypass Phase - III Programme of North - South - corridor Project, Contract Package C-02/14

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Scale
 1. Section and Plan to 1:2000
 2. The drawings shall be in A3 size and shall be on 1:2000 scale
 3. The drawings shall be in A3 size and shall be on 1:2000 scale
 4. The drawings shall be in A3 size and shall be on 1:2000 scale
 5. The drawings shall be in A3 size and shall be on 1:2000 scale
 6. The drawings shall be in A3 size and shall be on 1:2000 scale

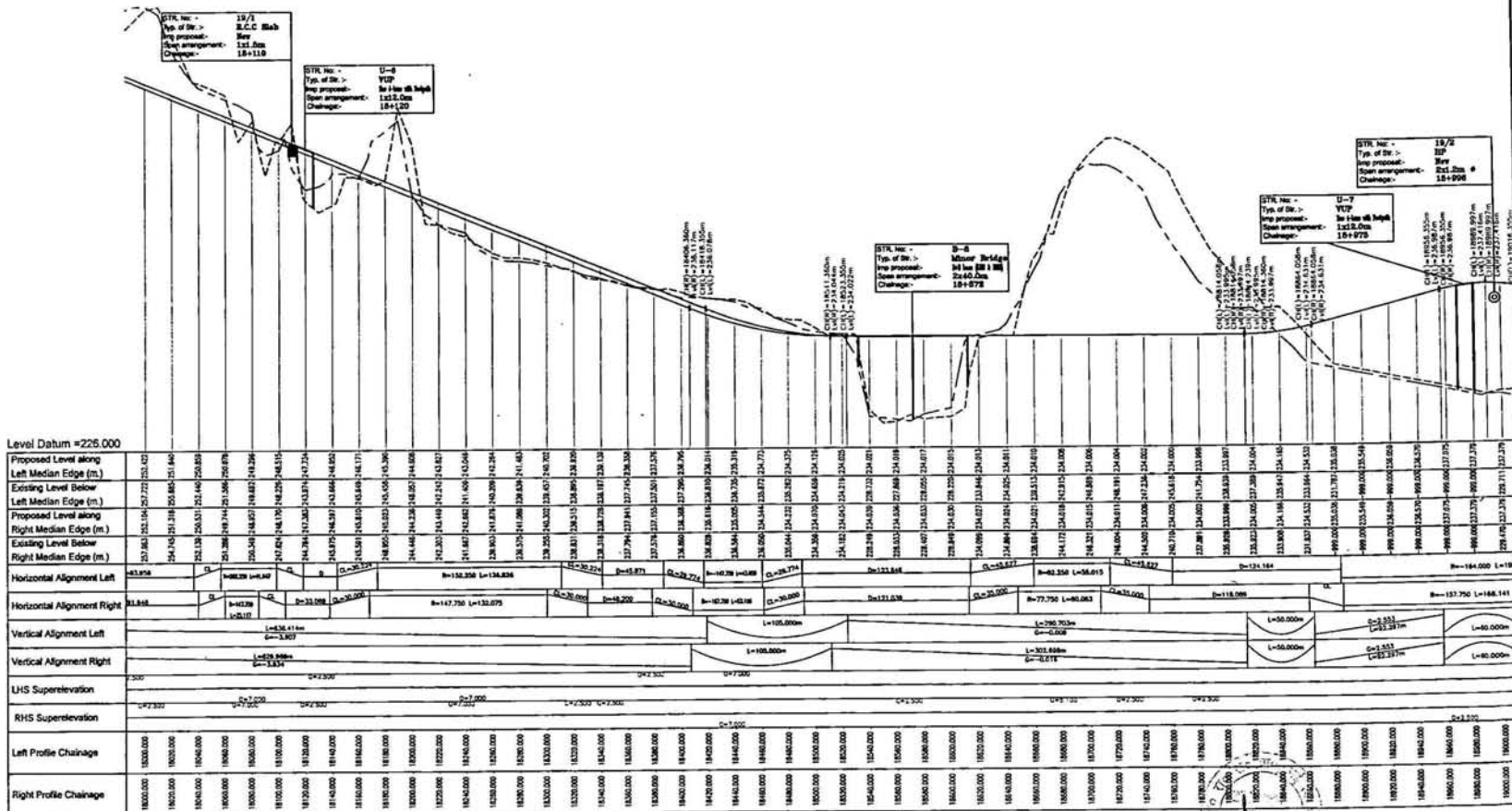
Client
NATIONAL HIGHWAYS AUTHORITY OF INDIA
 (Ministry of Shipping, Road Transport & Highways)

Drawing Title: PLAN & PROFILE
 Km. 18+000 to 19+000

Drawing No.: SW/1019/ROADS/PP - 16
Scale: HOR:-1:2000 VER:-1:200
Drawn by: K.L.N
Checked by: Dgn. K.PRASAD
Appd: BRG.C.D.PURI
Date: JUNE-2009

Client: Scott Wilson India Private Limited
 A-26/4, MOHAN CO-OPERATIVE IND. ESTATE
 MATHERA ROAD
 NEW DELHI - 110044
 Tele: (011) 41679340 - 49
 Fax: (011) 41679350
 E-mail: scwinfo@swi.com





| 18+000 | |
|----------------------------------|----------------------------------|
| For | Proposed |
| 1. Existing R.O. alignment | 1. Proposed R.O. alignment |
| 2. Existing L.O. alignment | 2. Proposed L.O. alignment |
| 3. Existing Right Hand Side Edge | 3. Proposed Right Hand Side Edge |
| 4. Existing Left Hand Side Edge | 4. Proposed Left Hand Side Edge |
| 5. Existing Top of Road | 5. Proposed Top of Road |
| 6. Existing Bottom of Road | 6. Proposed Bottom of Road |
| 7. Existing Slope | 7. Proposed Slope |
| 8. Existing Elevation | 8. Proposed Elevation |
| 9. Existing Chainage | 9. Proposed Chainage |
| 10. Existing Cross Section | 10. Proposed Cross Section |
| 11. Existing Profile | 11. Proposed Profile |
| 12. Existing Plan | 12. Proposed Plan |
| 13. Existing Details | 13. Proposed Details |
| 14. Existing Notes | 14. Proposed Notes |
| 15. Existing Remarks | 15. Proposed Remarks |

Project Title
 Consultancy services for preparation of detailed project report for Rehabilitation and upgrading of existing 2 lane road to 4 lane divided Carriageway and provision of Jammu - Udhampur section of NH-14 A (on 0.00 to 18.00 km) including Jammu(UJ) Bypass and Udhampur Bypass Phase - III Programme of North-South corridor Project, Contract Package-CB-18

Client
 NATIONAL HIGHWAYS AUTHORITY OF INDIA
 (Ministry of Shipping, Road Transport & Highways)

Drawing Title: PLAN & PROFILE
 Km. 18+000 to 19+000

Drawing No.: SW/1019/ROADS/PP-16

Scale: 1:2000
 HOR:-1:2000 VER:-1:200

Sheet: 2 of 2

Date: JUNE-2009

Author: K.L.N.
Checked: K.PRASAD
Appd: BRG.C.D.PURI

Scott Wilson India Private Limited
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 MATURA ROAD
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 Tel: (011) 41679340 - 49
 Fax: (011) 41679350
 E-mail: scwilson@psl.com



| | | |
|-----|---------------|----------------------|
| 1 | Scale | 1:1000 |
| 2 | Project Title | PLAN & PROFILE |
| 3 | Project No. | SW/1019/ROADS/P-17 |
| 4 | Sheet No. | 1 of 2 |
| 5 | Date | JUNE-2009 |
| 6 | Appd. | K.P.R.S.A.D |
| 7 | DRG.C.D.PUR | |
| 8 | DRG | |
| 9 | Scale | MOR-1:2000 VER-1:200 |
| 10 | Sheet | |
| 11 | Project Title | PLAN & PROFILE |
| 12 | Project No. | SW/1019/ROADS/P-17 |
| 13 | Sheet No. | 1 of 2 |
| 14 | Date | JUNE-2009 |
| 15 | Appd. | K.P.R.S.A.D |
| 16 | DRG.C.D.PUR | |
| 17 | DRG | |
| 18 | Scale | MOR-1:2000 VER-1:200 |
| 19 | Sheet | |
| 20 | Project Title | PLAN & PROFILE |
| 21 | Project No. | SW/1019/ROADS/P-17 |
| 22 | Sheet No. | 1 of 2 |
| 23 | Date | JUNE-2009 |
| 24 | Appd. | K.P.R.S.A.D |
| 25 | DRG.C.D.PUR | |
| 26 | DRG | |
| 27 | Scale | MOR-1:2000 VER-1:200 |
| 28 | Sheet | |
| 29 | Project Title | PLAN & PROFILE |
| 30 | Project No. | SW/1019/ROADS/P-17 |
| 31 | Sheet No. | 1 of 2 |
| 32 | Date | JUNE-2009 |
| 33 | Appd. | K.P.R.S.A.D |
| 34 | DRG.C.D.PUR | |
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| 36 | Scale | MOR-1:2000 VER-1:200 |
| 37 | Sheet | |
| 38 | Project Title | PLAN & PROFILE |
| 39 | Project No. | SW/1019/ROADS/P-17 |
| 40 | Sheet No. | 1 of 2 |
| 41 | Date | JUNE-2009 |
| 42 | Appd. | K.P.R.S.A.D |
| 43 | DRG.C.D.PUR | |
| 44 | DRG | |
| 45 | Scale | MOR-1:2000 VER-1:200 |
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| 47 | Project Title | PLAN & PROFILE |
| 48 | Project No. | SW/1019/ROADS/P-17 |
| 49 | Sheet No. | 1 of 2 |
| 50 | Date | JUNE-2009 |
| 51 | Appd. | K.P.R.S.A.D |
| 52 | DRG.C.D.PUR | |
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| 54 | Scale | MOR-1:2000 VER-1:200 |
| 55 | Sheet | |
| 56 | Project Title | PLAN & PROFILE |
| 57 | Project No. | SW/1019/ROADS/P-17 |
| 58 | Sheet No. | 1 of 2 |
| 59 | Date | JUNE-2009 |
| 60 | Appd. | K.P.R.S.A.D |
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| 64 | Sheet | |
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| 66 | Project No. | SW/1019/ROADS/P-17 |
| 67 | Sheet No. | 1 of 2 |
| 68 | Date | JUNE-2009 |
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| 70 | DRG.C.D.PUR | |
| 71 | DRG | |
| 72 | Scale | MOR-1:2000 VER-1:200 |
| 73 | Sheet | |
| 74 | Project Title | PLAN & PROFILE |
| 75 | Project No. | SW/1019/ROADS/P-17 |
| 76 | Sheet No. | 1 of 2 |
| 77 | Date | JUNE-2009 |
| 78 | Appd. | K.P.R.S.A.D |
| 79 | DRG.C.D.PUR | |
| 80 | DRG | |
| 81 | Scale | MOR-1:2000 VER-1:200 |
| 82 | Sheet | |
| 83 | Project Title | PLAN & PROFILE |
| 84 | Project No. | SW/1019/ROADS/P-17 |
| 85 | Sheet No. | 1 of 2 |
| 86 | Date | JUNE-2009 |
| 87 | Appd. | K.P.R.S.A.D |
| 88 | DRG.C.D.PUR | |
| 89 | DRG | |
| 90 | Scale | MOR-1:2000 VER-1:200 |
| 91 | Sheet | |
| 92 | Project Title | PLAN & PROFILE |
| 93 | Project No. | SW/1019/ROADS/P-17 |
| 94 | Sheet No. | 1 of 2 |
| 95 | Date | JUNE-2009 |
| 96 | Appd. | K.P.R.S.A.D |
| 97 | DRG.C.D.PUR | |
| 98 | DRG | |
| 99 | Scale | MOR-1:2000 VER-1:200 |
| 100 | Sheet | |

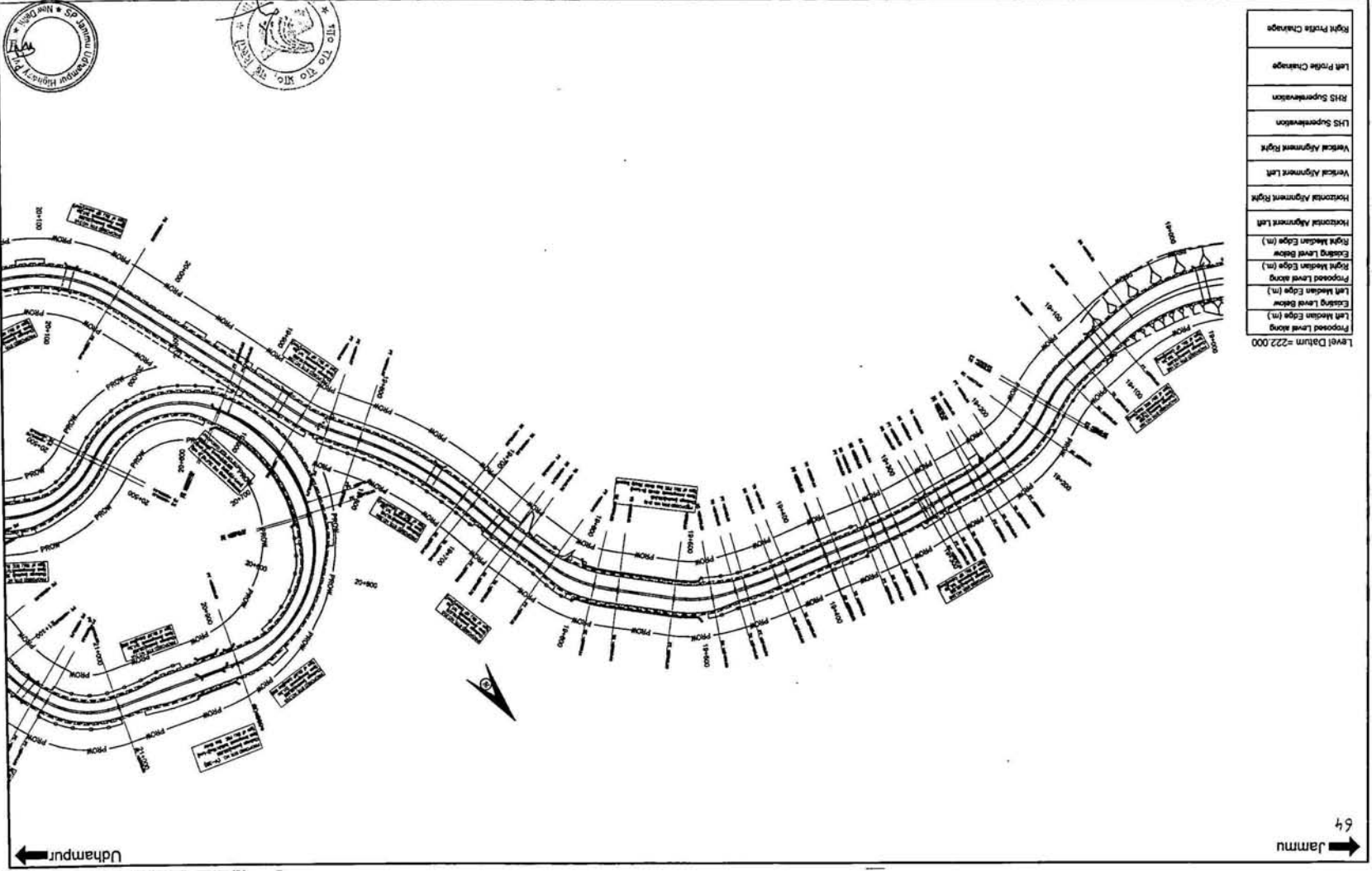
Project Title
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 Drawing No.: SW/1019/ROADS/P-17
 Scale: MOR-1:2000 VER-1:200
 Date: JUNE-2009
 Appd: K.P.R.S.A.D
 DRG.C.D.PUR
 DRG

Client
 NATIONAL HIGHWAYS
 AUTHORITY OF INDIA
 (Ministry of Shipping, Road Transport & Highways)

Contract No.: SW/1019/ROADS/P-17
 Scale: MOR-1:2000 VER-1:200
 Date: JUNE-2009
 Appd: K.P.R.S.A.D
 DRG.C.D.PUR
 DRG

Project Title
 PLAN & PROFILE
 Drawing No.: SW/1019/ROADS/P-17
 Scale: MOR-1:2000 VER-1:200
 Date: JUNE-2009
 Appd: K.P.R.S.A.D
 DRG.C.D.PUR
 DRG

Project Title
 PLAN & PROFILE
 Drawing No.: SW/1019/ROADS/P-17
 Scale: MOR-1:2000 VER-1:200
 Date: JUNE-2009
 Appd: K.P.R.S.A.D
 DRG.C.D.PUR
 DRG



| |
|--|
| Right Profile Change |
| Left Profile Change |
| RHS Superelevation |
| LHS Superelevation |
| Vertical Alignment Right |
| Vertical Alignment Left |
| Horizontal Alignment Right |
| Horizontal Alignment Left |
| Right Median Edge (m) |
| Existing Level Along Right Median Edge (m) |
| Proposed Level Along Right Median Edge (m) |
| Left Median Edge (m) |
| Existing Level Along Left Median Edge (m) |
| Proposed Level Along Left Median Edge (m) |
| Level Datum = 222.000 |

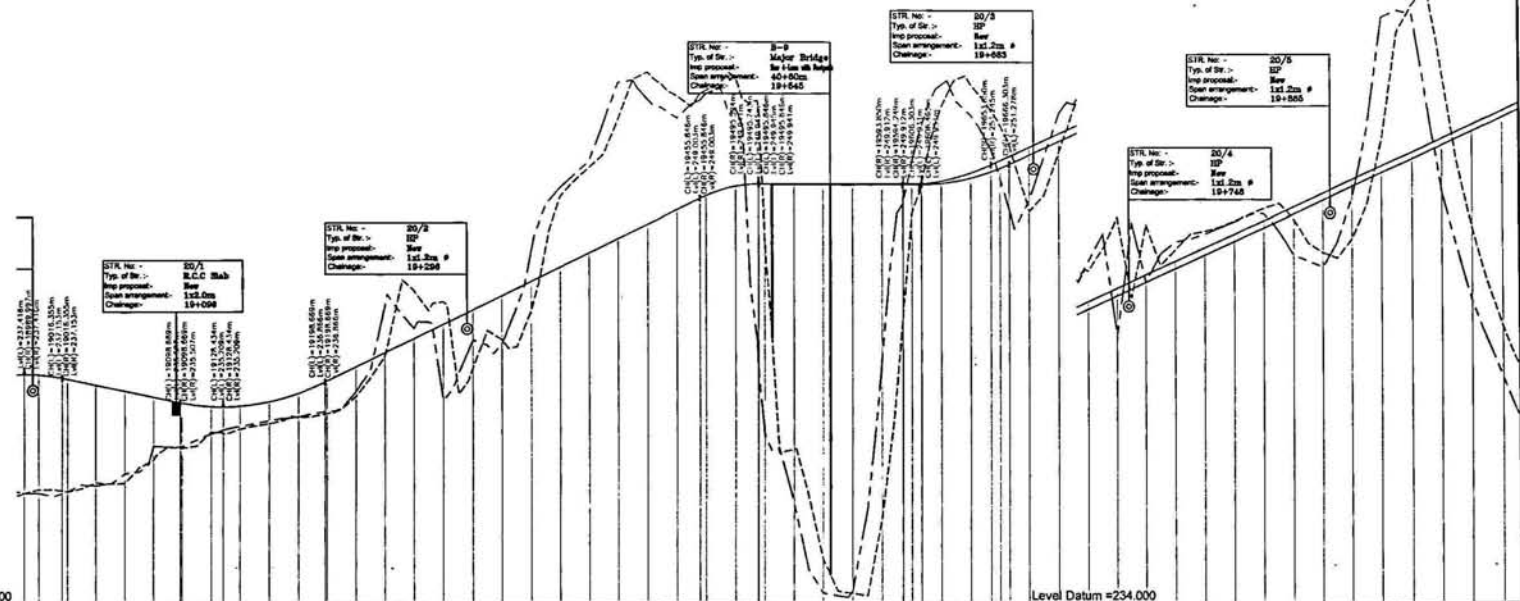


Uthampur

Jammu

Jammu ← 65

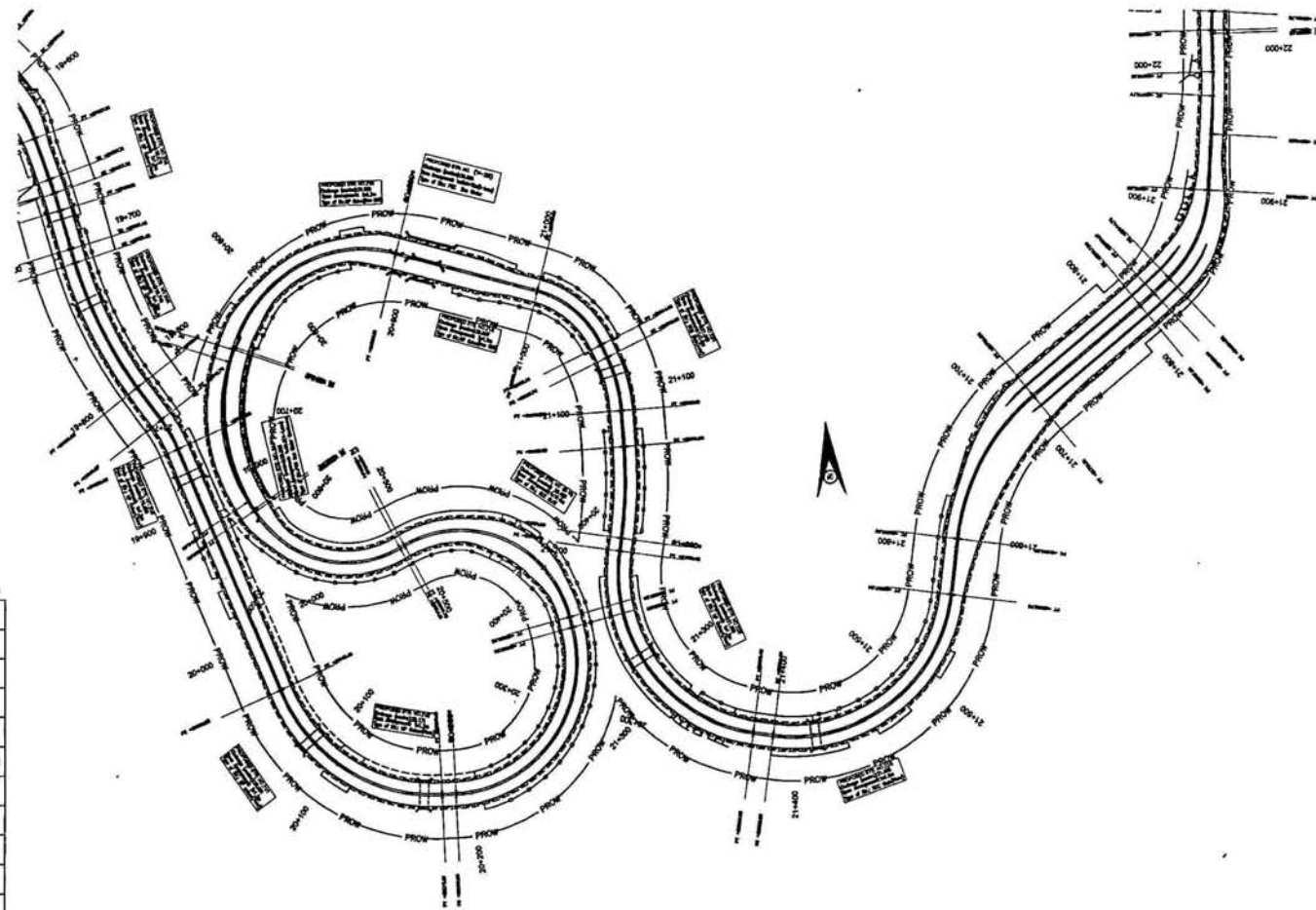
Udhampur →



| Level Datum = 222.000 | Level Datum = 234.000 | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
|--|--|--------------------|--------------------|--------------------|--------------------|--------------------|--------------------|--------------------|--------------------|--------------------|--------------------|--------------------|--------------------|--------------------|--------------------|--------------------|--------------------|--------------------|--------------------|--------------------|--------------------|--------------------|--------------------|--------------------|--------------------|--------------------|--------------------|--------------------|--------------------|--------------------|--------------------|--------------------|--------------------|--------------------|--------------------|--------------------|--------------------|--------------------|--------------------|--------------------|------------|--|--------------------|--------------------|--------------------|--------------------|--------------------|--------------------|--------------------|--------------------|--------------------|--------------------|--------------------|--------------------|--------------------|--------------------|--------------------|--------------------|--------------------|--------------------|--------------------|--------------------|--------------------|--------------------|--------------------|--------------------|--------------------|--------------------|--------------------|--------------------|--------------------|--------------------|--------------------|--------------------|--------------------|--------------------|--------------------|--------------------|--------------------|--------------------|--------------------|--|
| Proposed Level along Left Median Edge (m) | 228.670 227.270 226.311 225.276 | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| Existing Level Below Left Median Edge (m) | 228.670 227.260 226.644 225.986 | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| Proposed Level along Right Median Edge (m) | 229.845 228.660 227.336 225.986 | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| Existing Level Below Right Median Edge (m) | 229.845 228.200 227.128 226.236 | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| Horizontal Alignment Left | <table border="1"> <tr> <td>Station</td> <td>19+188.882</td> <td>19+211.117</td> <td>19+233.352</td> <td>19+255.587</td> <td>19+277.822</td> <td>19+300.057</td> <td>19+322.292</td> <td>19+344.527</td> <td>19+366.762</td> <td>19+389.000</td> <td>19+411.235</td> <td>19+433.470</td> <td>19+455.705</td> <td>19+477.940</td> <td>19+500.175</td> <td>19+522.410</td> <td>19+544.645</td> <td>19+566.880</td> <td>19+589.115</td> <td>19+611.350</td> <td>19+633.585</td> <td>19+655.820</td> <td>19+678.055</td> <td>19+700.290</td> <td>19+722.525</td> <td>19+744.760</td> <td>19+767.000</td> <td>19+789.235</td> <td>19+811.470</td> <td>19+833.705</td> <td>19+855.940</td> <td>19+878.175</td> <td>19+900.410</td> <td>19+922.645</td> <td>19+944.880</td> <td>19+971.370</td> <td>19+997.860</td> <td>20+000.000</td> </tr> <tr> <td>Curve Data</td> <td></td> <td>R=100.750 L=31.187</td> <td>R=100.750 L=31.187</td> <td>R=100.750 L=31.187</td> <td>R=100.750 L=31.187</td> <td>R=100.750 L=31.187</td> <td>R=100.750 L=31.187</td> <td>R=100.750 L=31.187</td> <td>R=100.750 L=31.187</td> <td>R=100.750 L=31.187</td> <td>R=100.750 L=31.187</td> <td>R=100.750 L=31.187</td> <td>R=100.750 L=31.187</td> <td>R=100.750 L=31.187</td> <td>R=100.750 L=31.187</td> <td>R=100.750 L=31.187</td> <td>R=100.750 L=31.187</td> <td>R=100.750 L=31.187</td> <td>R=100.750 L=31.187</td> <td>R=100.750 L=31.187</td> <td>R=100.750 L=31.187</td> <td>R=100.750 L=31.187</td> <td>R=100.750 L=31.187</td> <td>R=100.750 L=31.187</td> <td>R=100.750 L=31.187</td> <td>R=100.750 L=31.187</td> <td>R=100.750 L=31.187</td> <td>R=100.750 L=31.187</td> <td>R=100.750 L=31.187</td> <td>R=100.750 L=31.187</td> <td>R=100.750 L=31.187</td> <td>R=100.750 L=31.187</td> <td>R=100.750 L=31.187</td> <td>R=100.750 L=31.187</td> <td>R=100.750 L=31.187</td> <td>R=100.750 L=31.187</td> <td>R=100.750 L=31.187</td> <td>R=100.750 L=31.187</td> <td>R=100.750 L=31.187</td> <td>R=100.750 L=31.187</td> </tr> </table> | Station | 19+188.882 | 19+211.117 | 19+233.352 | 19+255.587 | 19+277.822 | 19+300.057 | 19+322.292 | 19+344.527 | 19+366.762 | 19+389.000 | 19+411.235 | 19+433.470 | 19+455.705 | 19+477.940 | 19+500.175 | 19+522.410 | 19+544.645 | 19+566.880 | 19+589.115 | 19+611.350 | 19+633.585 | 19+655.820 | 19+678.055 | 19+700.290 | 19+722.525 | 19+744.760 | 19+767.000 | 19+789.235 | 19+811.470 | 19+833.705 | 19+855.940 | 19+878.175 | 19+900.410 | 19+922.645 | 19+944.880 | 19+971.370 | 19+997.860 | 20+000.000 | Curve Data | | R=100.750 L=31.187 | R=100.750 L=31.187 | R=100.750 L=31.187 | R=100.750 L=31.187 | R=100.750 L=31.187 | R=100.750 L=31.187 | R=100.750 L=31.187 | R=100.750 L=31.187 | R=100.750 L=31.187 | R=100.750 L=31.187 | R=100.750 L=31.187 | R=100.750 L=31.187 | R=100.750 L=31.187 | R=100.750 L=31.187 | R=100.750 L=31.187 | R=100.750 L=31.187 | R=100.750 L=31.187 | R=100.750 L=31.187 | R=100.750 L=31.187 | R=100.750 L=31.187 | R=100.750 L=31.187 | R=100.750 L=31.187 | R=100.750 L=31.187 | R=100.750 L=31.187 | R=100.750 L=31.187 | R=100.750 L=31.187 | R=100.750 L=31.187 | R=100.750 L=31.187 | R=100.750 L=31.187 | R=100.750 L=31.187 | R=100.750 L=31.187 | R=100.750 L=31.187 | R=100.750 L=31.187 | R=100.750 L=31.187 | R=100.750 L=31.187 | R=100.750 L=31.187 | R=100.750 L=31.187 | R=100.750 L=31.187 | R=100.750 L=31.187 | |
| Station | 19+188.882 | 19+211.117 | 19+233.352 | 19+255.587 | 19+277.822 | 19+300.057 | 19+322.292 | 19+344.527 | 19+366.762 | 19+389.000 | 19+411.235 | 19+433.470 | 19+455.705 | 19+477.940 | 19+500.175 | 19+522.410 | 19+544.645 | 19+566.880 | 19+589.115 | 19+611.350 | 19+633.585 | 19+655.820 | 19+678.055 | 19+700.290 | 19+722.525 | 19+744.760 | 19+767.000 | 19+789.235 | 19+811.470 | 19+833.705 | 19+855.940 | 19+878.175 | 19+900.410 | 19+922.645 | 19+944.880 | 19+971.370 | 19+997.860 | 20+000.000 | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| Curve Data | | R=100.750 L=31.187 | R=100.750 L=31.187 | R=100.750 L=31.187 | R=100.750 L=31.187 | R=100.750 L=31.187 | R=100.750 L=31.187 | R=100.750 L=31.187 | R=100.750 L=31.187 | R=100.750 L=31.187 | R=100.750 L=31.187 | R=100.750 L=31.187 | R=100.750 L=31.187 | R=100.750 L=31.187 | R=100.750 L=31.187 | R=100.750 L=31.187 | R=100.750 L=31.187 | R=100.750 L=31.187 | R=100.750 L=31.187 | R=100.750 L=31.187 | R=100.750 L=31.187 | R=100.750 L=31.187 | R=100.750 L=31.187 | R=100.750 L=31.187 | R=100.750 L=31.187 | R=100.750 L=31.187 | R=100.750 L=31.187 | R=100.750 L=31.187 | R=100.750 L=31.187 | R=100.750 L=31.187 | R=100.750 L=31.187 | R=100.750 L=31.187 | R=100.750 L=31.187 | R=100.750 L=31.187 | R=100.750 L=31.187 | R=100.750 L=31.187 | R=100.750 L=31.187 | R=100.750 L=31.187 | R=100.750 L=31.187 | R=100.750 L=31.187 | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| Vertical Alignment Left | <table border="1"> <tr> <td>Station</td> <td>19+188.882</td> <td>19+211.117</td> <td>19+233.352</td> <td>19+255.587</td> <td>19+277.822</td> <td>19+300.057</td> <td>19+322.292</td> <td>19+344.527</td> <td>19+366.762</td> <td>19+389.000</td> <td>19+411.235</td> <td>19+433.470</td> <td>19+455.705</td> <td>19+477.940</td> <td>19+500.175</td> <td>19+522.410</td> <td>19+544.645</td> <td>19+566.880</td> <td>19+589.115</td> <td>19+611.350</td> <td>19+633.585</td> <td>19+655.820</td> <td>19+678.055</td> <td>19+700.290</td> <td>19+722.525</td> <td>19+744.760</td> <td>19+767.000</td> <td>19+789.235</td> <td>19+811.470</td> <td>19+833.705</td> <td>19+855.940</td> <td>19+878.175</td> <td>19+900.410</td> <td>19+922.645</td> <td>19+944.880</td> <td>19+971.370</td> <td>19+997.860</td> <td>20+000.000</td> </tr> <tr> <td>Curve Data</td> <td></td> <td>L=100.000m</td> <td>L=100.000m</td> <td>L=100.000m</td> <td>L=100.000m</td> <td>L=100.000m</td> <td>L=100.000m</td> <td>L=100.000m</td> <td>L=100.000m</td> <td>L=100.000m</td> <td>L=100.000m</td> <td>L=100.000m</td> <td>L=100.000m</td> <td>L=100.000m</td> <td>L=100.000m</td> <td>L=100.000m</td> <td>L=100.000m</td> <td>L=100.000m</td> <td>L=100.000m</td> <td>L=100.000m</td> <td>L=100.000m</td> <td>L=100.000m</td> <td>L=100.000m</td> <td>L=100.000m</td> <td>L=100.000m</td> <td>L=100.000m</td> <td>L=100.000m</td> <td>L=100.000m</td> <td>L=100.000m</td> <td>L=100.000m</td> <td>L=100.000m</td> <td>L=100.000m</td> <td>L=100.000m</td> <td>L=100.000m</td> <td>L=100.000m</td> <td>L=100.000m</td> <td>L=100.000m</td> <td>L=100.000m</td> <td>L=100.000m</td> <td>L=100.000m</td> </tr> </table> | Station | 19+188.882 | 19+211.117 | 19+233.352 | 19+255.587 | 19+277.822 | 19+300.057 | 19+322.292 | 19+344.527 | 19+366.762 | 19+389.000 | 19+411.235 | 19+433.470 | 19+455.705 | 19+477.940 | 19+500.175 | 19+522.410 | 19+544.645 | 19+566.880 | 19+589.115 | 19+611.350 | 19+633.585 | 19+655.820 | 19+678.055 | 19+700.290 | 19+722.525 | 19+744.760 | 19+767.000 | 19+789.235 | 19+811.470 | 19+833.705 | 19+855.940 | 19+878.175 | 19+900.410 | 19+922.645 | 19+944.880 | 19+971.370 | 19+997.860 | 20+000.000 | Curve Data | | L=100.000m | L=100.000m | L=100.000m | L=100.000m | L=100.000m | L=100.000m | L=100.000m | L=100.000m | L=100.000m | L=100.000m | L=100.000m | L=100.000m | L=100.000m | L=100.000m | L=100.000m | L=100.000m | L=100.000m | L=100.000m | L=100.000m | L=100.000m | L=100.000m | L=100.000m | L=100.000m | L=100.000m | L=100.000m | L=100.000m | L=100.000m | L=100.000m | L=100.000m | L=100.000m | L=100.000m | L=100.000m | L=100.000m | L=100.000m | L=100.000m | L=100.000m | L=100.000m | L=100.000m | L=100.000m | |
| Station | 19+188.882 | 19+211.117 | 19+233.352 | 19+255.587 | 19+277.822 | 19+300.057 | 19+322.292 | 19+344.527 | 19+366.762 | 19+389.000 | 19+411.235 | 19+433.470 | 19+455.705 | 19+477.940 | 19+500.175 | 19+522.410 | 19+544.645 | 19+566.880 | 19+589.115 | 19+611.350 | 19+633.585 | 19+655.820 | 19+678.055 | 19+700.290 | 19+722.525 | 19+744.760 | 19+767.000 | 19+789.235 | 19+811.470 | 19+833.705 | 19+855.940 | 19+878.175 | 19+900.410 | 19+922.645 | 19+944.880 | 19+971.370 | 19+997.860 | 20+000.000 | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| Curve Data | | L=100.000m | L=100.000m | L=100.000m | L=100.000m | L=100.000m | L=100.000m | L=100.000m | L=100.000m | L=100.000m | L=100.000m | L=100.000m | L=100.000m | L=100.000m | L=100.000m | L=100.000m | L=100.000m | L=100.000m | L=100.000m | L=100.000m | L=100.000m | L=100.000m | L=100.000m | L=100.000m | L=100.000m | L=100.000m | L=100.000m | L=100.000m | L=100.000m | L=100.000m | L=100.000m | L=100.000m | L=100.000m | L=100.000m | L=100.000m | L=100.000m | L=100.000m | L=100.000m | L=100.000m | L=100.000m | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| Left Profile Chainage | 19000.000 19020.000 19040.000 19060.000 19080.000 19100.000 19120.000 19140.000 19160.000 19180.000 19200.000 19220.000 19240.000 19260.000 19280.000 19300.000 19320.000 19340.000 19360.000 19380.000 19400.000 19420.000 19440.000 19460.000 19480.000 19500.000 19520.000 19540.000 19560.000 19580.000 19600.000 19620.000 19640.000 19660.000 19680.000 19700.000 19720.000 19740.000 19760.000 19780.000 19800.000 19820.000 19840.000 19860.000 19880.000 19900.000 19920.000 19940.000 19960.000 19980.000 20000.000 | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| Right Profile Chainage | 19000.000 19020.000 19040.000 19060.000 19080.000 19100.000 19120.000 19140.000 19160.000 19180.000 19200.000 19220.000 19240.000 19260.000 19280.000 19300.000 19320.000 19340.000 19360.000 19380.000 19400.000 19420.000 19440.000 19460.000 19480.000 19500.000 19520.000 19540.000 19560.000 19580.000 19600.000 19620.000 19640.000 19660.000 19680.000 19700.000 19720.000 19740.000 19760.000 19780.000 19800.000 19820.000 19840.000 19860.000 19880.000 19900.000 19920.000 19940.000 19960.000 19980.000 20000.000 | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |

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|--|--|---|---|---|
| <p>Legend</p> <p>Proposed Road Line</p> <p>Existing Road Line</p> <p>Right of Way</p> <p>Proposed Level along Right Median Edge</p> <p>Existing Level along Right Median Edge</p> <p>Proposed Level along Left Median Edge</p> <p>Existing Level along Left Median Edge</p> <p>Proposed L.L.</p> <p>Existing L.L.</p> <p>Proposed S.S.</p> <p>Existing S.S.</p> <p>Proposed P.P.</p> <p>Existing P.P.</p> | <p>Project Title</p> <p>Consultancy services for preparation of detailed project report for Rehabilitation and upgrading of existing 3 lane road to 4 lane divided Carriageway configuration of Jammu - Udhampur section of NH-1A (Km 8.00 to Km 64.00) including Jammu(U) Nagina and Udhampur Bypass Phase - II Programme of North-South corridor Project, Contract Package C/8-18</p> | <p>Client</p> <p>NATIONAL HIGHWAYS AUTHORITY OF INDIA (Ministry of Shipping, Road Transport & Highways)</p> | <p>Drawing Title: PLAN & PROFILE Km. 19+000 to 20+000</p> <p>Drawing No.: SW/1019/ROADS/PP-17</p> <p>Scale: HOR:-1:2000 VER:-1:200</p> <p>Date: JUNE-2009</p> | <p>Scott Wilson India Private Limited A-26/4, MOHAN CO-OPERATIVE IND. ESTATE MATHURA ROAD NEW DELHI - 110044 Tel: (011) 41679340 - 49 Fax: (011) 41679350 E-mail: swin@swindia.com</p> |
|--|--|---|---|---|





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|---|
| Level Datum = 246.000 |
| Proposed Level along Left Median Edge (m.) |
| Existing Level Below Left Median Edge (m.) |
| Proposed Level along Right Median Edge (m.) |
| Existing Level Below Right Median Edge (m.) |
| Horizontal Alignment Left |
| Horizontal Alignment Right |
| Vertical Alignment Left |
| Vertical Alignment Right |
| LHS Superelevation |
| RHS Superelevation |
| Left Profile Chainage |
| Right Profile Chainage |

| LEGEND | |
|---|--|
| For Proposed Four Lanes | For Proposed Two Lanes |
| 1. Right of Way | 1. Right of Way |
| 2. Proposed Road | 2. Proposed Road |
| 3. Proposed Land along Left Median Edge | 3. Proposed Land along Right Median Edge |
| 4. Existing Land along Left Median Edge | 4. Existing Land along Right Median Edge |
| 5. Proposed E.C. | 5. Existing Land along Right Median Edge |
| 6. Existing Road | 6. Existing Road |
| 7. Proposed L.S. & P.S. | 7. Proposed L.S. & P.S. |
| 8. Existing L.S. & P.S. | 8. Existing L.S. & P.S. |

Project Title
 Consultancy services for preparation of detailed project report for Rehabilitation and upgrading of existing 2-lane road to 4-lane divided carriageway configuration of Jammu - Udhampur section of NH-1A (Km 0+00 to Km 44+00) including Jammu(Ur) Nagar and Udhampur Bypass Phase - III Programme of North-South corridor Project. Contract Package C-65/11

Site
 The drawing is the property of SCORY WADSE INDIAN PRIVATE LIMITED and must not be issued or be any person or body and published, or be used in any way for the purpose of advertisement, notice, or other in full or part by any person or body without the prior permission in writing.

Client
NATIONAL HIGHWAYS AUTHORITY OF INDIA
 (Ministry of Shipping, Road Transport & Highways)

Drawing Title: PLAN & PROFILE
 Km. 20+000 to 21+000

Drawing No.: SW/1019/ROADS/PP-18
Scale: HOR:-1:2000 VER:-1:200

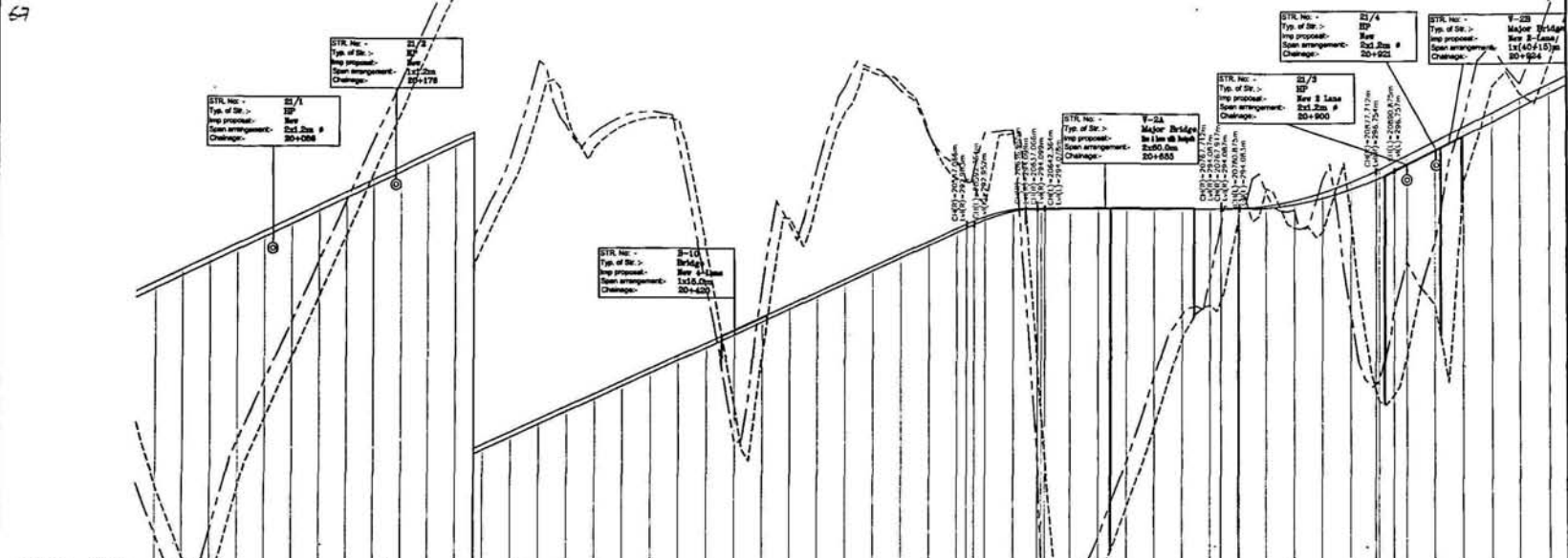
Drn: K.L.N
Dgn: K.PRASAD
Appd: BRIG.C.D.PURI
Date: JUNE-2009

Scott Wilson India Private Limited
 A-26/A, MOHAN CO-OPERATIVE IND. ESTATE
 MATHURA ROAD
 NEW DELHI - 110044
 Tele: (011) 41679340 - 49
 Fax: (011) 41679350
 E-mail: swindia@rediffmail.com



Jammu

Udhampur



| | Level Datum = 245.000 | Level Datum = 268.000 | |
|---|---|---|--------------------|
| Proposed Level along Left Median Edge (m.) | 248.112, 248.279, 252.478, 248.235 | 248.320, 252.821, 247.529, 257.762 | |
| Existing Level Below Left Median Edge (m.) | 249.488, 249.183, 243.331, 242.306, 242.706 | 244.327, 248.432, 252.536, 244.860 | |
| Proposed Level along Right Median Edge (m.) | 252.792, 270.304, 254.184, 248.386 | 252.792, 270.304, 254.184, 248.386 | |
| Existing Level Below Right Median Edge (m.) | 257.488, 272.862, 258.292, 272.206 | 257.488, 272.862, 258.292, 272.206 | |
| Horizontal Alignment Left | R=119.250 L=143.795 | R=85.250 L=138.286 | R=79.250 L=144.248 |
| Horizontal Alignment Right | R=126.750 L=145.263 | R=95.750 L=141.743 | R=82.750 L=147.101 |
| Vertical Alignment Left | G=4.500 L=432.261m | | |
| Vertical Alignment Right | G=4.472 L=433.318m | | |
| D/S Superelevation | | | |
| RHS Superelevation | | | |
| Left Profile Chainage | 20000.000, 20050.000, 20100.000, 20150.000, 20200.000, 20250.000, 20300.000, 20350.000, 20400.000, 20450.000, 20500.000, 20550.000, 20600.000, 20650.000, 20700.000, 20750.000, 20800.000, 20850.000, 20900.000, 20950.000, 21000.000 | | |
| Right Profile Chainage | | 20000.000, 20050.000, 20100.000, 20150.000, 20200.000, 20250.000, 20300.000, 20350.000, 20400.000, 20450.000, 20500.000, 20550.000, 20600.000, 20650.000, 20700.000, 20750.000, 20800.000, 20850.000, 20900.000, 20950.000, 21000.000 | |

| Rev | Description |
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Project Title
 Consultancy services for preparation of detailed project report for Rehabilitation and upgrading of existing 2-lane road to 4-lane divided carriageway configuration of Jammu - Udhampur section of NH-1 A (km 0+00 to km 64.00) including Jammu(Ur) bypass and Udhampur Bypass Phase - II Programme of North - South corridor Project, Contract Package C-07/18

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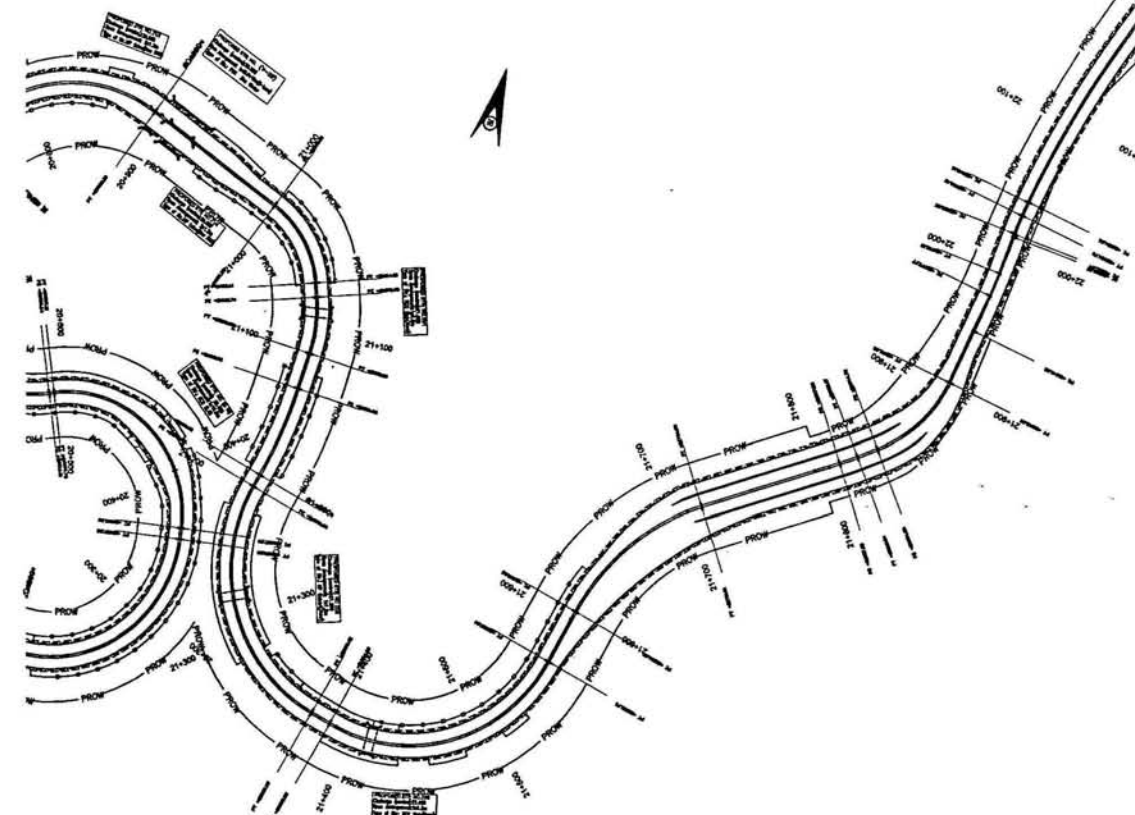
Drawing Title: PLAN & PROFILE
 Kms. 20+000 to 21+000
Drawing No.: SW/1019/ROADS/PP-18
Scale: 1:2000
Date: JUNE-2009

Sopt Wilson India Private Limited
 A-25/4, MOHAN CO-OPERATIVE IND. ESTATE
 MATARA ROAD
 NEW DELHI - 110044
 Tel: (011) 41679340 - 49
 Fax: (011) 41679350
 E-mail: info@swpt.com



← Jammu
65

Udhampur →



Level Datum = 300.000

| |
|---|
| Proposed Level along Left Median Edge (m.) |
| Existing Level Below Left Median Edge (m.) |
| Proposed Level along Right Median Edge (m.) |
| Existing Level Below Right Median Edge (m.) |
| Horizontal Alignment Left |
| Horizontal Alignment Right |
| Vertical Alignment Left |
| Vertical Alignment Right |
| LHS Super-elevation |
| RHS Super-elevation |
| Left Profile Chainage |
| Right Profile Chainage |

| |
|---|
| For Proposed Four Lanes |
| 1. Proposed Left Lane Right Side Slip |
| 2. Proposed Left Lane Left Side Slip |
| 3. Proposed Right Lane Right Side Slip |
| 4. Proposed Right Lane Left Side Slip |
| 5. Proposed Left Lane Right Side Slip |
| 6. Proposed Left Lane Left Side Slip |
| 7. Proposed Right Lane Right Side Slip |
| 8. Proposed Right Lane Left Side Slip |
| 9. Proposed Left Lane Right Side Slip |
| 10. Proposed Left Lane Left Side Slip |
| 11. Proposed Right Lane Right Side Slip |
| 12. Proposed Right Lane Left Side Slip |

Project Title
 Consultancy services for preparation of detailed project report for rehabilitation and upgrading of existing 2-lane road to 4-lane divided carriageway configuration of Jammu - Udhampur section of NH-1A (Km 0+00 to Km 64.00) including Jammu (Part) Nagrova and Udhampur Bypass Phase - III Programme of North-South corridor Project.
 Contract Package C-0211

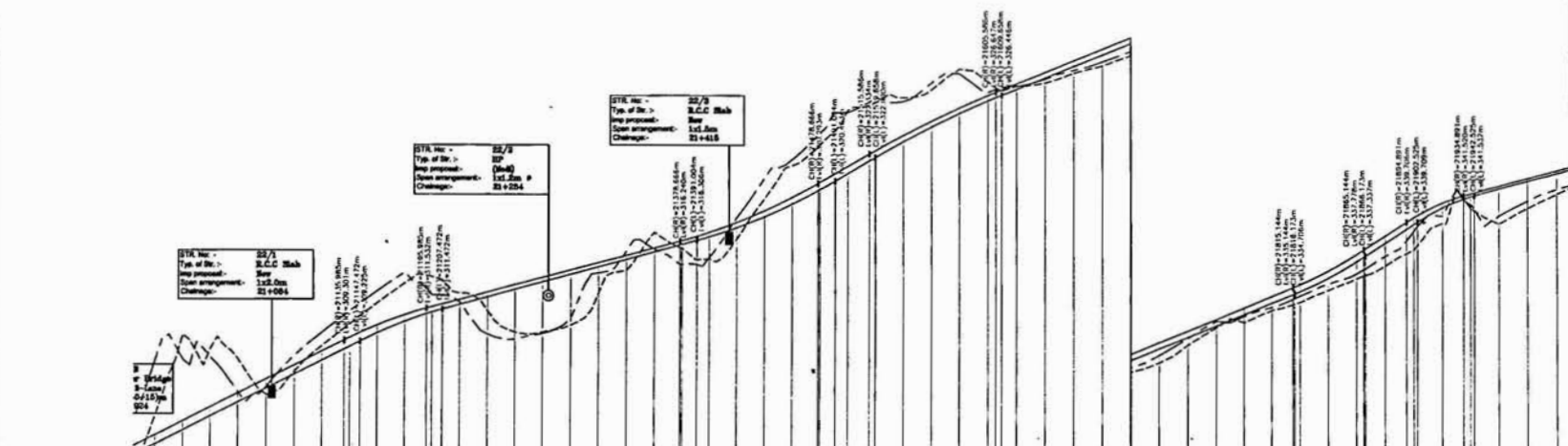
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Client
NATIONAL HIGHWAYS AUTHORITY OF INDIA
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Drawing Title: PLAN & PROFILE
 Km. 21+000 to 22+000
Drawing No.: SW/1019/ROADS/PP - 19
Scale: HDR-1:2000 VER-1:200
Dwg.: K.PRASAD
Appd.: BRIG.C.D.PURI
Date: JUNE-2009

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 K-28/A, MOHAN CO-OPERATIVE IND. ESTATE
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 Fax: (011) 41679350
 E-mail: swindia@swindia.com





| | | | | |
|---------------------------|--|--|---|---|
| Level Datum = 300.000 | Proposed Level along Left Median Edge (m.) | Existing Level Below Left Median Edge (m.) | Proposed Level along Right Median Edge (m.) | Existing Level Below Right Median Edge (m.) |
| Horizontal Alignment Left | Horizontal Alignment Right | | | |
| Vertical Alignment Left | Vertical Alignment Right | | | |
| UHS Super-elevation | RHS Super-elevation | | | |
| Left Profile Chainage | Right Profile Chainage | | | |

LEGEND

| Symbol | For Proposed Plan/Profile |
|--------|---|
| --- | 1. Existing level |
| --- | 2. Proposed level along left median edge |
| --- | 3. Proposed level along right median edge |
| --- | 4. Existing level along left median edge |
| --- | 5. Existing level along right median edge |
| --- | 6. Level datum |

Project Title
 Consultancy services for preparation of detailed project report for Rehabilitation and upgrading of existing 2-lane road to 4-lane divided carriageway configuration of Jammu - Udhampur section of NH-1A (Km 0.00 to Km 64.00) including Jammu(Ur) Nagrota and Udhampur Bypass Phase - III Programme of North - South corridor Project. Contract Package C-0218

Client
NATIONAL HIGHWAYS AUTHORITY OF INDIA
 (Ministry of Shipping, Road Transport & Highways)

Drawing Title: PLAN & PROFILE
 Km. 21+000 to 22+000
Drawing No.: SW/1019/ROADS/PP - 19
Scale: -
 HOR:-1:2000 VER:-1:200
Dm: K.L.N **Dgn.:** K.PRASAD **Appd:** BRG.C.D.PURI **Date:** JUNE-2009

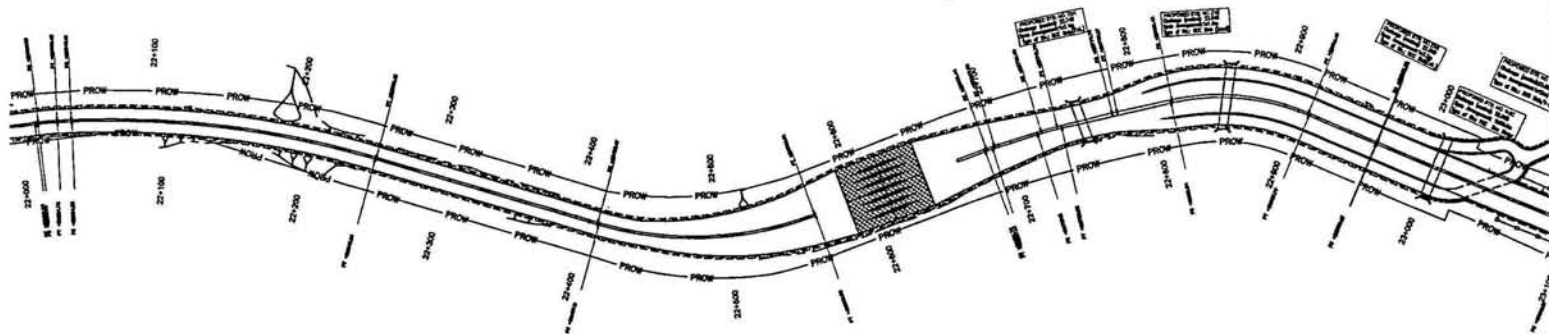
Sheet: 2 of 2
Scott Wilson India Private Limited
 5-26/4, MOHAN CO-OPERATIVE IND. ESTATE
 KATRA, ROAD
 NEW DELHI - 110044
 Tel: (011) 41679340 - 49
 Fax: (011) 41679350
 E-mail: scwilson@scwilson.com



← Jammu

Udhampur →

70



| |
|---|
| Level Datum = 340.000 |
| Proposed Level along Left Median Edge (m.) |
| Existing Level Below Left Median Edge (m.) |
| Proposed Level along Right Median Edge (m.) |
| Existing Level Below Right Median Edge (m.) |
| Horizontal Alignment Left |
| Horizontal Alignment Right |
| Vertical Alignment Left |
| Vertical Alignment Right |
| LHS Superelevation |
| RHS Superelevation |
| Left Profile Chainage |
| Right Profile Chainage |

| Rev. | Description |
|------|----------------|
| 1 | As per drawing |
| 2 | As per drawing |
| 3 | As per drawing |
| 4 | As per drawing |
| 5 | As per drawing |
| 6 | As per drawing |
| 7 | As per drawing |
| 8 | As per drawing |
| 9 | As per drawing |
| 10 | As per drawing |

Project Title
 Consultancy services for preparation of detailed project report for rehabilitation and upgrading of existing 2-lane road to 4-lane divided carriageway configuration of Jammu - Udhampur section of NH-1A (km 6.00 to km 64.00) including Jammu/Part II Nagrova and Udhampur Bypass Phase - III Programme of North-South corridor Project, Contract Package C-02/18

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 The drawings shall be the property of the consultant and shall not be used for any other purpose.

Client
NATIONAL HIGHWAYS AUTHORITY OF INDIA
 (Ministry of Shipping, Road Transport & Highways)

Drawing Title: PLAN & PROFILE
 Km. 22+000 to 23+000
Drawing No.: SW/1019/ROADS/PP - 20
Scale :- HOR:-1:2000 VERT:-1:200
Drawn: K.L.N
Dgn: K.PRASAD
Appd: BRG.C.D.PURI
Date: JUNE-2009

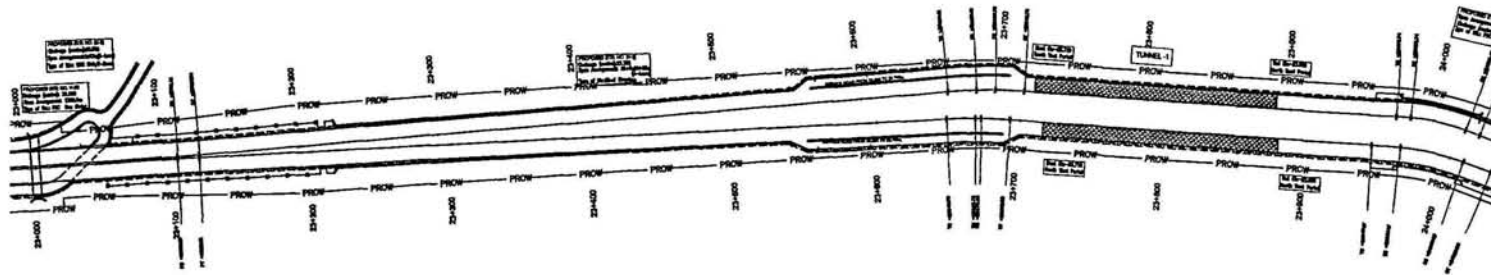
Scott Wilson India Private Limited
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 E-mail : swinfo@swi.com



Jammu

Udhampur

72



Level Datum =372.000

- Proposed Level along Left Median Edge (m)
- Existing Level Below Left Median Edge (m)
- Proposed Level along Right Median Edge (m)
- Existing Level Below Right Median Edge (m)
- Horizontal Alignment Left
- Horizontal Alignment Right
- Vertical Alignment Left
- Vertical Alignment Right
- LHS Superelevation
- RHS Superelevation
- Left Profile Chainage
- Right Profile Chainage

| LEGEND | Proposed | Existing |
|--|--|----------|
| 1. Right of way | 1. Right of way | |
| 2. Right of way | 2. Right of way | |
| 3. Proposed road along Left Median Edge | 3. Proposed road along Left Median Edge | |
| 4. Proposed road along Right Median Edge | 4. Proposed road along Right Median Edge | |
| 5. Proposed R/W | 5. Proposed R/W | |
| 6. Existing R/W | 6. Existing R/W | |
| 7. Existing R/W | 7. Existing R/W | |
| 8. Existing R/W | 8. Existing R/W | |
| 9. Existing R/W | 9. Existing R/W | |
| 10. Existing R/W | 10. Existing R/W | |

Project Title
 Consultancy services for preparation of detailed project report for Subsidisation and upgrading of existing 3-lane road to 4-lane divided Category configuration of Jammu - Udhampur section of NH-1 A (Km 6.89 to km 66.88) including Jammu(Urg) Nagrova and Udhampur System Phase - II Programme of North - South corridor Project, Contract Package C-0218

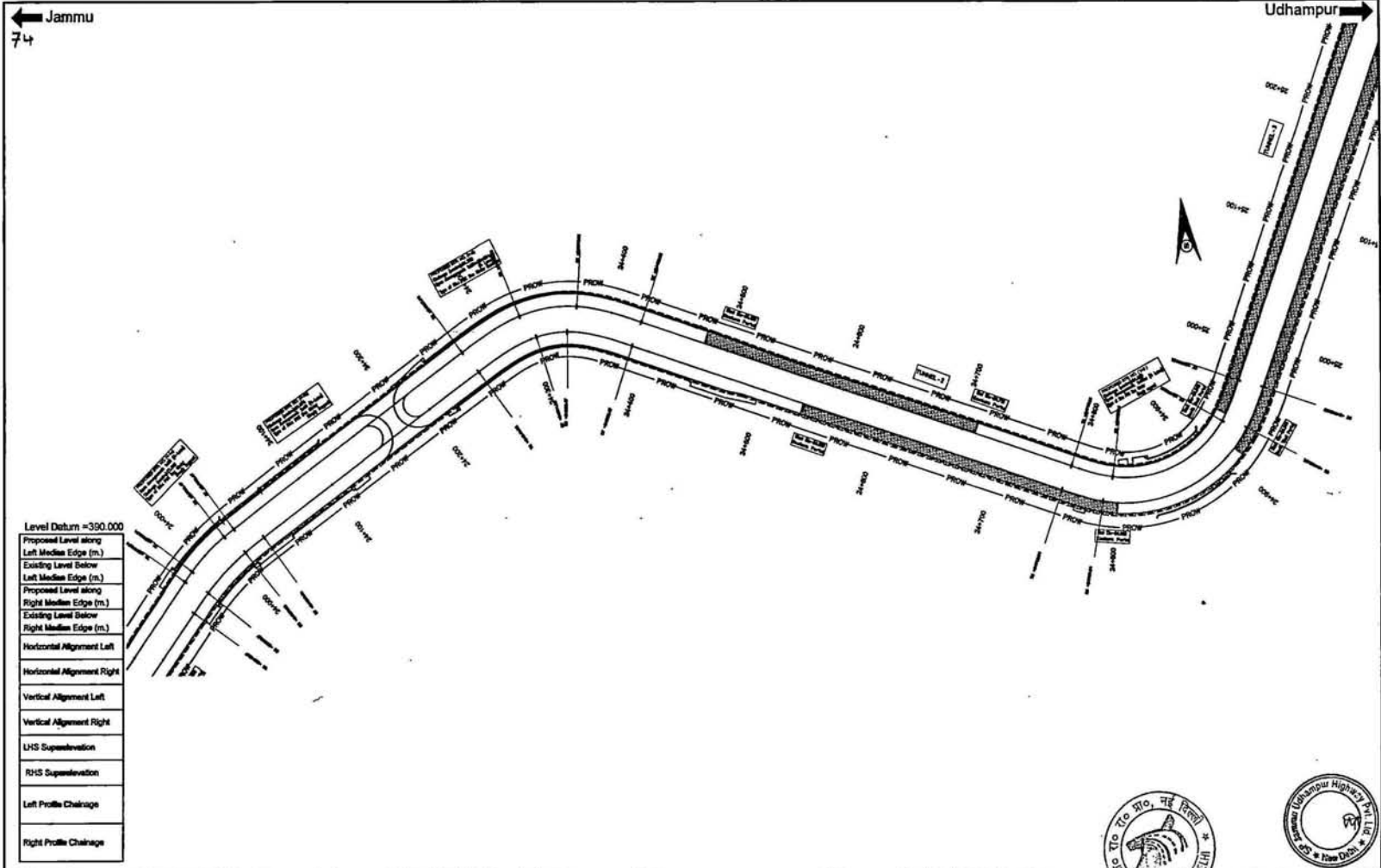
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Client
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 (Ministry of Shipping, Road Transport & Highways)

Drawing Title: PLAN & PROFILE
 Km. 23+000 to 24+000
Drawing No.: SW/1019/ROADS/PP - 21
Scale: HOR-1:2000 VER-1:200
Date: JUNE-2009
Dgn.: K.PRASAD
Appd: BRIG.C.D.PURJ

Scott Wilson India Private Limited
 A-26/4, MOHAWI CO-OPERATIVE IND. ESTATE
 MIDAPURA ROAD
 NEW DELHI - 110044
 Tele: (011) 41679340 - 49
 Fax: (011) 41679350
 E-mail: scwinds@vsnl.com





| 1:50000 | |
|------------------------|------------------------|
| 1. Proposed Four Lanes | 1. Proposed Four Lanes |
| 2. Existing 2 Lanes | 2. Existing 2 Lanes |
| 3. Existing 4 Lanes | 3. Existing 4 Lanes |
| 4. Proposed 2 Lanes | 4. Proposed 2 Lanes |
| 5. Proposed 4 Lanes | 5. Proposed 4 Lanes |
| 6. Proposed 6 Lanes | 6. Proposed 6 Lanes |
| 7. Proposed 8 Lanes | 7. Proposed 8 Lanes |
| 8. Proposed 10 Lanes | 8. Proposed 10 Lanes |
| 9. Proposed 12 Lanes | 9. Proposed 12 Lanes |
| 10. Proposed 14 Lanes | 10. Proposed 14 Lanes |
| 11. Proposed 16 Lanes | 11. Proposed 16 Lanes |
| 12. Proposed 18 Lanes | 12. Proposed 18 Lanes |
| 13. Proposed 20 Lanes | 13. Proposed 20 Lanes |
| 14. Proposed 22 Lanes | 14. Proposed 22 Lanes |
| 15. Proposed 24 Lanes | 15. Proposed 24 Lanes |
| 16. Proposed 26 Lanes | 16. Proposed 26 Lanes |
| 17. Proposed 28 Lanes | 17. Proposed 28 Lanes |
| 18. Proposed 30 Lanes | 18. Proposed 30 Lanes |
| 19. Proposed 32 Lanes | 19. Proposed 32 Lanes |
| 20. Proposed 34 Lanes | 20. Proposed 34 Lanes |
| 21. Proposed 36 Lanes | 21. Proposed 36 Lanes |
| 22. Proposed 38 Lanes | 22. Proposed 38 Lanes |
| 23. Proposed 40 Lanes | 23. Proposed 40 Lanes |
| 24. Proposed 42 Lanes | 24. Proposed 42 Lanes |
| 25. Proposed 44 Lanes | 25. Proposed 44 Lanes |
| 26. Proposed 46 Lanes | 26. Proposed 46 Lanes |
| 27. Proposed 48 Lanes | 27. Proposed 48 Lanes |
| 28. Proposed 50 Lanes | 28. Proposed 50 Lanes |
| 29. Proposed 52 Lanes | 29. Proposed 52 Lanes |
| 30. Proposed 54 Lanes | 30. Proposed 54 Lanes |
| 31. Proposed 56 Lanes | 31. Proposed 56 Lanes |
| 32. Proposed 58 Lanes | 32. Proposed 58 Lanes |
| 33. Proposed 60 Lanes | 33. Proposed 60 Lanes |
| 34. Proposed 62 Lanes | 34. Proposed 62 Lanes |
| 35. Proposed 64 Lanes | 35. Proposed 64 Lanes |
| 36. Proposed 66 Lanes | 36. Proposed 66 Lanes |
| 37. Proposed 68 Lanes | 37. Proposed 68 Lanes |
| 38. Proposed 70 Lanes | 38. Proposed 70 Lanes |
| 39. Proposed 72 Lanes | 39. Proposed 72 Lanes |
| 40. Proposed 74 Lanes | 40. Proposed 74 Lanes |
| 41. Proposed 76 Lanes | 41. Proposed 76 Lanes |
| 42. Proposed 78 Lanes | 42. Proposed 78 Lanes |
| 43. Proposed 80 Lanes | 43. Proposed 80 Lanes |
| 44. Proposed 82 Lanes | 44. Proposed 82 Lanes |
| 45. Proposed 84 Lanes | 45. Proposed 84 Lanes |
| 46. Proposed 86 Lanes | 46. Proposed 86 Lanes |
| 47. Proposed 88 Lanes | 47. Proposed 88 Lanes |
| 48. Proposed 90 Lanes | 48. Proposed 90 Lanes |
| 49. Proposed 92 Lanes | 49. Proposed 92 Lanes |
| 50. Proposed 94 Lanes | 50. Proposed 94 Lanes |
| 51. Proposed 96 Lanes | 51. Proposed 96 Lanes |
| 52. Proposed 98 Lanes | 52. Proposed 98 Lanes |
| 53. Proposed 100 Lanes | 53. Proposed 100 Lanes |

Project Title
 Consultancy services for preparation of detailed project report for Subsidisation and upgrading of existing 2-lane road to 4-lane divided highway condition of Jammu - Udhampur section of NH-1 A.

Site
 Km. 24.00 to Km. 25.00 including Jammu (NH) Nagrova and Udhampur Bypass Phase - III Programme of North-South corridor Project, Contract Package C-2213

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3. The drawing is the property of SCRR and shall remain the property of SCRR and shall not be used for any other purpose without the prior written consent of SCRR.
4. The drawing is the property of SCRR and shall remain the property of SCRR and shall not be used for any other purpose without the prior written consent of SCRR.

Client
NATIONAL HIGHWAYS AUTHORITY OF INDIA
 (Ministry of Shipping, Road Transport & Highways)

Drawing Title: PLAN & PROFILE
 Km. 24+000 to 25+000

Drawing No.: SW/1019/ROADS/PP - 22

Scale: HOR:-1:2000 VER:-1:200

Drn: K.L.N

Dgn: K.PRASAD

Appd: BROG.C.D.PURI

Sheet: 1 of 2

Date: JUNE-2009

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 NEW DELHI - 110044
 Tele: (011) 41679340 - 49
 Fax: (011) 41679350
 E-mail: scoti@wilson.com



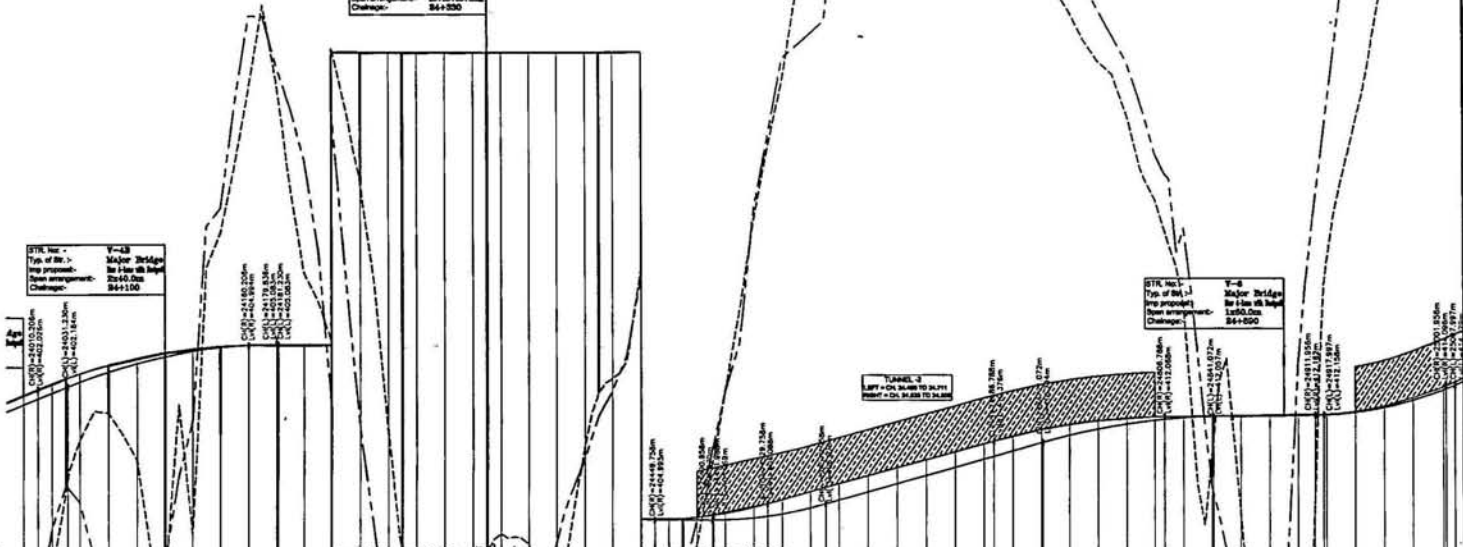
Jammu
75

Udhampur

STR. No. - V-8
Type of Str. - Major Bridge
Span arrangement - 8x4+500
Chaquep - 84+320

STR. No. - V-8
Type of Str. - Major Bridge
Span arrangement - 8x4+500
Chaquep - 84+100

STR. No. - V-8
Type of Str. - Major Bridge
Span arrangement - 8x4+500
Chaquep - 84+500



| | | | |
|---|-----------------------|-----------------------|-----------------------|
| | Level Datum = 390.000 | Level Datum = 370.000 | Level Datum = 402.000 |
| Proposed Level along Left Median Edge (m.) | 390.000 | 370.000 | 402.000 |
| Existing Level Below Left Median Edge (m.) | 385.000 | 365.000 | 398.000 |
| Proposed Level along Right Median Edge (m.) | 390.000 | 370.000 | 402.000 |
| Existing Level Below Right Median Edge (m.) | 385.000 | 365.000 | 398.000 |
| Horizontal Alignment Left | S=100.000 | S=100.000 | S=100.000 |
| Horizontal Alignment Right | S=100.000 | S=100.000 | S=100.000 |
| Vertical Alignment Left | L=100.000m | L=100.000m | L=100.000m |
| Vertical Alignment Right | L=100.000m | L=100.000m | L=100.000m |
| LHS Super-elevation | S=2.000 | S=2.000 | S=2.000 |
| RHS Super-elevation | S=2.000 | S=2.000 | S=2.000 |
| Left Profile Chainage | 24000.000 | 24000.000 | 24000.000 |
| Right Profile Chainage | 24000.000 | 24000.000 | 24000.000 |

| Rev. | Proposed | By | Date |
|------|----------|--------|--------|
| 1 | As per | As per | As per |
| 2 | As per | As per | As per |
| 3 | As per | As per | As per |
| 4 | As per | As per | As per |
| 5 | As per | As per | As per |
| 6 | As per | As per | As per |
| 7 | As per | As per | As per |
| 8 | As per | As per | As per |
| 9 | As per | As per | As per |
| 10 | As per | As per | As per |

Project Title
Consultancy services for preparation of detailed project report for rehabilitation and upgrading of existing 3-lane road to 4-lane divided highway including construction of Jammu - Udhampur section of NH-14 A.
On a fee to be 66.000 (including Jammu-Udhampur section and Udhampur Bypass Phase - II) Preparation of North-South-eastern Project, Contract Package C-0518

Client
NATIONAL HIGHWAYS AUTHORITY OF INDIA
(Ministry of Shipping, Road Transport & Highways)

Drawing Title: PLAN & PROFILE
Km. 24+000 to 25+000
Drawing No.: SWM/1019/ROADS/PP - 22
Scale: HOR - 1:2000 VCR - 1:200



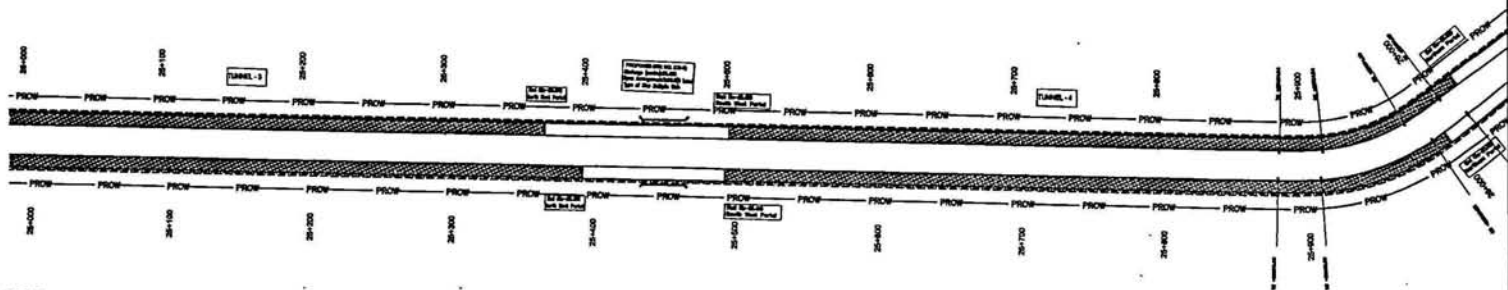
Client: **Scoti Wilson India Private Limited**
MATHURA ROAD
NEW DELHI - 110044
Tel: (011) 41679340 - 48
Fax: (011) 41679350
E-mail: scoti@scoti.com



Jammu

76

Udhampur



Level Datum =412.000

| |
|---|
| Proposed Level along Left Median Edge (m.) |
| Existing Level Below Left Median Edge (m.) |
| Proposed Level along Right Median Edge (m.) |
| Existing Level Below Right Median Edge (m.) |
| Horizontal Alignment Left |
| Horizontal Alignment Right |
| Vertical Alignment Left |
| Vertical Alignment Right |
| LHS Superelevation |
| RHS Superelevation |
| Left Profile Chainage |
| Right Profile Chainage |

| REV | REVISION |
|-----|------------|
| 1 | As per L1 |
| 2 | As per L2 |
| 3 | As per L3 |
| 4 | As per L4 |
| 5 | As per L5 |
| 6 | As per L6 |
| 7 | As per L7 |
| 8 | As per L8 |
| 9 | As per L9 |
| 10 | As per L10 |

Project Title
 Consultancy services for preparation of detailed project report for Rehabilitation and upgrading of existing 2 lane road to 4 lane divided Carriageway configuration of Jammu - Udhampur section of NH-1 A
 Km 8.05 to Km 16.00 including Jammu(Pur) Nagrota and Udhampur Bypass Phase - III Programme of North - South - corridor Project, Central Package C-0318

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Scale: 1. Vertical: 1:1000
 2. Horizontal: 1:1000
 3. Profile: 1:1000
 4. Plan: 1:1000
 5. Section: 1:1000
 6. Elevation: 1:1000
 7. Profile: 1:1000
 8. Plan: 1:1000
 9. Section: 1:1000
 10. Elevation: 1:1000

Client
NATIONAL HIGHWAYS AUTHORITY OF INDIA
 (Ministry of Shipping, Road Transport & Highways)

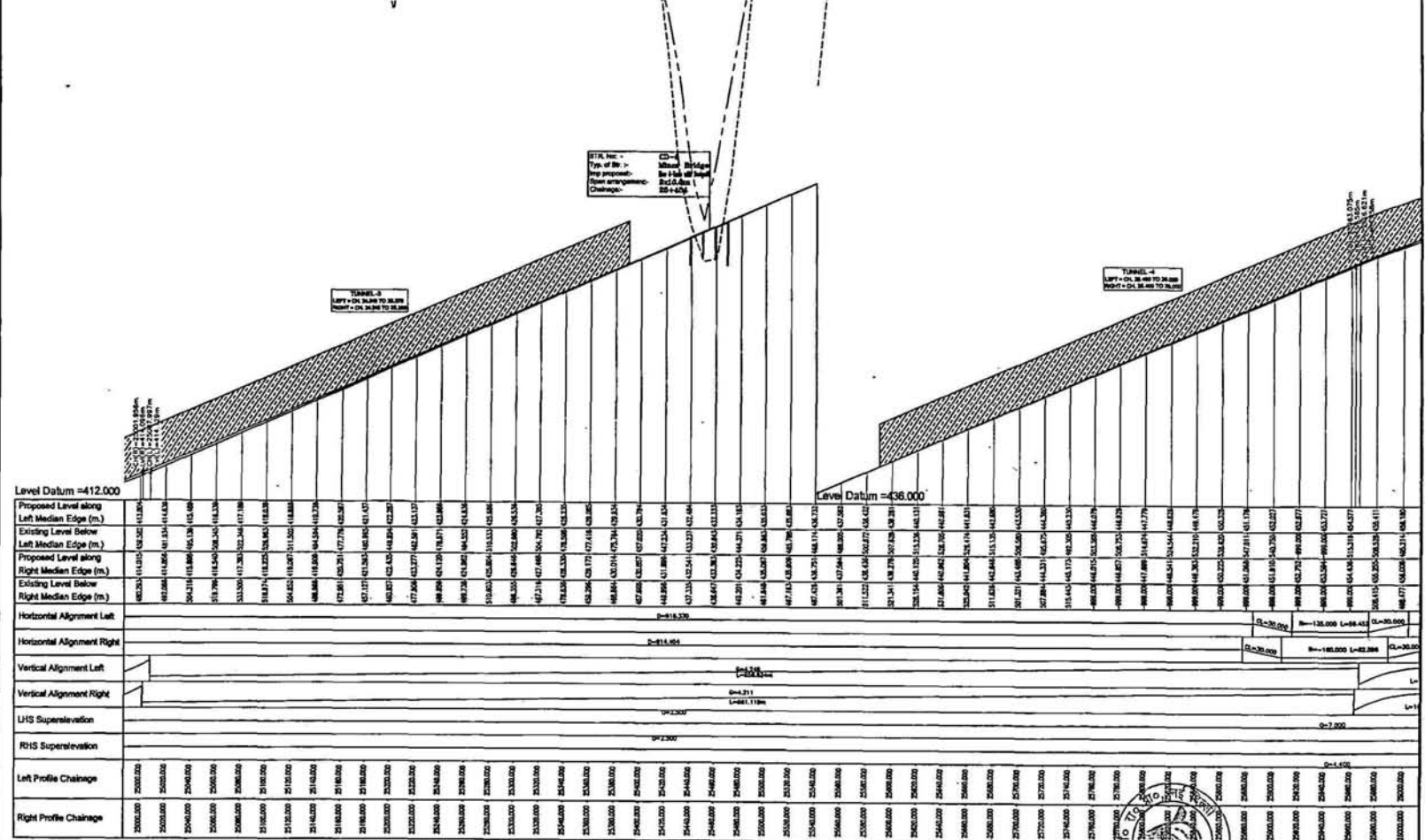
Drawing Title: PLAN & PROFILE
 Km. 25+000 to 28+000
Drawing No.: SW/101B/ROADS/PP-23
Scale: HORIZ-1:2000 VERT-1:200
Drn: K.L.N
Dgn: K.PRASAD
Appd: BRG.C.D.PUR
Date: JUNE-2009

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 MATURBA ROAD
 NEW DELHI - 110044
 Tele: (011) 41679340 - 49
 Fax: (011) 41679350
 E-mail: swindia@gmail.com



Jammu

Udhampur



| | |
|---|---|
| <p>Level Datum = 412.000</p> <p>Proposed Level along Left Median Edge (m.)</p> <p>Existing Level Below Left Median Edge (m.)</p> <p>Proposed Level along Right Median Edge (m.)</p> <p>Existing Level Below Right Median Edge (m.)</p> <p>Horizontal Alignment Left</p> <p>Horizontal Alignment Right</p> <p>Vertical Alignment Left</p> <p>Vertical Alignment Right</p> <p>LHS Super-elevation</p> <p>RHS Super-elevation</p> <p>Left Profile Chainage</p> <p>Right Profile Chainage</p> | <p>Level Datum = 436.000</p> <p>Proposed Level along Left Median Edge (m.)</p> <p>Existing Level Below Left Median Edge (m.)</p> <p>Proposed Level along Right Median Edge (m.)</p> <p>Existing Level Below Right Median Edge (m.)</p> <p>Horizontal Alignment Left</p> <p>Horizontal Alignment Right</p> <p>Vertical Alignment Left</p> <p>Vertical Alignment Right</p> <p>LHS Super-elevation</p> <p>RHS Super-elevation</p> <p>Left Profile Chainage</p> <p>Right Profile Chainage</p> |
|---|---|

| | | | | | |
|--|--|--|--|---|---|
| <p>REVISIONS</p> <p>For Proposed Work</p> <p>1. ...</p> <p>2. ...</p> <p>3. ...</p> <p>4. ...</p> <p>5. ...</p> | <p>Project Title</p> <p>Construction services for preparation of detailed project report for Rehabilitation and upgrading of existing 2-lane road to 4-lane divided carriageway and provision of Jammu - Udhampur section of NH-1A</p> <p>Span 5.00 to be 64.00 including Jammu/Udhampur and Udhampur bypass Phase - II Programme of North-South corridor Project, Connect Package-C-0116</p> | <p>Client</p> <p>NATIONAL HIGHWAYS AUTHORITY OF INDIA</p> <p>(Ministry of Shipping, Road Transport & Highways)</p> | <p>Drawing Title: PLAN & PROFILE</p> <p>Drawing No.: SWV1019/ROADS/PP-23</p> <p>Scale: 1:2000</p> <p>Date: JUNE-2009</p> | <p>Sheet: 2 of 2</p> <p>Author: K.P. PRASAD</p> <p>Appd: BRIG. C.D. PURI</p> | <p>Scott Wilson India Private Limited</p> <p>26/4, MOHAN CO-OPERATIVE IND. ESTATE</p> <p>MATHURA ROAD</p> <p>NEW DELHI - 110044</p> <p>Tel: (011) 41679340 - 49</p> <p>Fac: (011) 41679350</p> <p>E-mail: scwinfo@swil.com</p> |
|--|--|--|--|---|---|

← Jammu
7B

Udhampur →



Level Datum =454.000

| |
|---|
| Proposed Level along Left Median Edge (m.) |
| Existing Level Below Left Median Edge (m.) |
| Proposed Level along Right Median Edge (m.) |
| Existing Level Below Right Median Edge (m.) |
| Horizontal Alignment Left |
| Horizontal Alignment Right |
| Vertical Alignment Left |
| Vertical Alignment Right |
| LHS Superelevation |
| RHS Superelevation |
| Left Profile Chainage |
| Right Profile Chainage |

| LEGEND | |
|---|-------|
| For Proposed Road Layout | |
| 1. Existing G.S. of alignment | _____ |
| 2. Proposed Road along left Median Edge | _____ |
| 3. Proposed Road along Right Median Edge | _____ |
| 4. Existing Level Below Left Median Edge | _____ |
| 5. Existing Level Below Right Median Edge | _____ |
| 6. Proposed L.S. | _____ |
| 7. Proposed R.S. | _____ |
| 8. Existing G.S. of Road | _____ |
| 9. Right of Way | _____ |

Project Title
 Consultancy services for preparation of detailed project report for Rehabilitation and upgrading of existing 2-lane road to 4-lane divided carriageway configuration of Jammu - Udhampur section of NH-1 A. (km 0.00 to km 66.00) including Jammu/Parit Nagar and Udhampur Bypass Phase - III Programme of North - South corridor Project, Contract Package C-03/18

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Scale: 1. Vertical scale 1:10 m and 1:20 m (as shown) 2. The drawing scale is 1:10 m and 1:20 m (as shown) 3. The drawing scale is 1:10 m and 1:20 m (as shown) 4. The drawing scale is 1:10 m and 1:20 m (as shown) 5. The drawing scale is 1:10 m and 1:20 m (as shown) 6. The drawing scale is 1:10 m and 1:20 m (as shown) 7. The drawing scale is 1:10 m and 1:20 m (as shown) 8. The drawing scale is 1:10 m and 1:20 m (as shown) 9. The drawing scale is 1:10 m and 1:20 m (as shown) 10. The drawing scale is 1:10 m and 1:20 m (as shown)

Client
NATIONAL HIGHWAYS AUTHORITY OF INDIA
 (Ministry of Shipping, Road Transport & Highways)

Drawing Title: PLAN & PROFILE
 Km. 26+000 to 27+000

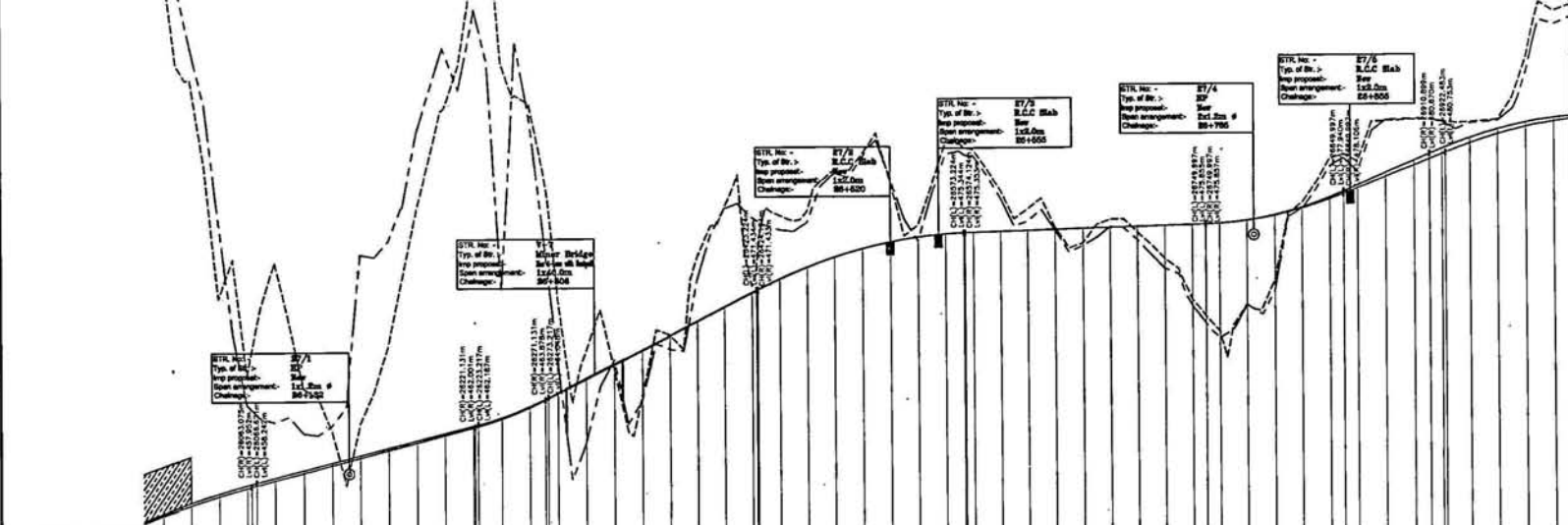
Drawing No.: SW/1019/ROADS/PP-24
 Sheet: 1 of 2

Scale: HOR-1:2000 VER-1:200

Des.: K.PRASAD
Appd.: BRIG.C.D.PURI
Date: JUNE-2009

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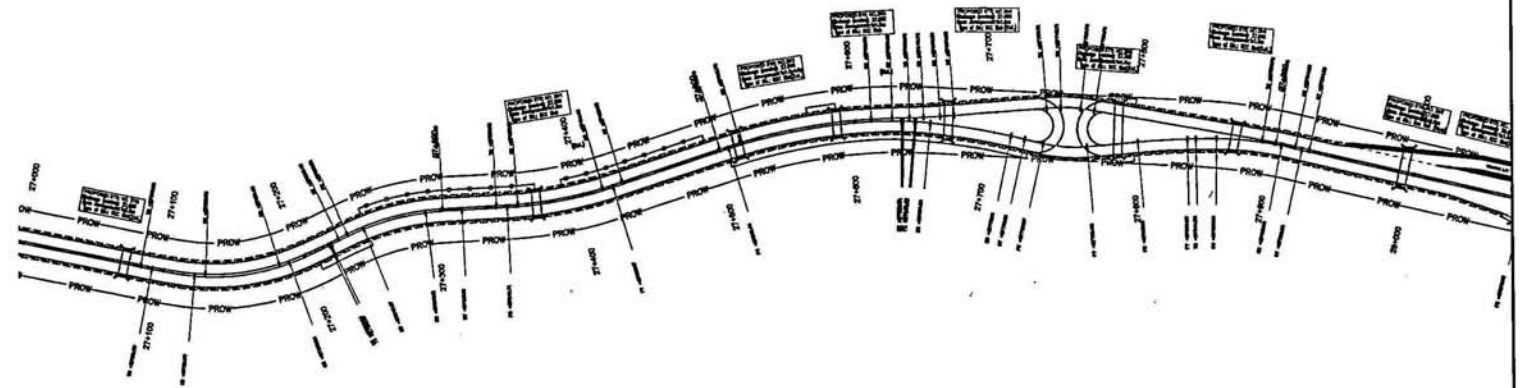


Level Datum = 454.000

| Stationing | Left Median Edge (m) | Existing Level Below Left Median Edge (m) | Proposed Level along Right Median Edge (m) | Existing Level Below Right Median Edge (m) | Horizontal Alignment Left | Horizontal Alignment Right | Vertical Alignment Left | Vertical Alignment Right | LHS Super-elevation | RHS Super-elevation | Left Profile Chainage | Right Profile Chainage |
|------------|----------------------|---|--|--|---------------------------|----------------------------|-------------------------|--------------------------|---------------------|---------------------|-----------------------|------------------------|
| 100+000 | 465.110 | 465.110 | 465.110 | 465.110 | | | | | | | 26000.000 | 26000.000 |
| 100+250 | 465.110 | 465.110 | 465.110 | 465.110 | | | | | | | 26020.000 | 26020.000 |
| 100+500 | 465.110 | 465.110 | 465.110 | 465.110 | | | | | | | 26040.000 | 26040.000 |
| 100+750 | 465.110 | 465.110 | 465.110 | 465.110 | | | | | | | 26060.000 | 26060.000 |
| 101+000 | 465.110 | 465.110 | 465.110 | 465.110 | | | | | | | 26080.000 | 26080.000 |
| 101+250 | 465.110 | 465.110 | 465.110 | 465.110 | | | | | | | 26100.000 | 26100.000 |
| 101+500 | 465.110 | 465.110 | 465.110 | 465.110 | | | | | | | 26120.000 | 26120.000 |
| 101+750 | 465.110 | 465.110 | 465.110 | 465.110 | | | | | | | 26140.000 | 26140.000 |
| 102+000 | 465.110 | 465.110 | 465.110 | 465.110 | | | | | | | 26160.000 | 26160.000 |
| 102+250 | 465.110 | 465.110 | 465.110 | 465.110 | | | | | | | 26180.000 | 26180.000 |
| 102+500 | 465.110 | 465.110 | 465.110 | 465.110 | | | | | | | 26200.000 | 26200.000 |
| 102+750 | 465.110 | 465.110 | 465.110 | 465.110 | | | | | | | 26220.000 | 26220.000 |
| 103+000 | 465.110 | 465.110 | 465.110 | 465.110 | | | | | | | 26240.000 | 26240.000 |
| 103+250 | 465.110 | 465.110 | 465.110 | 465.110 | | | | | | | 26260.000 | 26260.000 |
| 103+500 | 465.110 | 465.110 | 465.110 | 465.110 | | | | | | | 26280.000 | 26280.000 |
| 103+750 | 465.110 | 465.110 | 465.110 | 465.110 | | | | | | | 26300.000 | 26300.000 |
| 104+000 | 465.110 | 465.110 | 465.110 | 465.110 | | | | | | | 26320.000 | 26320.000 |
| 104+250 | 465.110 | 465.110 | 465.110 | 465.110 | | | | | | | 26340.000 | 26340.000 |
| 104+500 | 465.110 | 465.110 | 465.110 | 465.110 | | | | | | | 26360.000 | 26360.000 |
| 104+750 | 465.110 | 465.110 | 465.110 | 465.110 | | | | | | | 26380.000 | 26380.000 |
| 105+000 | 465.110 | 465.110 | 465.110 | 465.110 | | | | | | | 26400.000 | 26400.000 |
| 105+250 | 465.110 | 465.110 | 465.110 | 465.110 | | | | | | | 26420.000 | 26420.000 |
| 105+500 | 465.110 | 465.110 | 465.110 | 465.110 | | | | | | | 26440.000 | 26440.000 |
| 105+750 | 465.110 | 465.110 | 465.110 | 465.110 | | | | | | | 26460.000 | 26460.000 |
| 106+000 | 465.110 | 465.110 | 465.110 | 465.110 | | | | | | | 26480.000 | 26480.000 |
| 106+250 | 465.110 | 465.110 | 465.110 | 465.110 | | | | | | | 26500.000 | 26500.000 |
| 106+500 | 465.110 | 465.110 | 465.110 | 465.110 | | | | | | | 26520.000 | 26520.000 |
| 106+750 | 465.110 | 465.110 | 465.110 | 465.110 | | | | | | | 26540.000 | 26540.000 |
| 107+000 | 465.110 | 465.110 | 465.110 | 465.110 | | | | | | | 26560.000 | 26560.000 |
| 107+250 | 465.110 | 465.110 | 465.110 | 465.110 | | | | | | | 26580.000 | 26580.000 |
| 107+500 | 465.110 | 465.110 | 465.110 | 465.110 | | | | | | | 26600.000 | 26600.000 |
| 107+750 | 465.110 | 465.110 | 465.110 | 465.110 | | | | | | | 26620.000 | 26620.000 |
| 108+000 | 465.110 | 465.110 | 465.110 | 465.110 | | | | | | | 26640.000 | 26640.000 |
| 108+250 | 465.110 | 465.110 | 465.110 | 465.110 | | | | | | | 26660.000 | 26660.000 |
| 108+500 | 465.110 | 465.110 | 465.110 | 465.110 | | | | | | | 26680.000 | 26680.000 |
| 108+750 | 465.110 | 465.110 | 465.110 | 465.110 | | | | | | | 26700.000 | 26700.000 |
| 109+000 | 465.110 | 465.110 | 465.110 | 465.110 | | | | | | | 26720.000 | 26720.000 |
| 109+250 | 465.110 | 465.110 | 465.110 | 465.110 | | | | | | | 26740.000 | 26740.000 |
| 109+500 | 465.110 | 465.110 | 465.110 | 465.110 | | | | | | | 26760.000 | 26760.000 |
| 109+750 | 465.110 | 465.110 | 465.110 | 465.110 | | | | | | | 26780.000 | 26780.000 |
| 110+000 | 465.110 | 465.110 | 465.110 | 465.110 | | | | | | | 26800.000 | 26800.000 |
| 110+250 | 465.110 | 465.110 | 465.110 | 465.110 | | | | | | | 26820.000 | 26820.000 |
| 110+500 | 465.110 | 465.110 | 465.110 | 465.110 | | | | | | | 26840.000 | 26840.000 |
| 110+750 | 465.110 | 465.110 | 465.110 | 465.110 | | | | | | | 26860.000 | 26860.000 |
| 111+000 | 465.110 | 465.110 | 465.110 | 465.110 | | | | | | | 26880.000 | 26880.000 |
| 111+250 | 465.110 | 465.110 | 465.110 | 465.110 | | | | | | | 26900.000 | 26900.000 |
| 111+500 | 465.110 | 465.110 | 465.110 | 465.110 | | | | | | | 26920.000 | 26920.000 |
| 111+750 | 465.110 | 465.110 | 465.110 | 465.110 | | | | | | | 26940.000 | 26940.000 |
| 112+000 | 465.110 | 465.110 | 465.110 | 465.110 | | | | | | | 26960.000 | 26960.000 |
| 112+250 | 465.110 | 465.110 | 465.110 | 465.110 | | | | | | | 26980.000 | 26980.000 |
| 112+500 | 465.110 | 465.110 | 465.110 | 465.110 | | | | | | | 27000.000 | 27000.000 |
| 112+750 | 465.110 | 465.110 | 465.110 | 465.110 | | | | | | | 27020.000 | 27020.000 |
| 113+000 | 465.110 | 465.110 | 465.110 | 465.110 | | | | | | | 27040.000 | 27040.000 |
| 113+250 | 465.110 | 465.110 | 465.110 | 465.110 | | | | | | | 27060.000 | 27060.000 |
| 113+500 | 465.110 | 465.110 | 465.110 | 465.110 | | | | | | | 27080.000 | 27080.000 |
| 113+750 | 465.110 | 465.110 | 465.110 | 465.110 | | | | | | | 27100.000 | 27100.000 |
| 114+000 | 465.110 | 465.110 | 465.110 | 465.110 | | | | | | | 27120.000 | 27120.000 |
| 114+250 | 465.110 | 465.110 | 465.110 | 465.110 | | | | | | | 27140.000 | 27140.000 |
| 114+500 | 465.110 | 465.110 | 465.110 | 465.110 | | | | | | | 27160.000 | 27160.000 |
| 114+750 | 465.110 | 465.110 | 465.110 | 465.110 | | | | | | | 27180.000 | 27180.000 |
| 115+000 | 465.110 | 465.110 | 465.110 | 465.110 | | | | | | | 27200.000 | 27200.000 |
| 115+250 | 465.110 | 465.110 | 465.110 | 465.110 | | | | | | | 27220.000 | 27220.000 |
| 115+500 | 465.110 | 465.110 | 465.110 | 465.110 | | | | | | | 27240.000 | 27240.000 |
| 115+750 | 465.110 | 465.110 | 465.110 | 465.110 | | | | | | | 27260.000 | 27260.000 |
| 116+000 | 465.110 | 465.110 | 465.110 | 465.110 | | | | | | | 27280.000 | 27280.000 |
| 116+250 | 465.110 | 465.110 | 465.110 | 465.110 | | | | | | | 27300.000 | 27300.000 |
| 116+500 | 465.110 | 465.110 | 465.110 | 465.110 | | | | | | | 27320.000 | 27320.000 |
| 116+750 | 465.110 | 465.110 | 465.110 | 465.110 | | | | | | | 27340.000 | 27340.000 |
| 117+000 | 465.110 | 465.110 | 465.110 | 465.110 | | | | | | | 27360.000 | 27360.000 |
| 117+250 | 465.110 | 465.110 | 465.110 | 465.110 | | | | | | | 27380.000 | 27380.000 |
| 117+500 | 465.110 | 465.110 | 465.110 | 465.110 | | | | | | | 27400.000 | 27400.000 |
| 117+750 | 465.110 | 465.110 | 465.110 | 465.110 | | | | | | | 27420.000 | 27420.000 |
| 118+000 | 465.110 | 465.110 | 465.110 | 465.110 | | | | | | | 27440.000 | 27440.000 |
| 118+250 | 465.110 | 465.110 | 465.110 | 465.110 | | | | | | | 27460.000 | 27460.000 |
| 118+500 | 465.110 | 465.110 | 465.110 | 465.110 | | | | | | | 27480.000 | 27480.000 |
| 118+750 | 465.110 | 465.110 | 465.110 | 465.110 | | | | | | | 27500.000 | 27500.000 |
| 119+000 | 465.110 | 465.110 | 465.110 | 465.110 | | | | | | | 27520.000 | 27520.000 |
| 119+250 | 465.110 | 465.110 | 465.110 | 465.110 | | | | | | | 27540.000 | 27540.000 |
| 119+500 | 465.110 | 465.110 | 465.110 | 465.110 | | | | | | | 27560.000 | 27560.000 |
| 119+750 | 465.110 | 465.110 | 465.110 | 465.110 | | | | | | | 27580.000 | 27580.000 |
| 120+000 | 465.110 | 465.110 | 465.110 | 465.110 | | | | | | | 27600.000 | 27600.000 |
| 120+250 | 465.110 | 465.110 | 465.110 | 465.110 | | | | | | | 27620.000 | 27620.000 |
| 120+500 | 465.110 | 465.110 | 465.110 | 465.110 | | | | | | | 27640.000 | 27640.000 |
| 120+750 | 465.110 | 465.110 | 465.110 | 465.110 | | | | | | | 27660.000 | 27660.000 |
| 121+000 | 465.110 | 465.110 | 465.110 | 465.110 | | | | | | | 27680.000 | 27680.000 |
| 121+250 | 465.110 | 465.110 | 465.110 | 465.110 | | | | | | | 27700.000 | 27700.000 |
| 121+500 | 465.110 | 465.110 | 465.110 | 465.110 | | | | | | | 27720.000 | 27720.000 |
| 121+750 | 465.110 | 465.110 | 465.110 | 465.110 | | | | | | | 27740.000 | 27740.000 |
| 122+000 | 465.110 | 465.110 | 465.110 | 465.110 | | | | | | | 27760.000 | 27760.000 |
| 122+250 | 465.110 | 465.110 | 465.110 | 465.110 | | | | | | | 27780.000 | 27780.000 |
| 122+500 | 465.110 | 465.110 | 465.110 | 465.110 | | | | | | | 27800.000 | 27800.000 |
| 122+750 | 465.110 | 465.110 | 465.110 | 465.110 | | | | | | | 27820.000 | 27820.000 |
| 123+000 | 465.110 | 465.110 | 465.110 | 465.110 | | | | | | | 27840.000 | 27840.000 |
| 123+250 | 465.110 | 465.110 | 465.110 | 465.110 | | | | | | | 27860.000 | 27860.000 |
| 123+500 | 465.110 | 465.110 | 465.110 | 465.110 | | | | | | | 27880.000 | 27880.000 |
| 123+750 | 465.110 | 465.110 | 465.110 | 465.110 | | | | | | | 27900.000 | 27900.000 |
| 124+000 | 465.110 | 465.110 | 465.110 | 465.110 | | | | | | | 27920.000 | 27920.000 |
| 124+250 | 465.110 | 465.110 | 465.110 | 465.110 | | | | | | | 27940.000 | 27940.000 |
| 124+500 | 465.110 | 465.110 | 465.110 | 465.110 | | | | | | | 27960.000 | 27960.000 |
| 124+750 | 465.110 | 465.110 | 465.110 | 465.110 | | | | | | | 27980.000 | 27980.000 |
| 125+000 | 465.110 | 465.110 | 465.110 | 465.110 | | | | | | | 28000.000 | 28000.000 |
| 125+250 | 465.110 | 465.110 | 465.110 | 465.110 | | | | | | | 28020.000 | 28020.000 |
| 125+500 | 465.110 | 465.110 | | | | | | | | | | |

← Jammu
80

Udhampur →



Level Datum =470.000

| |
|---|
| Proposed Level along Left Median Edge (m.) |
| Existing Level Below Left Median Edge (m.) |
| Proposed Level along Right Median Edge (m.) |
| Existing Level Below Right Median Edge (m.) |
| Horizontal Alignment Left |
| Horizontal Alignment Right |
| Vertical Alignment Left |
| Vertical Alignment Right |
| LHS Superelevation |
| RHS Superelevation |
| Left Profile Chainage |
| Right Profile Chainage |

| LEGEND: | |
|--------------|---|
| For Proposed | From Existing |
| Ch | 1. Existing G.L. of embankment |
| 1 | 2. Proposed level along left Median Edge |
| 2 | 3. Proposed level along right Median Edge |
| 3 | 4. Existing level below left Median Edge |
| 4 | 5. Existing level below right Median Edge |
| 5 | 6. Proposed L.L. |
| 6 | 7. Proposed H.W. level |
| 7 | 8. Existing H.W. level |

Project Title
 Consultancy services for preparation of detailed project report for Rehabilitation and upgrading of existing 2-lane road to 4-lane divided carriageway configuration of Jammu - Udhampur section of NH-1 A (km 0.00 to km 66.00) including Jammu/Chard/Nagaria and Udhampur bypass Phase - II Programme of North-South corridor Project, Contract Package C-0318

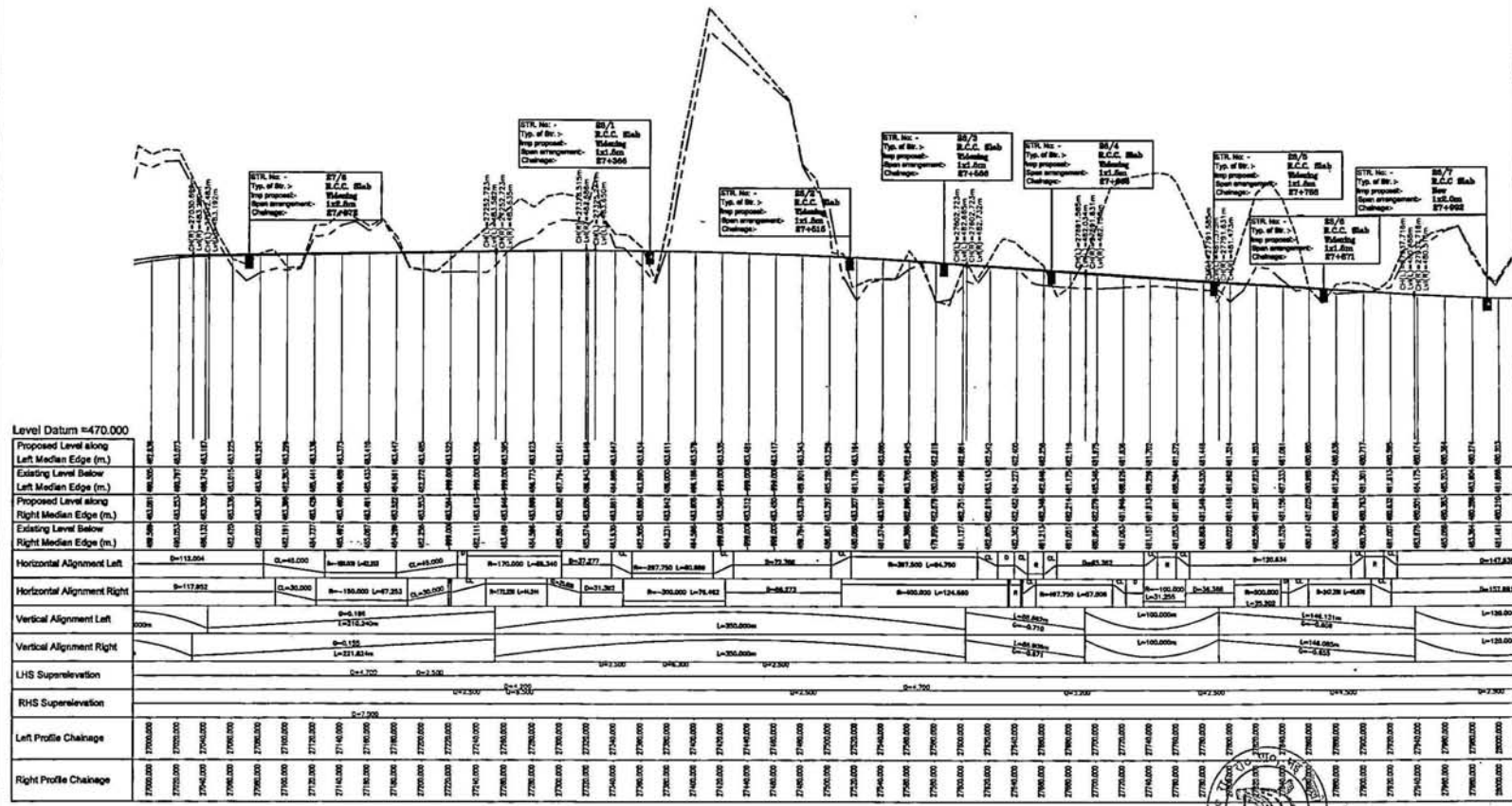
Notes:
 1. The drawings are to be prepared on the basis of the data furnished by the client and the field data collected by the consultant. The consultant shall be responsible for the accuracy of the data furnished by the client and the field data collected by the consultant.
 2. The drawings shall be prepared on the basis of the data furnished by the client and the field data collected by the consultant. The consultant shall be responsible for the accuracy of the data furnished by the client and the field data collected by the consultant.
 3. The drawings shall be prepared on the basis of the data furnished by the client and the field data collected by the consultant. The consultant shall be responsible for the accuracy of the data furnished by the client and the field data collected by the consultant.
 4. The drawings shall be prepared on the basis of the data furnished by the client and the field data collected by the consultant. The consultant shall be responsible for the accuracy of the data furnished by the client and the field data collected by the consultant.

Client
NATIONAL HIGHWAYS AUTHORITY OF INDIA
 (Ministry of Shipping, Road Transport & Highways)

Drawing Title: PLAN & PROFILE
 Km. 27+000 to 28+000
Drawing No.: SW/M019/ROADS/PP-25
Scale: HOR-1:2000 VER-1:200
Dm: K.L.N. **Dgn:** K.PRASAD **Appd:** BRG.C.D.PURU **Date:** JUNE-2009

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| Item | Quantity | Unit | Rate | Amount |
|---------------------|----------|------|------|--------|
| 1. Survey | | | | |
| 2. Preliminary work | | | | |
| 3. Right of Way | | | | |
| 4. Earthwork | | | | |
| 5. Road metal | | | | |
| 6. Drainage | | | | |
| 7. Fencing | | | | |
| 8. Miscellaneous | | | | |
| 9. Contingency | | | | |
| 10. Total | | | | |

Project Title
 Rehabilitation and upgrading of existing 2-lane and 4-lane divided highway on alignment of Jammu - Udhampur section of NH-1 A (Km 0.00 to Km 66.95) including Jammu (P.O.) Nagaur and Udhampur Bypass Phase - II (Programme of North-South corridor Project, Contract Package C-2/11)

Client
NATIONAL HIGHWAYS AUTHORITY OF INDIA
 (Ministry of Shipping, Road Transport & Highways)

Drawing Title: PLAN & PROFILE
 Km. 27+000 to 28+000

Drawing No.: SW/101/ROADS/PP-25
 2 of 2

Scale: HORIZ-1:2000 VERT-1:200

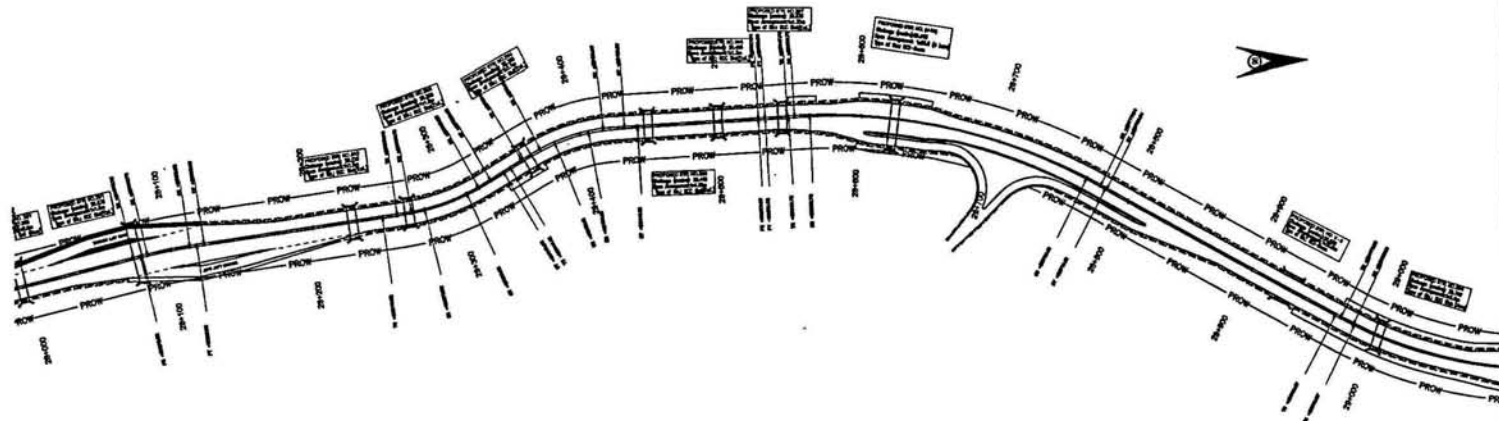
Drawn by: K.L.N
Checked by: K.PRASAD
Appd. by: BRG.C.D.PURI
Date: JUNE-2009

Client's Stamp: NATIONAL HIGHWAYS AUTHORITY OF INDIA
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 E-mail: nhai@nhai.com

Consultant's Stamp: Scott Wilson India Private Limited
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 NEW DELHI - 110044
 Tel: (011) 41673140 - 49
 Fax: (011) 41673350
 E-mail: swi@swi.com

← Jammu
82

Udhampur →



Level Datum = 458.000

| |
|---|
| Proposed Level along Left Median Edge (m.) |
| Existing Level Below Left Median Edge (m.) |
| Proposed Level along Right Median Edge (m.) |
| Existing Level Below Right Median Edge (m.) |
| Horizontal Alignment Left |
| Horizontal Alignment Right |
| Vertical Alignment Left |
| Vertical Alignment Right |
| LHS Superelevation |
| RHS Superelevation |
| Left Profile Chainage |
| Right Profile Chainage |

| LEGEND | |
|-------------------------------------|-------------------------------------|
| For Proposed Four Lanes | For Proposed Two Lanes |
| 1. Right of Way | 1. Right of Way |
| 2. Proposed Road | 2. Proposed Road |
| 3. Proposed Median | 3. Proposed Median |
| 4. Proposed Lanes | 4. Proposed Lanes |
| 5. Proposed Footpath | 5. Proposed Footpath |
| 6. Proposed Drainage | 6. Proposed Drainage |
| 7. Proposed Level | 7. Proposed Level |
| 8. Proposed Existing Level | 8. Proposed Existing Level |
| 9. Proposed Horizontal Alignment | 9. Proposed Horizontal Alignment |
| 10. Proposed Vertical Alignment | 10. Proposed Vertical Alignment |
| 11. Proposed LHS Superelevation | 11. Proposed LHS Superelevation |
| 12. Proposed RHS Superelevation | 12. Proposed RHS Superelevation |
| 13. Proposed Left Profile Chainage | 13. Proposed Left Profile Chainage |
| 14. Proposed Right Profile Chainage | 14. Proposed Right Profile Chainage |

Project Title
 Consultancy services for preparation of detailed project report for Rehabilitation and upgrading of existing 2-lane road to 4-lane 8-ft wide Carriageway along Jammu - Udhampur section of NH-11 A (Km 0.00 to Km 66.00) including Jammu(Ur) Bypass and Udhampur Bypass Phase - III Programme of North-South corridor Project, Contract Package C-0911

Notes:
 1. The drawings are the property of the Client and shall not be used for any other purpose without the written permission of the Client.
 2. The drawings shall be used for the purpose only for which they are intended and shall not be used for any other purpose without the written permission of the Client.
 3. The drawings shall be used for the purpose only for which they are intended and shall not be used for any other purpose without the written permission of the Client.
 4. The drawings shall be used for the purpose only for which they are intended and shall not be used for any other purpose without the written permission of the Client.

Client
NATIONAL HIGHWAYS AUTHORITY OF INDIA
 (Ministry of Shipping, Road Transport & Highways)

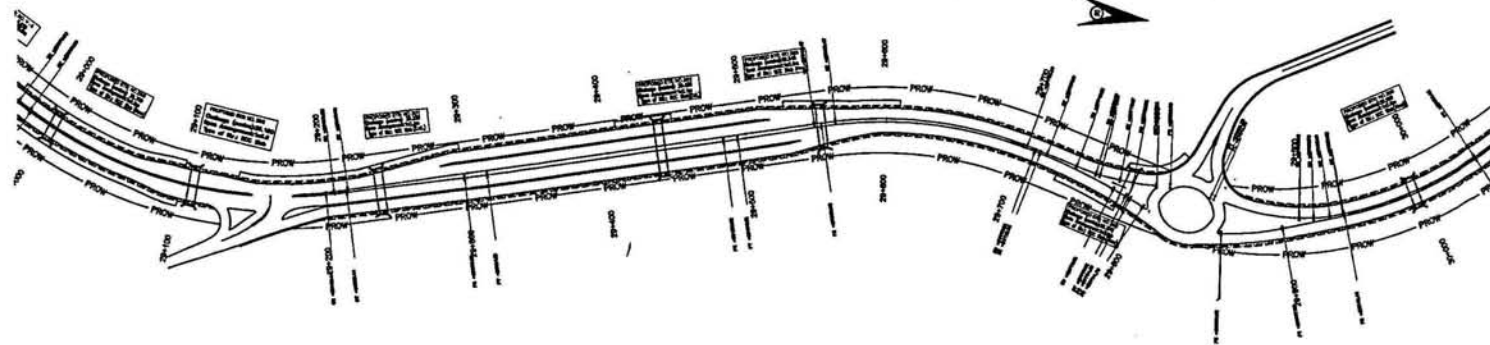
Drawing Title: PLAN & PROFILE
 Km. 28+000 to 29+000
Drawing No.: SW/1018/ROADS/PP-26
Scale: HOR:-1:2000 VER:-1:200
Date: JUNE-2009
Appd: B.R.G.C.D.PURI
Drawn: K.L.N.
Checked: K.PRASAD

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 Fax: (011) 41679350
 E-mail: swinfo@swi.com



← Jammu
84

Udhampur →



Level Datum =436.000

| |
|---|
| Proposed Level along Left Median Edge (m.) |
| Existing Level Below Left Median Edge (m.) |
| Proposed Level along Right Median Edge (m.) |
| Existing Level Below Right Median Edge (m.) |
| Horizontal Alignment Left |
| Horizontal Alignment Right |
| Vertical Alignment Left |
| Vertical Alignment Right |
| LHS Superelevation |
| RHS Superelevation |
| Left Profile Chainage |
| Right Profile Chainage |

| LEGEND | FOR | PROPOSED | FOR | EXISTING |
|----------------------|----------------------|----------------------|----------------------|----------------------|
| 1. Road | 1. Road | 1. Road | 1. Road | 1. Road |
| 2. Drain | 2. Drain | 2. Drain | 2. Drain | 2. Drain |
| 3. Structure | 3. Structure | 3. Structure | 3. Structure | 3. Structure |
| 4. Earthwork | 4. Earthwork | 4. Earthwork | 4. Earthwork | 4. Earthwork |
| 5. Proposed Level | 5. Proposed Level | 5. Proposed Level | 5. Proposed Level | 5. Proposed Level |
| 6. Existing Level | 6. Existing Level | 6. Existing Level | 6. Existing Level | 6. Existing Level |
| 7. Proposed Chainage | 7. Proposed Chainage | 7. Proposed Chainage | 7. Proposed Chainage | 7. Proposed Chainage |
| 8. Existing Chainage | 8. Existing Chainage | 8. Existing Chainage | 8. Existing Chainage | 8. Existing Chainage |
| 9. Proposed Profile | 9. Proposed Profile | 9. Proposed Profile | 9. Proposed Profile | 9. Proposed Profile |
| 10. Existing Profile | 10. Existing Profile | 10. Existing Profile | 10. Existing Profile | 10. Existing Profile |

Project Title
 Consultancy services for preparation of detailed project report for Rehabilitation and upgrading of existing 2-lane road to 4-lane divided carriageway along Jammu - Udhampur section of NH-1A (km 0.00 to km 66.00) including Jammu(Ur) Nagar and Udhampur bypass Phase - II Programme of North-South corridor Project, Contract Package C-02/2

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Scale: 1:2000

Client
NATIONAL HIGHWAYS AUTHORITY OF INDIA
 (Ministry of Shipping, Road Transport & Highways)

Drawing Title: PLAN & PROFILE
 Km. 29+000 to 30+000

Drawing No.: SWW/1019/ROADS/PP - 27

Scale: HOR:-1:2000 VER:-1:200

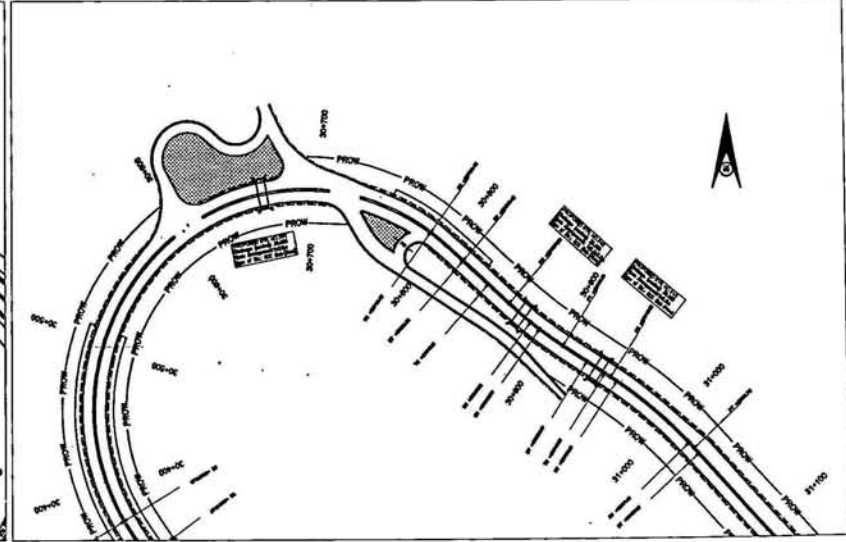
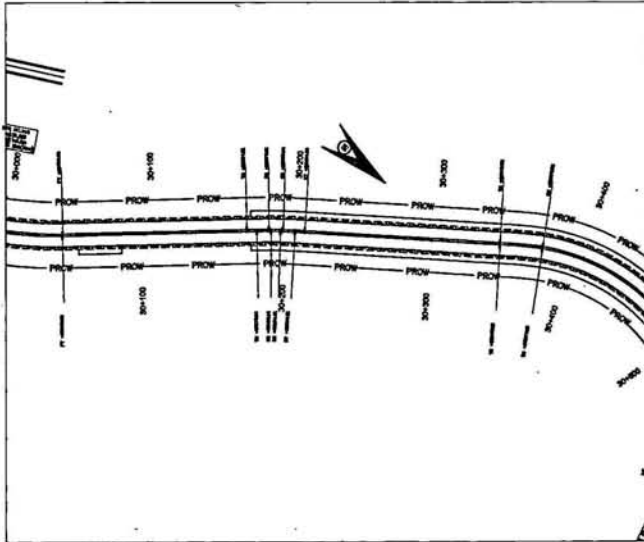
Drn: K.L.N
Dgn: K.PRASAD
Appd: BRIG.C.D.PUR
Date: JUNE-2009

Client: Wilson India Private Limited
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 NEW DELHI -110044
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 Fax: (011) 41679350
 E-mail: info@wilson.com



← Jammu
B6

Udhampur →



Level Datum =404.000

| |
|---|
| Proposed Level along Left Median Edge (m.) |
| Existing Level Below Left Median Edge (m.) |
| Proposed Level along Right Median Edge (m.) |
| Existing Level Below Right Median Edge (m.) |
| Horizontal Alignment Left |
| Horizontal Alignment Right |
| Vertical Alignment Left |
| Vertical Alignment Right |
| LHS Superelevation |
| RHS Superelevation |
| Left Profile Chainage |
| Right Profile Chainage |

| | |
|-----------|---------------------------------|
| REVISIONS | |
| No. | Description |
| 1. | As per Approved Tender Schedule |
| 2. | As per Approved Tender Schedule |
| 3. | As per Approved Tender Schedule |
| 4. | As per Approved Tender Schedule |
| 5. | As per Approved Tender Schedule |
| 6. | As per Approved Tender Schedule |
| 7. | As per Approved Tender Schedule |
| 8. | As per Approved Tender Schedule |
| 9. | As per Approved Tender Schedule |
| 10. | As per Approved Tender Schedule |

Project Title
 Consultancy services for preparation of detailed project report for Rehabilitation and upgrading of existing 2-lane road to 4-lane divided carriageway configuration of Jammu - Udhampur section of NH-1 A (Km 0.00 to Km 66.00) including Jammu(Ur) Nagaria and Udhampur Bypass Phase - III Programme of North-South corridor Project, Contract Package C-2018

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Client
NATIONAL HIGHWAYS AUTHORITY OF INDIA
 (Ministry of Shipping, Road Transport & Highways)

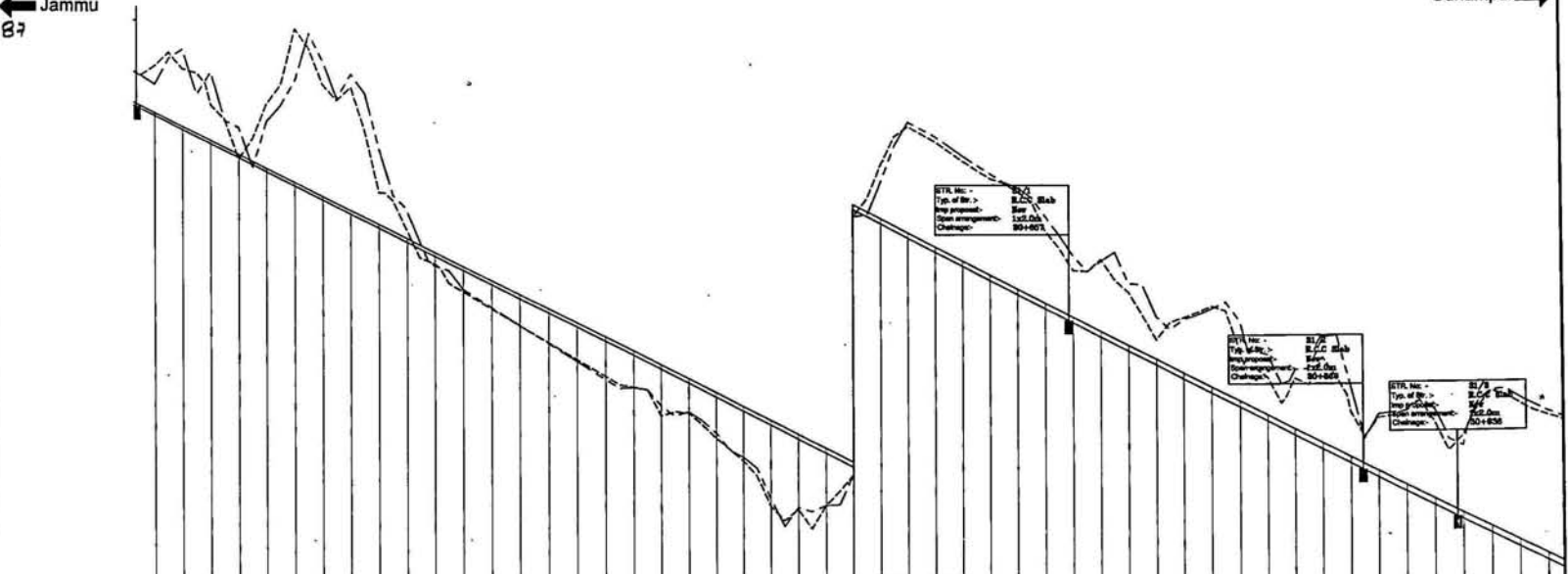
Drawing Title: PLAN & PROFILE
 Km. 30+000 to 31+100
Drawing No.: SW/1019/ROADS/PP-29
Scale: HOR:-1:2000 VER:-1:200
Date: JUNE-2009
Appd.: BRG.C.D.POR
Date: JUNE-2009

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 E-mail: info@swindia.com



← Jammu
87

Udhampur →



| | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
|---|--|---|---|------------|------------|------------|------------|------------|------------|------------|------------|------------|------------|------------|------------|------------|------------|------------|------------|------------|------------|------------|------------|------------|------------|------------|------------|------------|------------|------------|----------------|------------|------------|------------|------------|------------|------------|------------|------------|------------|------------|------------|------------|------------|------------|------------|------------|------------|------------|------------|------------|------------|------------|------------|------------|------------|------------|----------------|------------|------------|------------|------------|------------|------------|------------|------------|------------|------------|------------|------------|------------|------------|------------|------------|------------|------------|------------|------------|------------|------------|------------|------------|------------|------------|----------------|------------|------------|------------|------------|------------|------------|------------|------------|------------|------------|------------|------------|------------|------------|------------|------------|------------|------------|------------|------------|------------|------------|------------|------------|------------|------------|----------------|------------|------------|------------|------------|------------|------------|------------|------------|------------|------------|------------|------------|------------|------------|------------|------------|------------|------------|------------|------------|------------|------------|------------|------------|------------|------------|
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| Proposed Level along Left Median Edge (m.) | Existing Level Below Left Median Edge (m.) | Proposed Level along Right Median Edge (m.) | Existing Level Below Right Median Edge (m.) | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| Horizontal Alignment Left | Horizontal Alignment Right | Vertical Alignment Left | Vertical Alignment Right | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| LHS Superelevation | RHS Superelevation | Left Profile Chainage | Right Profile Chainage | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| <table border="1"> <tr> <td>Stationing</td> <td>30+000.000</td><td>30+040.000</td><td>30+080.000</td><td>30+120.000</td><td>30+160.000</td><td>30+200.000</td><td>30+240.000</td><td>30+280.000</td><td>30+320.000</td><td>30+360.000</td><td>30+400.000</td><td>30+440.000</td><td>30+480.000</td><td>30+520.000</td><td>30+560.000</td><td>30+600.000</td><td>30+640.000</td><td>30+680.000</td><td>30+720.000</td><td>30+760.000</td><td>30+800.000</td><td>30+840.000</td><td>30+880.000</td><td>30+920.000</td><td>30+960.000</td><td>31+000.000</td> </tr> <tr> <td>Proposed Level</td> <td>30+000.000</td><td>30+040.000</td><td>30+080.000</td><td>30+120.000</td><td>30+160.000</td><td>30+200.000</td><td>30+240.000</td><td>30+280.000</td><td>30+320.000</td><td>30+360.000</td><td>30+400.000</td><td>30+440.000</td><td>30+480.000</td><td>30+520.000</td><td>30+560.000</td><td>30+600.000</td><td>30+640.000</td><td>30+680.000</td><td>30+720.000</td><td>30+760.000</td><td>30+800.000</td><td>30+840.000</td><td>30+880.000</td><td>30+920.000</td><td>30+960.000</td><td>31+000.000</td> </tr> <tr> <td>Existing Level</td> <td>30+000.000</td><td>30+040.000</td><td>30+080.000</td><td>30+120.000</td><td>30+160.000</td><td>30+200.000</td><td>30+240.000</td><td>30+280.000</td><td>30+320.000</td><td>30+360.000</td><td>30+400.000</td><td>30+440.000</td><td>30+480.000</td><td>30+520.000</td><td>30+560.000</td><td>30+600.000</td><td>30+640.000</td><td>30+680.000</td><td>30+720.000</td><td>30+760.000</td><td>30+800.000</td><td>30+840.000</td><td>30+880.000</td><td>30+920.000</td><td>30+960.000</td><td>31+000.000</td> </tr> <tr> <td>Proposed Level</td> <td>30+000.000</td><td>30+040.000</td><td>30+080.000</td><td>30+120.000</td><td>30+160.000</td><td>30+200.000</td><td>30+240.000</td><td>30+280.000</td><td>30+320.000</td><td>30+360.000</td><td>30+400.000</td><td>30+440.000</td><td>30+480.000</td><td>30+520.000</td><td>30+560.000</td><td>30+600.000</td><td>30+640.000</td><td>30+680.000</td><td>30+720.000</td><td>30+760.000</td><td>30+800.000</td><td>30+840.000</td><td>30+880.000</td><td>30+920.000</td><td>30+960.000</td><td>31+000.000</td> </tr> <tr> <td>Existing Level</td> <td>30+000.000</td><td>30+040.000</td><td>30+080.000</td><td>30+120.000</td><td>30+160.000</td><td>30+200.000</td><td>30+240.000</td><td>30+280.000</td><td>30+320.000</td><td>30+360.000</td><td>30+400.000</td><td>30+440.000</td><td>30+480.000</td><td>30+520.000</td><td>30+560.000</td><td>30+600.000</td><td>30+640.000</td><td>30+680.000</td><td>30+720.000</td><td>30+760.000</td><td>30+800.000</td><td>30+840.000</td><td>30+880.000</td><td>30+920.000</td><td>30+960.000</td><td>31+000.000</td> </tr> </table> | | | | Stationing | 30+000.000 | 30+040.000 | 30+080.000 | 30+120.000 | 30+160.000 | 30+200.000 | 30+240.000 | 30+280.000 | 30+320.000 | 30+360.000 | 30+400.000 | 30+440.000 | 30+480.000 | 30+520.000 | 30+560.000 | 30+600.000 | 30+640.000 | 30+680.000 | 30+720.000 | 30+760.000 | 30+800.000 | 30+840.000 | 30+880.000 | 30+920.000 | 30+960.000 | 31+000.000 | Proposed Level | 30+000.000 | 30+040.000 | 30+080.000 | 30+120.000 | 30+160.000 | 30+200.000 | 30+240.000 | 30+280.000 | 30+320.000 | 30+360.000 | 30+400.000 | 30+440.000 | 30+480.000 | 30+520.000 | 30+560.000 | 30+600.000 | 30+640.000 | 30+680.000 | 30+720.000 | 30+760.000 | 30+800.000 | 30+840.000 | 30+880.000 | 30+920.000 | 30+960.000 | 31+000.000 | Existing Level | 30+000.000 | 30+040.000 | 30+080.000 | 30+120.000 | 30+160.000 | 30+200.000 | 30+240.000 | 30+280.000 | 30+320.000 | 30+360.000 | 30+400.000 | 30+440.000 | 30+480.000 | 30+520.000 | 30+560.000 | 30+600.000 | 30+640.000 | 30+680.000 | 30+720.000 | 30+760.000 | 30+800.000 | 30+840.000 | 30+880.000 | 30+920.000 | 30+960.000 | 31+000.000 | Proposed Level | 30+000.000 | 30+040.000 | 30+080.000 | 30+120.000 | 30+160.000 | 30+200.000 | 30+240.000 | 30+280.000 | 30+320.000 | 30+360.000 | 30+400.000 | 30+440.000 | 30+480.000 | 30+520.000 | 30+560.000 | 30+600.000 | 30+640.000 | 30+680.000 | 30+720.000 | 30+760.000 | 30+800.000 | 30+840.000 | 30+880.000 | 30+920.000 | 30+960.000 | 31+000.000 | Existing Level | 30+000.000 | 30+040.000 | 30+080.000 | 30+120.000 | 30+160.000 | 30+200.000 | 30+240.000 | 30+280.000 | 30+320.000 | 30+360.000 | 30+400.000 | 30+440.000 | 30+480.000 | 30+520.000 | 30+560.000 | 30+600.000 | 30+640.000 | 30+680.000 | 30+720.000 | 30+760.000 | 30+800.000 | 30+840.000 | 30+880.000 | 30+920.000 | 30+960.000 | 31+000.000 |
| Stationing | 30+000.000 | 30+040.000 | 30+080.000 | 30+120.000 | 30+160.000 | 30+200.000 | 30+240.000 | 30+280.000 | 30+320.000 | 30+360.000 | 30+400.000 | 30+440.000 | 30+480.000 | 30+520.000 | 30+560.000 | 30+600.000 | 30+640.000 | 30+680.000 | 30+720.000 | 30+760.000 | 30+800.000 | 30+840.000 | 30+880.000 | 30+920.000 | 30+960.000 | 31+000.000 | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| Proposed Level | 30+000.000 | 30+040.000 | 30+080.000 | 30+120.000 | 30+160.000 | 30+200.000 | 30+240.000 | 30+280.000 | 30+320.000 | 30+360.000 | 30+400.000 | 30+440.000 | 30+480.000 | 30+520.000 | 30+560.000 | 30+600.000 | 30+640.000 | 30+680.000 | 30+720.000 | 30+760.000 | 30+800.000 | 30+840.000 | 30+880.000 | 30+920.000 | 30+960.000 | 31+000.000 | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| Existing Level | 30+000.000 | 30+040.000 | 30+080.000 | 30+120.000 | 30+160.000 | 30+200.000 | 30+240.000 | 30+280.000 | 30+320.000 | 30+360.000 | 30+400.000 | 30+440.000 | 30+480.000 | 30+520.000 | 30+560.000 | 30+600.000 | 30+640.000 | 30+680.000 | 30+720.000 | 30+760.000 | 30+800.000 | 30+840.000 | 30+880.000 | 30+920.000 | 30+960.000 | 31+000.000 | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| Proposed Level | 30+000.000 | 30+040.000 | 30+080.000 | 30+120.000 | 30+160.000 | 30+200.000 | 30+240.000 | 30+280.000 | 30+320.000 | 30+360.000 | 30+400.000 | 30+440.000 | 30+480.000 | 30+520.000 | 30+560.000 | 30+600.000 | 30+640.000 | 30+680.000 | 30+720.000 | 30+760.000 | 30+800.000 | 30+840.000 | 30+880.000 | 30+920.000 | 30+960.000 | 31+000.000 | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| Existing Level | 30+000.000 | 30+040.000 | 30+080.000 | 30+120.000 | 30+160.000 | 30+200.000 | 30+240.000 | 30+280.000 | 30+320.000 | 30+360.000 | 30+400.000 | 30+440.000 | 30+480.000 | 30+520.000 | 30+560.000 | 30+600.000 | 30+640.000 | 30+680.000 | 30+720.000 | 30+760.000 | 30+800.000 | 30+840.000 | 30+880.000 | 30+920.000 | 30+960.000 | 31+000.000 | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |

| REVISION | |
|----------|--------------------------------|
| No. | Description |
| 1 | As per discussion with client. |
| 2 | As per discussion with client. |
| 3 | As per discussion with client. |
| 4 | As per discussion with client. |
| 5 | As per discussion with client. |
| 6 | As per discussion with client. |
| 7 | As per discussion with client. |
| 8 | As per discussion with client. |
| 9 | As per discussion with client. |
| 10 | As per discussion with client. |

Project Title
 Rehabilitation and widening of existing 2-lane road to 4-lane divided Carriageway with integration of Jammu - Udhampur section of NH-1A (km 0.00 to km 66.00) including Jammu(Ur) Bypass and Udhampur Bypass Phase - II Programme of North-South corridor Project, Contract Package C-0718

Client
 NATIONAL HIGHWAYS AUTHORITY OF INDIA
 (Ministry of Shipping, Road Transport & Highways)

Drawing Title: PLAN & PROFILE
 Km. 30+000 to 31+000

Drawing No.: SW/1019/ROADS/PP-28

Scale:- HOR:-1:2000 VER:-1:200

Date: JUNE-2009

Dra: K.L.N **Dgn:** K.PRASAD **Appd:** BRG.C.P.U.R **Date:** JUNE-2009

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 E-mail: scwilson@scwilson.com

Scale: HOR:-1:2000 VER:-1:200

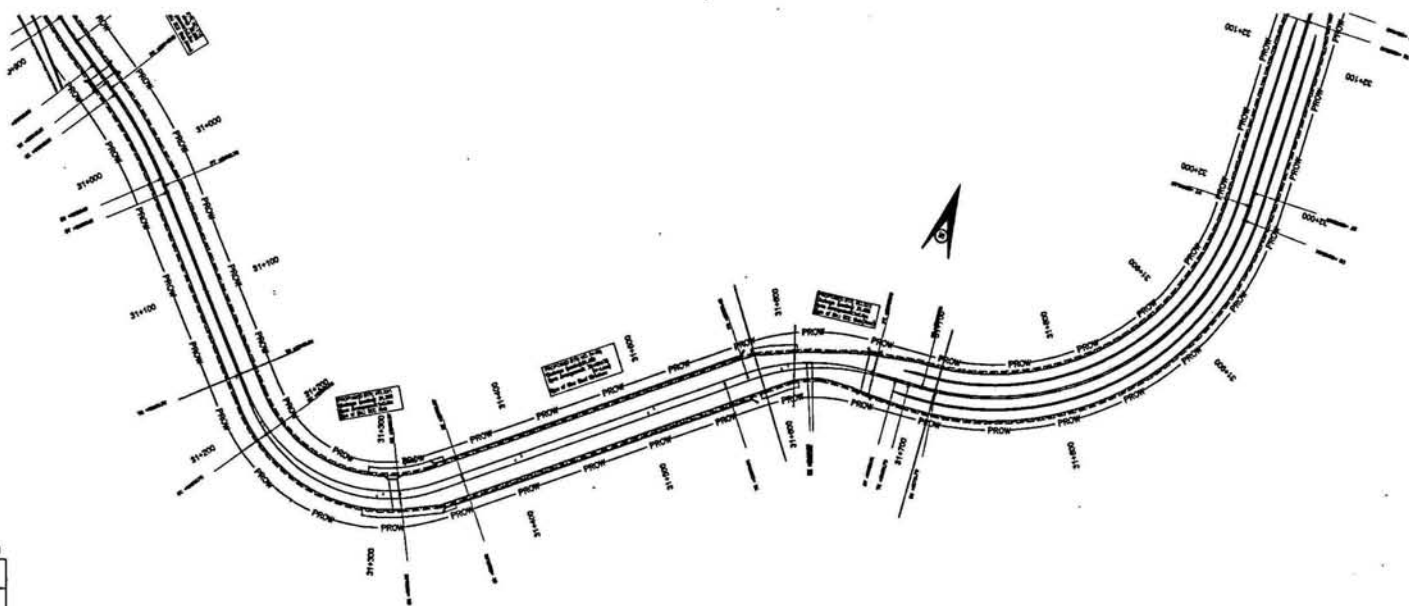
Date: JUNE-2009

Dra: K.L.N **Dgn:** K.PRASAD **Appd:** BRG.C.P.U.R **Date:** JUNE-2009



← Jammu
BB

Udhampur →



| |
|---|
| Level Datum =362.000 |
| Proposed Level along Left Median Edge (m.) |
| Existing Level Below Left Median Edge (m.) |
| Proposed Level along Right Median Edge (m.) |
| Existing Level Below Right Median Edge (m.) |
| Horizontal Alignment Left |
| Horizontal Alignment Right |
| Vertical Alignment Left |
| Vertical Alignment Right |
| LHS Superlevation |
| RHS Superlevation |
| Left Profile Chainage |
| Right Profile Chainage |

| LEGEND | |
|--------------------------|--------------------------|
| For Proposed | For Existing |
| 1. Right of Way | 1. Right of Way |
| 2. Proposed Level | 2. Existing Level |
| 3. Proposed L.S. Profile | 3. Existing L.S. Profile |
| 4. Proposed R.S. Profile | 4. Existing R.S. Profile |
| 5. Proposed Chainage | 5. Existing Chainage |
| 6. Proposed L.S. Profile | 6. Existing L.S. Profile |
| 7. Proposed R.S. Profile | 7. Existing R.S. Profile |
| 8. Proposed Chainage | 8. Existing Chainage |

Project Title
 Consultancy services for preparation of detailed project report for establishment and upgrading of existing 2-lane road to 4-lane divided carriageway configuration of Jammu - Udhampur section of NH-1A
 Km. 0.70 to Km. 64.50 including Jammu(Ur) Bypass and Udhampur Bypass Phase - III Programme of North-South - corridor Project.
 Contract Package C-4214

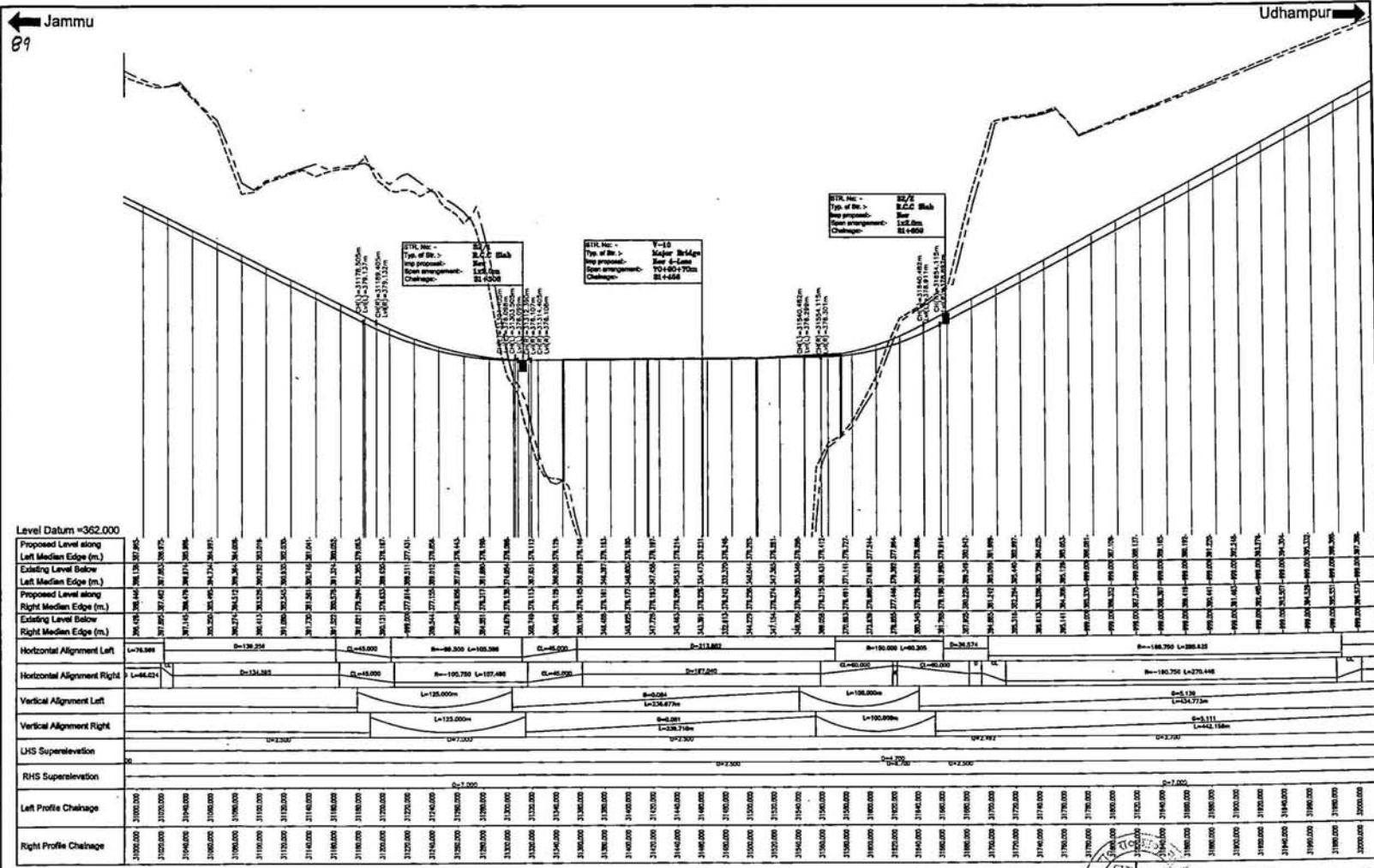
Client
 NATIONAL HIGHWAYS AUTHORITY OF INDIA
 (Ministry of Shipping, Road Transport & Highways)

Drawing Title: PLAN & PROFILE
 Km. 31+000 to 32+000
Drawing No.: SW/1019/ROADS/PP-29
Scale: HOR:-1:2000 VCR:-1:200
Date: JUNE-2009
Appd: BRG.C.D.PUR
Date: JUNE-2009

Sheet: 1 of 2
 Dm: K.L.N. Dpt: K.PRASAD
 Appd: BRG.C.D.PUR Date: JUNE-2009

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| Rev | Description |
|-----|----------------|
| 1 | As per drawing |
| 2 | As per drawing |
| 3 | As per drawing |
| 4 | As per drawing |
| 5 | As per drawing |
| 6 | As per drawing |
| 7 | As per drawing |
| 8 | As per drawing |
| 9 | As per drawing |
| 10 | As per drawing |

Project Title
 Consultancy services for preparation of detailed project report for Rehabilitation and upgrading of existing 2-lane road to 4-lane divided carriageway and expansion of Jammu - Udhampur section of NH-1A (On 0.00 to km 66.00) including Jammu(Ur) Bypass and Udhampur Bypass Phase - III Programme of North-South corridor Project. Contract Package C-0218

Client
NATIONAL HIGHWAYS AUTHORITY OF INDIA
 (Ministry of Shipping, Road Transport & Highways)

Drawing Title: PLAN & PROFILE
 Km. 31+000 to 32+000
Drawing No.: SW/1019/ROADS/PP-ZR
 Scale: ---
 Date: ---
 Dn: K.L.N
 Dgm: K.PRASAD
 Appd: BRG.C.D.PURI
 JUNE-2009

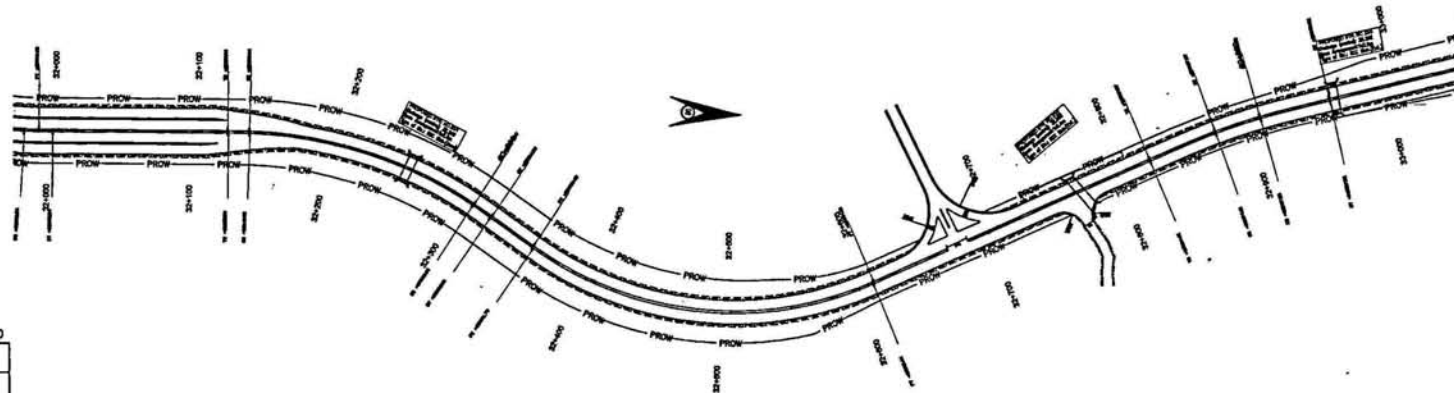
Scott Wilson India Private Limited
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 NEW DELHI - 110044
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 Fax: (011) 41679350
 E-mail: scwilson@scwilson.com



← Jammu

Udhampur →

90



Level Datum =394.000

| |
|---|
| Proposed Level along Left Median Edge (m.) |
| Existing Level Below Left Median Edge (m.) |
| Proposed Level along Right Median Edge (m.) |
| Existing Level Below Right Median Edge (m.) |
| Horizontal Alignment Left |
| Horizontal Alignment Right |
| Vertical Alignment Left |
| Vertical Alignment Right |
| LHS Super-elevation |
| RHS Super-elevation |
| Left Profile Chainage |
| Right Profile Chainage |

| |
|---|
| LEGEND |
| For Proposed Four Lanes |
| 1. Existing 2.5% super-elevation |
| 2. Proposed level along Left Median Edge |
| 3. Existing level along Right Median Edge |
| 4. Proposed level along Right Median Edge |
| 5. Existing level along Left Median Edge |
| 6. Proposed 1.5% super-elevation |
| 7. Proposed 2.5% super-elevation |
| 8. Existing level along Right Median Edge |
| 9. Proposed 1.5% super-elevation |
| 10. Existing level along Left Median Edge |

Project Title
 Consultancy services for preparation of detailed project report for Rehabilitation and upgrading of existing 2-lane road to 4-lane divided carriageway configuration of Jammu - Udhampur section of NH-1A (Km 0.00 to Km 66.00) including Jammu(U+I) Nagrota and Udhampur Bypass Phase - III Programme of North - South - corridor Project, Contract Package C-0718

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Scale: 1:2000
 Date: 01/06/2009

Client
NATIONAL HIGHWAYS AUTHORITY OF INDIA
 (Ministry of Shipping, Road Transport & Highways)

Drawing Title: PLAN & PROFILE
 Km. 32+000 to 33+000

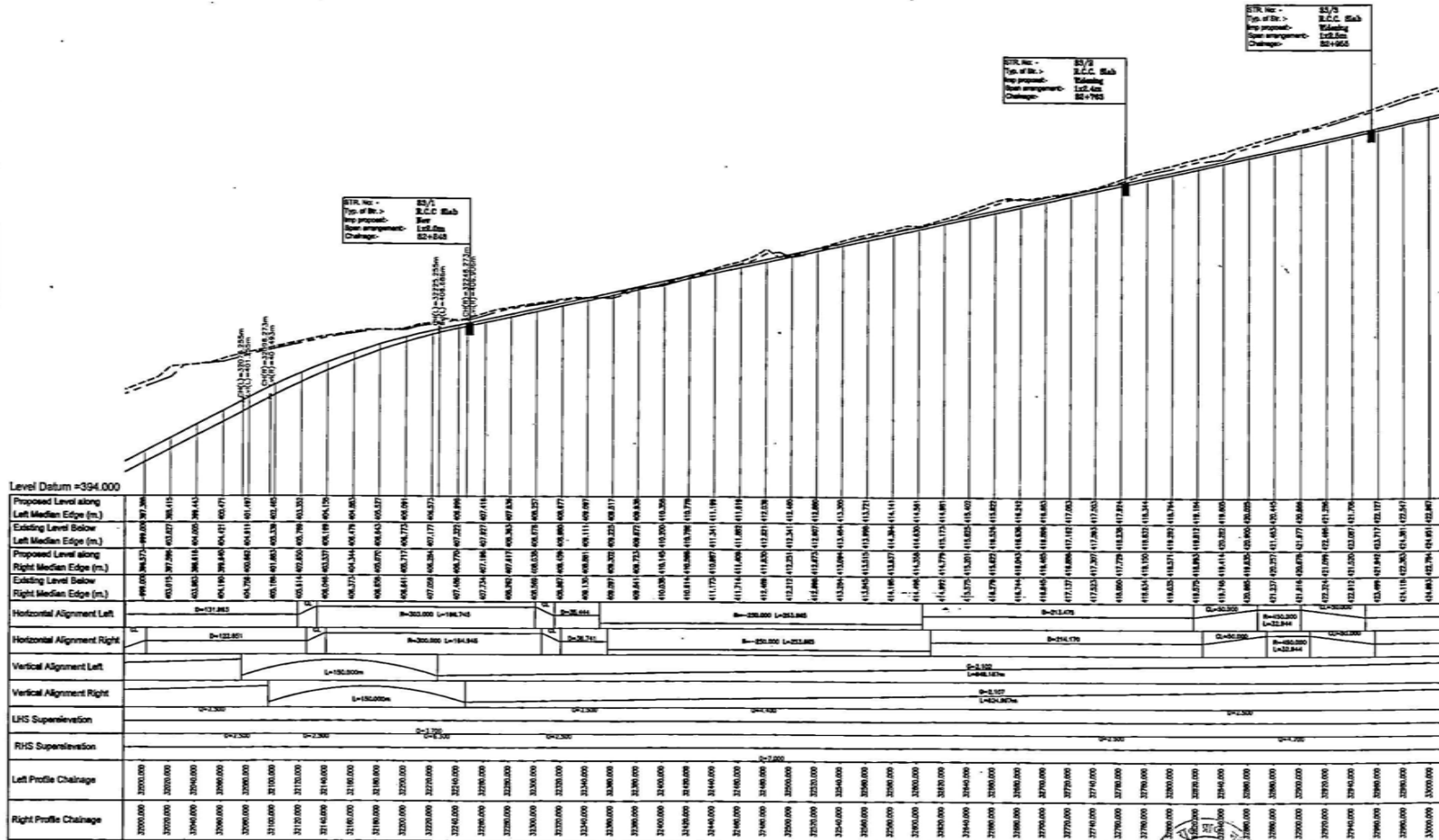
Drawing No.: SW/1018/ROADS/PP - 30

Scale: HOR:-1:2000 VER:-1:200

Drn: K.L.N **Dgn:** K.PRASAD **Appd:** BRIG.C.D.PURI **Date:** JUNE-2009

Scott Wilson India Private Limited
 84-26/4, MOHAN CO-OPERATIVE IND. ESTATE
 MATURA ROAD
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 Fax: (011) 41679350
 E-mail: swi@scwip.com





R/R No. - 53/3
 Top of Br. - R.C.C. Slab
 Prop. proposed - 1.25.2m
 Road arrangement - 53+9.5
 Challenge -

R/R No. - 53/3
 Top of Br. - R.C.C. Slab
 Prop. proposed - 1.25.2m
 Road arrangement - 53+9.5
 Challenge -

R/R No. - 53/3
 Top of Br. - R.C.C. Slab
 Prop. proposed - 1.25.2m
 Road arrangement - 53+9.5
 Challenge -

| Legend | Project Title |
|--|---|
| For Proposed Year Loading | Construction |
| For Proposed Year Unloading | Operation |
| 1. Proposed Left Side Left Side Edge | 2. Proposed Right Side Right Side Edge |
| 3. Proposed Left Side Right Side Edge | 4. Proposed Right Side Left Side Edge |
| 5. Proposed Left Side Left Side Edge | 6. Proposed Right Side Right Side Edge |
| 7. Proposed Left Side Right Side Edge | 8. Proposed Right Side Left Side Edge |
| 9. Proposed Left Side Left Side Edge | 10. Proposed Right Side Right Side Edge |
| 11. Proposed Left Side Right Side Edge | 12. Proposed Right Side Left Side Edge |

Project Title
 Consultancy services for preparation of detailed project report for Rehabilitation and upgrading of existing 3-lane road to 4-lane divided carriageway configuration of Jammu - Udhampur section of NH-1 A (Km 32+00 to Km 33+00) including Jammu(Ur) Bypass and Udhampur Bypass Phase - III Programme of North-South corridor Project, Contract Package C-0318

Client
NATIONAL HIGHWAYS AUTHORITY OF INDIA
 (Ministry of Shipping, Road Transport & Highways)

Drawing Title: **PLAN & PROFILE**
 Km. 32+000 to 33+000

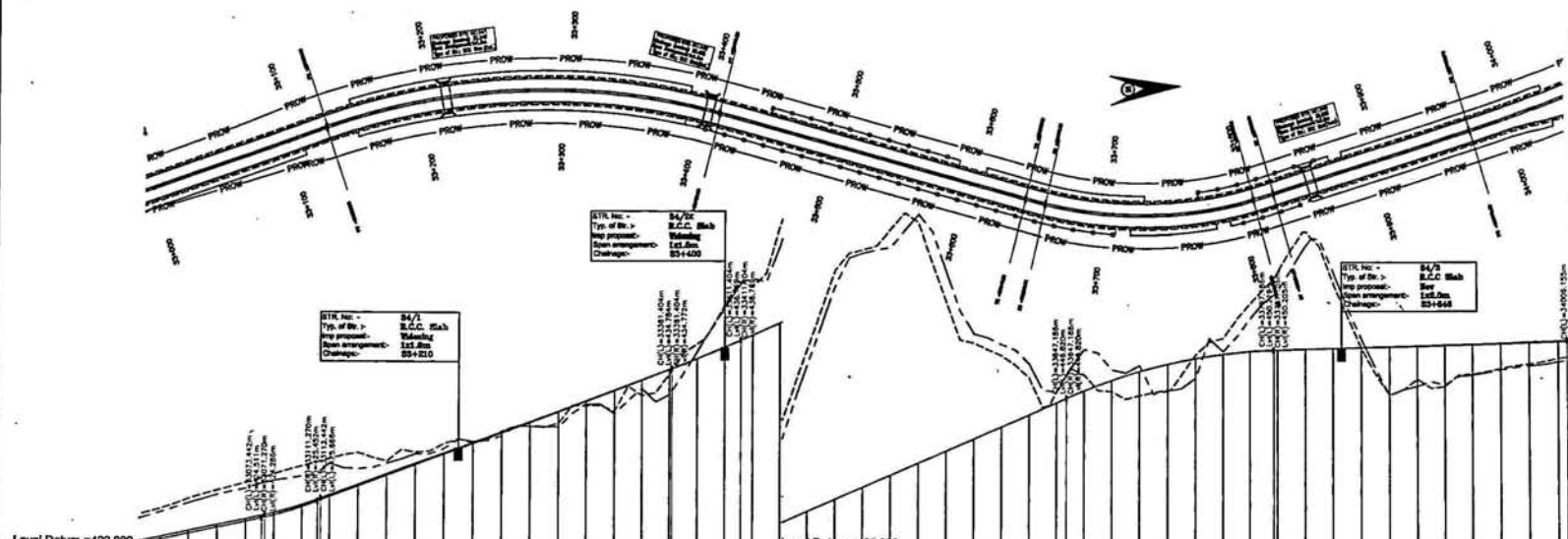
Drawing No.: **SW/1019/ROADS/FPP - 33** Sheet No. **2** of **2**

Scale: **HOR:-1:2000 VER:-1:200**

Drawn: **K.L.M.** Checked: **K.PRASAD** Date: **18/05/2023**

Client: **Scott Wilson India Private Limited**
 A-157C MOHAN CO-OPERATIVE HO. ESTATE
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 E-mail: info@swi.com





| Level Datum = 422.000 | Level Datum = 36.000 | | | | | | | | | | | | | | |
|---|----------------------|-----------|-----------|-----------|------------|---------------------|-----------|-----------|------------|-----------|-----------|-----------|------------|-----------|-----------|
| Proposed Level along Left Median Edge (m.) | 44.880 | 44.910 | 44.940 | 44.970 | 45.000 | 45.030 | 45.060 | 45.090 | 45.120 | 45.150 | 45.180 | 45.210 | 45.240 | 45.270 | 45.300 |
| Existing Level Below Left Median Edge (m.) | 43.200 | 43.250 | 43.300 | 43.350 | 43.400 | 43.450 | 43.500 | 43.550 | 43.600 | 43.650 | 43.700 | 43.750 | 43.800 | 43.850 | 43.900 |
| Proposed Level along Right Median Edge (m.) | 46.150 | 46.180 | 46.210 | 46.240 | 46.270 | 46.300 | 46.330 | 46.360 | 46.390 | 46.420 | 46.450 | 46.480 | 46.510 | 46.540 | 46.570 |
| Existing Level Below Right Median Edge (m.) | 44.500 | 44.550 | 44.600 | 44.650 | 44.700 | 44.750 | 44.800 | 44.850 | 44.900 | 44.950 | 45.000 | 45.050 | 45.100 | 45.150 | 45.200 |
| Horizontal Alignment Left | D=122.472 | | | | | R=480.000 L=283.214 | | | | | D=228.817 | | | | |
| Horizontal Alignment Right | D=121.422 | | | | | R=481.500 L=284.088 | | | | | D=228.433 | | | | |
| Vertical Alignment Left | L=40.000m | | C=3.877 | | L=247.862m | | C=1.263 | | L=150.000m | | C=0.388 | | L=206.866m | | |
| Vertical Alignment Right | L=40.000m | | C=3.728 | | L=90.000m | | C=4.783 | | L=150.000m | | C=0.250 | | L=222.097m | | |
| LHS Superelevation | D=2.500 | | | C=2.518 | | | D=2.800 | | | D=2.500 | | | D=2.200 | | |
| RHS Superelevation | D=2.500 | | | C=2.518 | | | D=2.800 | | | D=2.500 | | | D=2.200 | | |
| Left Profile Chainage | 33000.000 | 33020.000 | 33040.000 | 33060.000 | 33080.000 | 33100.000 | 33120.000 | 33140.000 | 33160.000 | 33180.000 | 33200.000 | 33220.000 | 33240.000 | 33260.000 | 33280.000 |
| Right Profile Chainage | 33000.000 | 33020.000 | 33040.000 | 33060.000 | 33080.000 | 33100.000 | 33120.000 | 33140.000 | 33160.000 | 33180.000 | 33200.000 | 33220.000 | 33240.000 | 33260.000 | 33280.000 |

- LEGEND:**
- For Proposed Four Lanes
- 1. Right of Way
 - 2. Right of Way
 - 3. Proposed Left Hand Side Slope
 - 4. Proposed Right Hand Side Slope
 - 5. Proposed Left Hand Side Slope
 - 6. Proposed Right Hand Side Slope
 - 7. Proposed Left Hand Side Slope
 - 8. Proposed Right Hand Side Slope
 - 9. Proposed Left Hand Side Slope
 - 10. Proposed Right Hand Side Slope

Project Title
 Consistency services for preparation of detailed project report for Rehabilitation and upgrading of existing 2-lane road to 4-lane divided carriageway configuration of Jammu - Udhampur section of NH-1A (Km 0.00 to Km 66.00) including Jammu(Ur) Bypass and Udhampur Bypass Phase - II Construction of North-South corridor Project, Contract Package C-0218

Client
NATIONAL HIGHWAYS AUTHORITY OF INDIA
 (Ministry of Shipping, Road Transport & Highways)

Drawing Title: PLAN & PROFILE
 Km. 33+000 to 34+000

Drawing No.: SW/1019/ROADS/PP-31

Scale: HOR-1:2000 VERT-1:2000

Date: JUNE-2009

Drn: K.L.N
Dgn: K.PRASAD
Appd: BRG.C.D.PURI

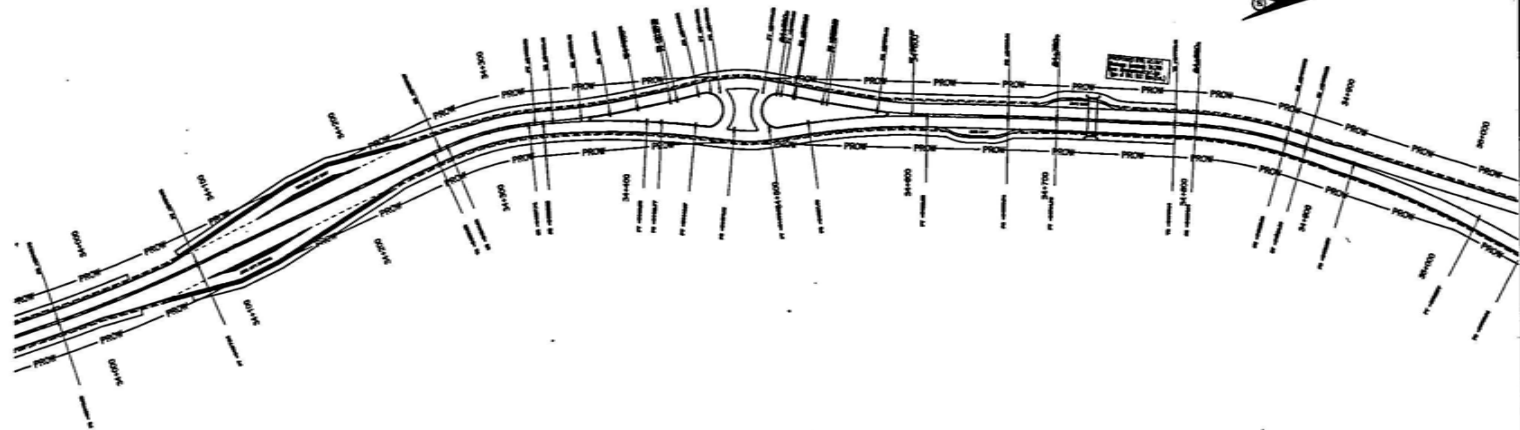
Scott Wilson India Private Limited
 26/4, MOHAN CO-OPERATIVE IND. ESTATE
 MATURA ROAD
 NEW DELHI - 110044
 Tel: (011) 41679340 - 49
 Fax: (011) 41679350
 E-mail: s_windia@rediffmail.com



Jammu

93

Udhampur



Level Datum = 448.000

| |
|--|
| Proposed Level along Left Median Edge (m) |
| Existing Level Below Left Median Edge (m) |
| Proposed Level along Right Median Edge (m) |
| Existing Level Below Right Median Edge (m) |
| Horizontal Alignment Left |
| Horizontal Alignment Right |
| Vertical Alignment Left |
| Vertical Alignment Right |
| LHS Super-elevation |
| RHS Super-elevation |
| Left Profile Chisage |
| Right Profile Chisage |

| REVISION | DATE | BY | CHKD | APPD |
|----------|------|----|------|------|
| 1 | | | | |
| 2 | | | | |
| 3 | | | | |
| 4 | | | | |
| 5 | | | | |
| 6 | | | | |
| 7 | | | | |
| 8 | | | | |
| 9 | | | | |
| 10 | | | | |

Project Title
 Consultancy services for preparation of detailed project report for Rehabilitation and upgrading of existing 2-lane road to 4-lane divided carriageway configuration of Jammu - Udhampur section of NH-1A (Km 0.00 to Km 64.00) including Jammu(Ur) Nagpur and Udhampur Bypass Phase - II Programme of North-South corridor Project, Contract Package CD11

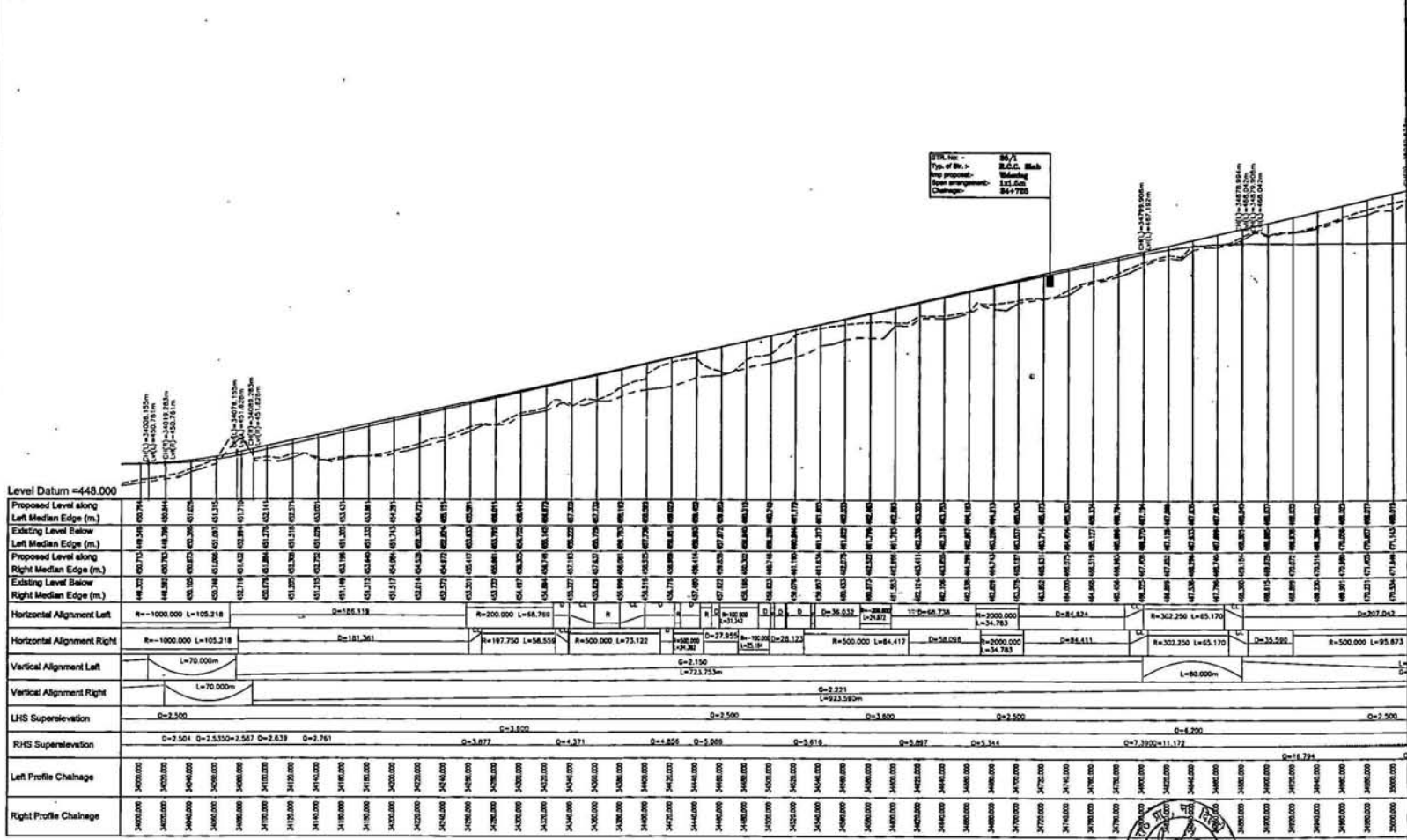
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Client
NATIONAL HIGHWAYS AUTHORITY OF INDIA
 (Ministry of Shipping, Road Transport & Highways)

Drawing Title: PLAN & PROFILE
 Km. 34+000 to 35+000
Drawing No.: SW/1019/ROADSPP - 32
Scale: HOR:-1:2000 VER:-1:200
Drn: K.L.N. **Dgn.:** K.PRASAD **Appd:** ERIC.C.D.PURI **Date:** JUNE-2009

Client: Scott Wilson India Private Limited
 22/74, MIDRAW CO-OPERATIVE IND. ESTATE
 ANAPARA ROAD
 NEW DELHI - 110044
 Tele: (011) 41679340 - 49
 Fax: (011) 41679350
 E-mail: scw@swil.com





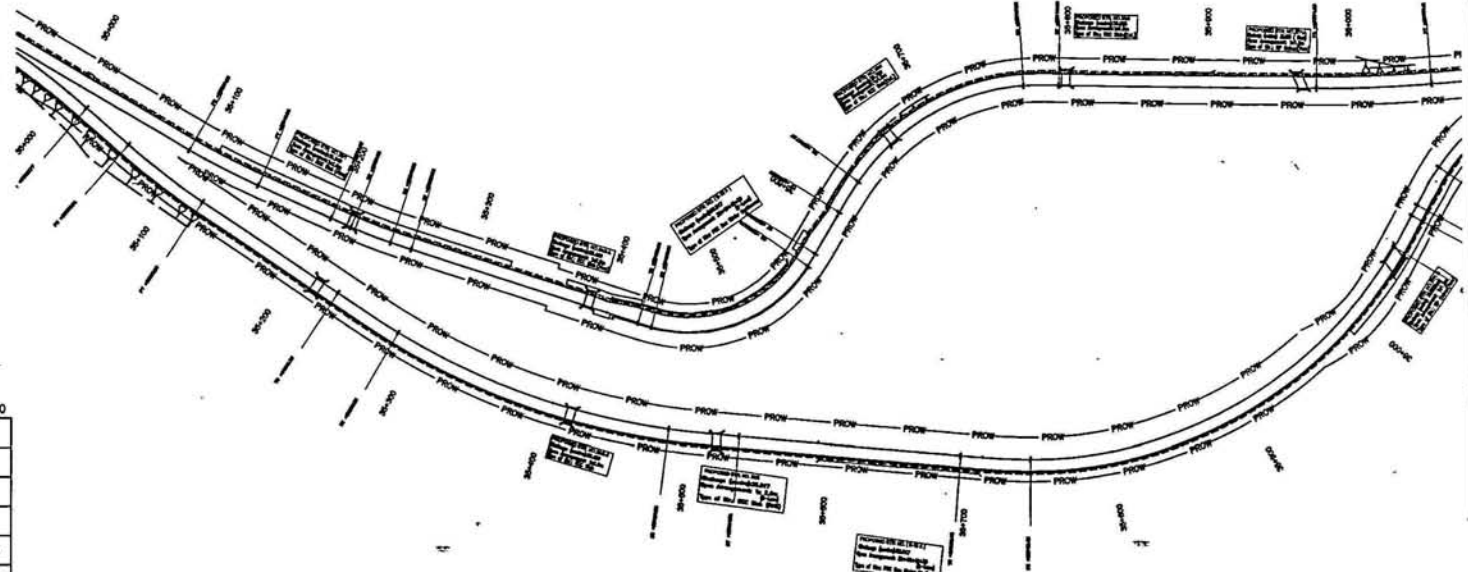
| | | | | |
|---|--|---|---|---|
| <p>1. Proposed Level</p> <p>2. Existing Level</p> <p>3. Proposed Level</p> <p>4. Existing Level</p> <p>5. Proposed Level</p> <p>6. Existing Level</p> <p>7. Proposed Level</p> <p>8. Existing Level</p> <p>9. Proposed Level</p> <p>10. Existing Level</p> <p>11. Proposed Level</p> <p>12. Existing Level</p> <p>13. Proposed Level</p> <p>14. Existing Level</p> <p>15. Proposed Level</p> <p>16. Existing Level</p> <p>17. Proposed Level</p> <p>18. Existing Level</p> <p>19. Proposed Level</p> <p>20. Existing Level</p> <p>21. Proposed Level</p> <p>22. Existing Level</p> <p>23. Proposed Level</p> <p>24. Existing Level</p> <p>25. Proposed Level</p> <p>26. Existing Level</p> <p>27. Proposed Level</p> <p>28. Existing Level</p> <p>29. Proposed Level</p> <p>30. Existing Level</p> <p>31. Proposed Level</p> <p>32. Existing Level</p> <p>33. Proposed Level</p> <p>34. Existing Level</p> <p>35. Proposed Level</p> <p>36. Existing Level</p> <p>37. Proposed Level</p> <p>38. Existing Level</p> <p>39. Proposed Level</p> <p>40. Existing Level</p> <p>41. Proposed Level</p> <p>42. Existing Level</p> <p>43. Proposed Level</p> <p>44. Existing Level</p> <p>45. Proposed Level</p> <p>46. Existing Level</p> <p>47. Proposed Level</p> <p>48. Existing Level</p> <p>49. Proposed Level</p> <p>50. Existing Level</p> <p>51. Proposed Level</p> <p>52. Existing Level</p> <p>53. Proposed Level</p> <p>54. Existing Level</p> <p>55. Proposed Level</p> <p>56. Existing Level</p> <p>57. Proposed Level</p> <p>58. Existing Level</p> <p>59. Proposed Level</p> <p>60. Existing Level</p> <p>61. Proposed Level</p> <p>62. Existing Level</p> <p>63. Proposed Level</p> <p>64. Existing Level</p> <p>65. Proposed Level</p> <p>66. Existing Level</p> <p>67. Proposed Level</p> <p>68. Existing Level</p> <p>69. Proposed Level</p> <p>70. Existing Level</p> <p>71. Proposed Level</p> <p>72. Existing Level</p> <p>73. Proposed Level</p> <p>74. Existing Level</p> <p>75. Proposed Level</p> <p>76. Existing Level</p> <p>77. Proposed Level</p> <p>78. Existing Level</p> <p>79. Proposed Level</p> <p>80. Existing Level</p> <p>81. Proposed Level</p> <p>82. Existing Level</p> <p>83. Proposed Level</p> <p>84. Existing Level</p> <p>85. Proposed Level</p> <p>86. Existing Level</p> <p>87. Proposed Level</p> <p>88. Existing Level</p> <p>89. Proposed Level</p> <p>90. Existing Level</p> <p>91. Proposed Level</p> <p>92. Existing Level</p> <p>93. Proposed Level</p> <p>94. Existing Level</p> <p>95. Proposed Level</p> <p>96. Existing Level</p> <p>97. Proposed Level</p> <p>98. Existing Level</p> <p>99. Proposed Level</p> <p>100. Existing Level</p> | <p>Project Title</p> <p>Consultancy services for preparation of detailed project report for establishment and upgrading of existing 3-lane road to 4-lane divided carriageway configuration of Jammu - Udhampur section of NH-1-A. (Km 0.00 to km 66.00) including Jaanoo(Pan) Nagrota and Udhampur Bypass Phase - III Programme of North-South - considers Project Contract Package C-02/14</p> | <p>Client</p> <p>NATIONAL HIGHWAYS AUTHORITY OF INDIA</p> <p>(Ministry of Shipping, Road Transport & Highways)</p> | <p>Drawing Title: PLAN & PROFILE</p> <p>Km. 34+000 to 35+000</p> <p>Drawing No.: SW/1010/ROADS/PP-32</p> <p>Scale: -</p> <p>DATE: JUNE-2009</p> | <p>Scott Wilson India Private Limited</p> <p>A-2679, MOHAN CO-OPERATIVE HO. ESTATE, MADHVA ROAD, NEW DELHI - 110044</p> <p>Phone: (011) 41679340 - 49</p> <p>Fax: (011) 41679350</p> <p>E-mail: swi@swi.com</p> |
|---|--|---|---|---|



← Jammu

Udhampur →

95



Level Datum = 465.000

| |
|---|
| Proposed Level along Left Median Edge (m.) |
| Existing Level Below Left Median Edge (m.) |
| Proposed Level along Right Median Edge (m.) |
| Existing Level Below Right Median Edge (m.) |
| Horizontal Alignment Left |
| Horizontal Alignment Right |
| Vertical Alignment Left |
| Vertical Alignment Right |
| LHS Superelevation |
| RHS Superelevation |
| Left Profile Chainage |
| Right Profile Chainage |

| REVISION | |
|----------|--------------------------------|
| No. | Description |
| 1. | As per approved Plan & Profile |
| 2. | As per approved Plan & Profile |
| 3. | As per approved Plan & Profile |
| 4. | As per approved Plan & Profile |
| 5. | As per approved Plan & Profile |
| 6. | As per approved Plan & Profile |
| 7. | As per approved Plan & Profile |
| 8. | As per approved Plan & Profile |
| 9. | As per approved Plan & Profile |
| 10. | As per approved Plan & Profile |

Project Title
 Consultancy services for preparation of detailed project report for Rehabilitation and upgrading of existing 2-lane road to 4-lane divided carriageway configuration of Jammu - Udhampur section of NH-1A (Km 0.00 to Km 66.00) including Jammu(Ur) Bypass and Udhampur Bypass Phase - III Programme of North-South corridor Project. Contract Package C-2013

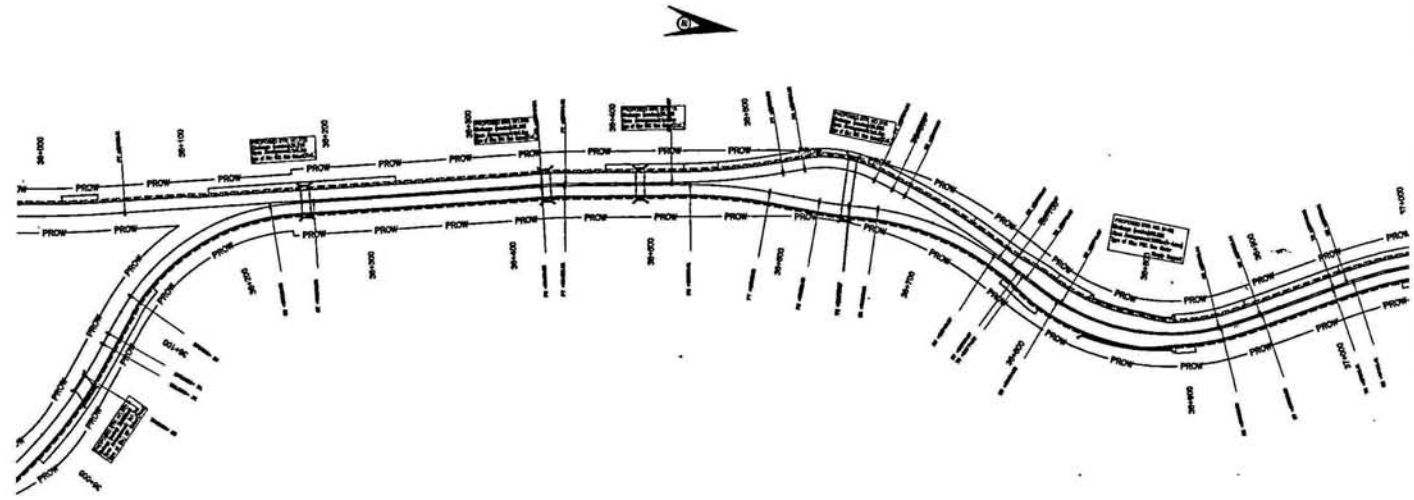
The drawings to be prepared of ROAD WIDTH, MEDIAN WIDTH and should not be treated as the only drawings to be used and shall be used in conjunction with the other drawings and specifications and shall be subject to the approval of the client. The drawings shall be prepared in accordance with the specifications of the client. The drawings shall be prepared in accordance with the specifications of the client. The drawings shall be prepared in accordance with the specifications of the client.

Client
NATIONAL HIGHWAYS AUTHORITY OF INDIA
 (Ministry of Shipping, Road Transport & Highways)

Drawing Title: PLAN & PROFILE
Km. 35+000 to 38+000
Drawing No.: SW/1019/ROADS/PP-33
Scale: HOR:-1:2000 VER:-1:200
Date: JUNE-2009
Appd: BRG.C.D.PURI
DR: K.L.N
DRG: K.PRASAD

Scale: Wilson India Private Limited
 A-26/4 MOHAN CO-OPERATIVE HO. ESTATE
 WATSON ROAD
 NEW DELHI - 110044
 Tel: (011) 41679340 - 49
 Fax: (011) 41679350
 E-mail: wipro@wipro.com





- Level Datum = 486.000
- Proposed Level along Left Median Edge (m.)
- Existing Level Below Left Median Edge (m.)
- Proposed Level along Right Median Edge (m.)
- Existing Level Below Right Median Edge (m.)
- Horizontal Alignment Left
- Horizontal Alignment Right
- Vertical Alignment Left
- Vertical Alignment Right
- LHS Superelevation
- RHS Superelevation
- Left Profile Chainage
- Right Profile Chainage

| REVISIONS | |
|-----------|----------------|
| No. | Description |
| 1 | As per drawing |
| 2 | As per drawing |
| 3 | As per drawing |
| 4 | As per drawing |
| 5 | As per drawing |
| 6 | As per drawing |
| 7 | As per drawing |
| 8 | As per drawing |
| 9 | As per drawing |
| 10 | As per drawing |

Project Title
 Consultancy services for preparation of detailed project report for rehabilitation and upgrading of existing 2-lane road to 4-lane divided carriageway configuration of Jammu - Udhampur section of NH-1A from 0.00 to km 66.00 (including Jammu/UD) Jammu and Udhampur Bypass Phase - III Programme of North - South - corridor Project. Contract Package C-2/11

Client
NATIONAL HIGHWAYS AUTHORITY OF INDIA
 (Ministry of Shipping, Road Transport & Highways)

Drawing Title: PLAN & PROFILE
 Km. 36+000 to 37+000

Drawing No.: SW/1019/ROADS/PP - 34
 Sheet 1 of 2

Scale: HOR:-1:2000 VER:-1:200

Drn: K.L.N
Dgn: K.PRASAD
Appd: BRIG.C.D.PURI
Date: JUNE-2009

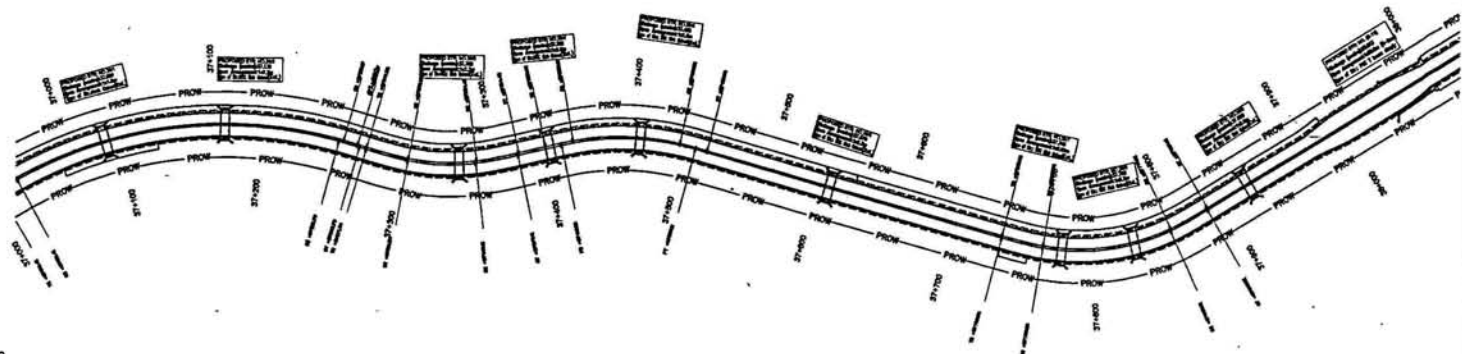
Scott Wilson India Private Limited
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 MADHURA ROAD
 NEW DELHI - 110044
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 E-mail: scwilson@scwilson.com



← Jammu

99

Udhampur →



Level Datum = 512.000

| |
|---|
| Proposed Level along Left Median Edge (m.) |
| Existing Level Below Left Median Edge (m.) |
| Proposed Level along Right Median Edge (m.) |
| Existing Level Below Right Median Edge (m.) |
| Horizontal Alignment Left |
| Horizontal Alignment Right |
| Vertical Alignment Left |
| Vertical Alignment Right |
| LHS Superelevation |
| RHS Superelevation |
| Left Profile Chainage |
| Right Profile Chainage |

| LEGEND | |
|----------------------------------|----------------------------------|
| For Proposed Four Lining | |
| 1. Existing 4-lane | 1. Proposed Left Hand Side Slip |
| 2. Proposed Right Hand Side Slip | 2. Proposed Left Hand Side Slip |
| 3. Proposed Right Hand Side Slip | 3. Existing Left Hand Side Slip |
| 4. Existing Left Hand Side Slip | 4. Existing Right Hand Side Slip |
| 5. Existing Right Hand Side Slip | 5. Existing Left Hand Side Slip |
| 6. Existing Right Hand Side Slip | 6. Existing Right Hand Side Slip |

Project Title
 Consultancy services for preparation of detailed project report for Rehabilitation and upgrading of existing 2-lane road to 4-lane divided carriageway and provision of Jammu - Udhampur section of NH-1A (km 0.00 to km 64.00) including Jammu(Utd) Nagrota and Udhampur Bypass Phase - III Programme of North-South - corridor Project. Contract Package-C/218

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Notes:
 1. Bridge 2nd to 11 km from Jammu (200m) (R-100)
 2. The drawing shall be in full and complete accordance with the provisions of Part II of Public and Industrial Works and the provisions of the Indian Standards Code of Practice for Road and Bridge Engineering.
 3. Levels shown are not rounded down.
 4. Clearance of structures shown is to profile 1.1m below top of road.

Client
NATIONAL HIGHWAYS AUTHORITY OF INDIA
 (Ministry of Shipping, Road Transport & Highways)

Drawing Title: PLAN & PROFILE
 Km. 37+000 to 38+000

Drawing No.: SW/1019/ROADS/PP - 35

Scale: HORZ-1:2000 VER-1:200

Drn: K.L.N **Dgn:** K.PRASAD **Appd:** BRIG.C.D.PURI

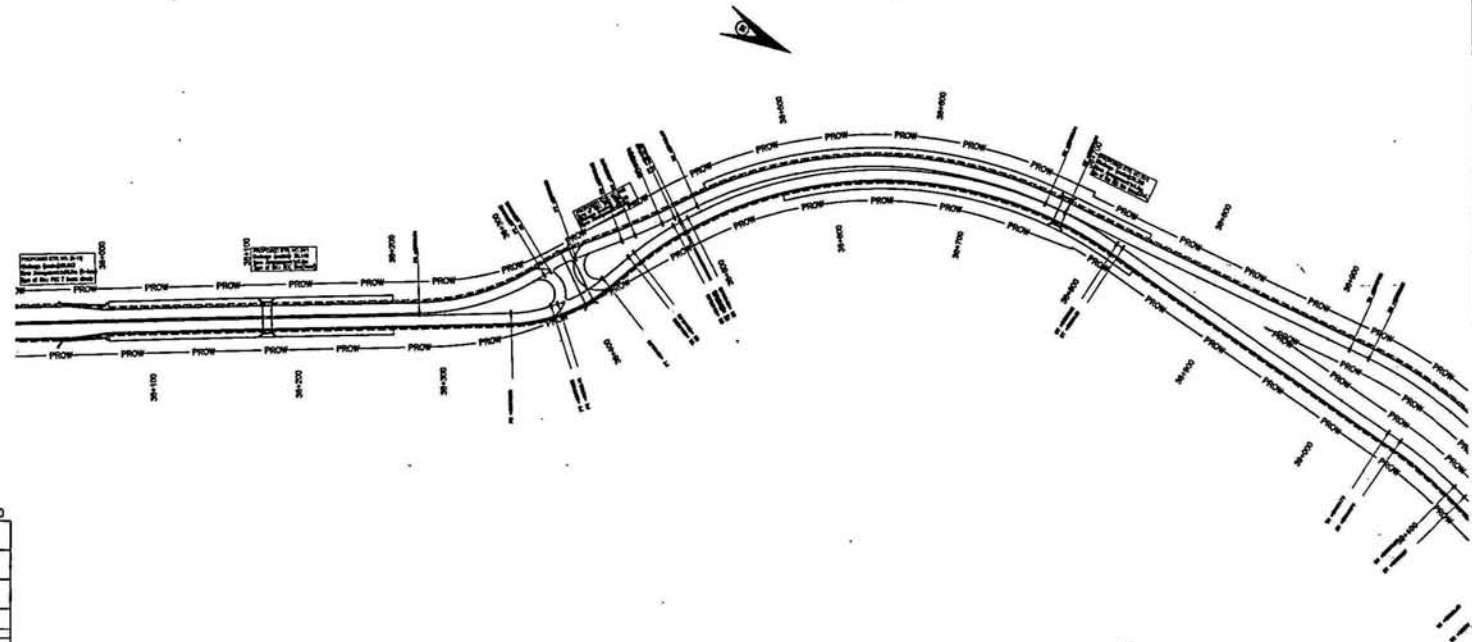
Date: JUNE-2009

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 Fax: (011) 41679350
 E-mail: swinfo@swi.com



← Jammu
101

Udhampur →



Level Datum = 532.000

| |
|---|
| Proposed Level along Left Median Edge (m.) |
| Existing Level Below Left Median Edge (m.) |
| Proposed Level along Right Median Edge (m.) |
| Existing Level Below Right Median Edge (m.) |
| Horizontal Alignment Left |
| Horizontal Alignment Right |
| Vertical Alignment Left |
| Vertical Alignment Right |
| LHS Superelevation |
| RHS Superelevation |
| Left Profile Chainage |
| Right Profile Chainage |

| Code | Legend |
|------|---------------------|
| 1 | Proposed Four Lanes |
| 2 | Proposed Four Lanes |
| 3 | Proposed Four Lanes |
| 4 | Proposed Four Lanes |
| 5 | Proposed Four Lanes |
| 6 | Proposed Four Lanes |
| 7 | Proposed Four Lanes |
| 8 | Proposed Four Lanes |
| 9 | Proposed Four Lanes |
| 10 | Proposed Four Lanes |
| 11 | Proposed Four Lanes |
| 12 | Proposed Four Lanes |
| 13 | Proposed Four Lanes |
| 14 | Proposed Four Lanes |
| 15 | Proposed Four Lanes |
| 16 | Proposed Four Lanes |
| 17 | Proposed Four Lanes |
| 18 | Proposed Four Lanes |
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| 20 | Proposed Four Lanes |
| 21 | Proposed Four Lanes |
| 22 | Proposed Four Lanes |
| 23 | Proposed Four Lanes |
| 24 | Proposed Four Lanes |
| 25 | Proposed Four Lanes |
| 26 | Proposed Four Lanes |
| 27 | Proposed Four Lanes |
| 28 | Proposed Four Lanes |
| 29 | Proposed Four Lanes |
| 30 | Proposed Four Lanes |
| 31 | Proposed Four Lanes |
| 32 | Proposed Four Lanes |
| 33 | Proposed Four Lanes |
| 34 | Proposed Four Lanes |
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| 46 | Proposed Four Lanes |
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| 95 | Proposed Four Lanes |
| 96 | Proposed Four Lanes |
| 97 | Proposed Four Lanes |
| 98 | Proposed Four Lanes |
| 99 | Proposed Four Lanes |
| 100 | Proposed Four Lanes |

Project Title
 Consultancy services for preparation of detailed project report for Rehabilitation and upgrading of existing 2-lanes road to 4-lanes divided carriageway and provision of Jammu - Udhampur section of NH-101 A (Km 0.00 to Km 66.00) including Jammu(Ur) Bypass and Udhampur Bypass Phase - III Programme of North-South corridor Project. Contract Package C-22/14

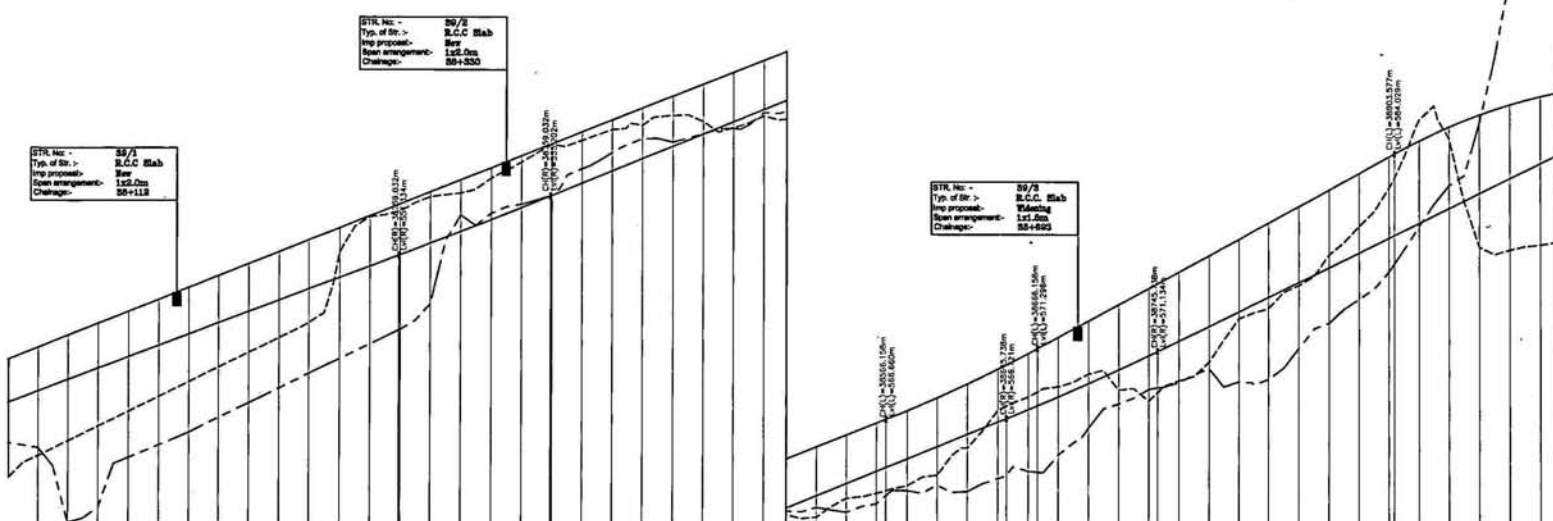
Scale
 Horizontal: 1:2000
 Vertical: 1:200

Client
NATIONAL HIGHWAYS AUTHORITY OF INDIA
 (Ministry of Shipping, Road Transport & Highways)

Drawing Title: PLAN & PROFILE
 Km. 38+000 to 39+000
Drawing No.: SW/101B/ROADS/PP-36
Scale: HOR:-1:2000 VER:-1:200
Date: JUNE-2009

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| Stationing | Proposed Level along Left Median Edge (m) | Existing Level Below Left Median Edge (m) | Proposed Level along Right Median Edge (m) | Existing Level Below Right Median Edge (m) |
|------------|---|---|--|--|
| 3000.000 | 544.519 | 538.174 | 544.519 | 538.174 |
| 3005.000 | 544.520 | 538.175 | 544.520 | 538.175 |
| 3010.000 | 544.521 | 538.176 | 544.521 | 538.176 |
| 3015.000 | 544.522 | 538.177 | 544.522 | 538.177 |
| 3020.000 | 544.523 | 538.178 | 544.523 | 538.178 |
| 3025.000 | 544.524 | 538.179 | 544.524 | 538.179 |
| 3030.000 | 544.525 | 538.180 | 544.525 | 538.180 |
| 3035.000 | 544.526 | 538.181 | 544.526 | 538.181 |
| 3040.000 | 544.527 | 538.182 | 544.527 | 538.182 |
| 3045.000 | 544.528 | 538.183 | 544.528 | 538.183 |
| 3050.000 | 544.529 | 538.184 | 544.529 | 538.184 |
| 3055.000 | 544.530 | 538.185 | 544.530 | 538.185 |
| 3060.000 | 544.531 | 538.186 | 544.531 | 538.186 |
| 3065.000 | 544.532 | 538.187 | 544.532 | 538.187 |
| 3070.000 | 544.533 | 538.188 | 544.533 | 538.188 |
| 3075.000 | 544.534 | 538.189 | 544.534 | 538.189 |
| 3080.000 | 544.535 | 538.190 | 544.535 | 538.190 |
| 3085.000 | 544.536 | 538.191 | 544.536 | 538.191 |
| 3090.000 | 544.537 | 538.192 | 544.537 | 538.192 |
| 3095.000 | 544.538 | 538.193 | 544.538 | 538.193 |
| 3100.000 | 544.539 | 538.194 | 544.539 | 538.194 |
| 3105.000 | 544.540 | 538.195 | 544.540 | 538.195 |
| 3110.000 | 544.541 | 538.196 | 544.541 | 538.196 |
| 3115.000 | 544.542 | 538.197 | 544.542 | 538.197 |
| 3120.000 | 544.543 | 538.198 | 544.543 | 538.198 |
| 3125.000 | 544.544 | 538.199 | 544.544 | 538.199 |
| 3130.000 | 544.545 | 538.200 | 544.545 | 538.200 |
| 3135.000 | 544.546 | 538.201 | 544.546 | 538.201 |
| 3140.000 | 544.547 | 538.202 | 544.547 | 538.202 |
| 3145.000 | 544.548 | 538.203 | 544.548 | 538.203 |
| 3150.000 | 544.549 | 538.204 | 544.549 | 538.204 |
| 3155.000 | 544.550 | 538.205 | 544.550 | 538.205 |
| 3160.000 | 544.551 | 538.206 | 544.551 | 538.206 |
| 3165.000 | 544.552 | 538.207 | 544.552 | 538.207 |
| 3170.000 | 544.553 | 538.208 | 544.553 | 538.208 |
| 3175.000 | 544.554 | 538.209 | 544.554 | 538.209 |
| 3180.000 | 544.555 | 538.210 | 544.555 | 538.210 |
| 3185.000 | 544.556 | 538.211 | 544.556 | 538.211 |
| 3190.000 | 544.557 | 538.212 | 544.557 | 538.212 |
| 3195.000 | 544.558 | 538.213 | 544.558 | 538.213 |
| 3200.000 | 544.559 | 538.214 | 544.559 | 538.214 |
| 3205.000 | 544.560 | 538.215 | 544.560 | 538.215 |
| 3210.000 | 544.561 | 538.216 | 544.561 | 538.216 |
| 3215.000 | 544.562 | 538.217 | 544.562 | 538.217 |
| 3220.000 | 544.563 | 538.218 | 544.563 | 538.218 |
| 3225.000 | 544.564 | 538.219 | 544.564 | 538.219 |
| 3230.000 | 544.565 | 538.220 | 544.565 | 538.220 |
| 3235.000 | 544.566 | 538.221 | 544.566 | 538.221 |
| 3240.000 | 544.567 | 538.222 | 544.567 | 538.222 |
| 3245.000 | 544.568 | 538.223 | 544.568 | 538.223 |
| 3250.000 | 544.569 | 538.224 | 544.569 | 538.224 |
| 3255.000 | 544.570 | 538.225 | 544.570 | 538.225 |
| 3260.000 | 544.571 | 538.226 | 544.571 | 538.226 |
| 3265.000 | 544.572 | 538.227 | 544.572 | 538.227 |
| 3270.000 | 544.573 | 538.228 | 544.573 | 538.228 |
| 3275.000 | 544.574 | 538.229 | 544.574 | 538.229 |
| 3280.000 | 544.575 | 538.230 | 544.575 | 538.230 |
| 3285.000 | 544.576 | 538.231 | 544.576 | 538.231 |
| 3290.000 | 544.577 | 538.232 | 544.577 | 538.232 |
| 3295.000 | 544.578 | 538.233 | 544.578 | 538.233 |
| 3300.000 | 544.579 | 538.234 | 544.579 | 538.234 |
| 3305.000 | 544.580 | 538.235 | 544.580 | 538.235 |
| 3310.000 | 544.581 | 538.236 | 544.581 | 538.236 |
| 3315.000 | 544.582 | 538.237 | 544.582 | 538.237 |
| 3320.000 | 544.583 | 538.238 | 544.583 | 538.238 |
| 3325.000 | 544.584 | 538.239 | 544.584 | 538.239 |
| 3330.000 | 544.585 | 538.240 | 544.585 | 538.240 |
| 3335.000 | 544.586 | 538.241 | 544.586 | 538.241 |
| 3340.000 | 544.587 | 538.242 | 544.587 | 538.242 |
| 3345.000 | 544.588 | 538.243 | 544.588 | 538.243 |
| 3350.000 | 544.589 | 538.244 | 544.589 | 538.244 |
| 3355.000 | 544.590 | 538.245 | 544.590 | 538.245 |
| 3360.000 | 544.591 | 538.246 | 544.591 | 538.246 |
| 3365.000 | 544.592 | 538.247 | 544.592 | 538.247 |
| 3370.000 | 544.593 | 538.248 | 544.593 | 538.248 |
| 3375.000 | 544.594 | 538.249 | 544.594 | 538.249 |
| 3380.000 | 544.595 | 538.250 | 544.595 | 538.250 |
| 3385.000 | 544.596 | 538.251 | 544.596 | 538.251 |
| 3390.000 | 544.597 | 538.252 | 544.597 | 538.252 |
| 3395.000 | 544.598 | 538.253 | 544.598 | 538.253 |
| 3400.000 | 544.599 | 538.254 | 544.599 | 538.254 |

130000

| For Proposed Four Lanes | For Proposed Two Lanes |
|---|---|
| 1. Existing G.P. alignment | 1. Existing G.P. alignment |
| 2. Proposed level along left median edge | 2. Proposed level along left median edge |
| 3. Proposed level along right median edge | 3. Proposed level along right median edge |
| 4. Existing level below left median edge | 4. Existing level below left median edge |
| 5. Existing level below right median edge | 5. Existing level below right median edge |
| 6. Proposed G.P. alignment | 6. Proposed G.P. alignment |
| 7. Proposed level at P.M. | 7. Proposed level at P.M. |
| 8. Existing level at P.M. | 8. Existing level at P.M. |
| 9. Proposed level at P.M. | 9. Proposed level at P.M. |
| 10. Existing level at P.M. | 10. Existing level at P.M. |

Project Title
 Rehabilitation and upgrading of existing 2-lane road to 4-lane divided carriageway configuration of Jammu - Udhampur section of NH-1A (km 0.00 to km 66.00) including Jammu(Ur) Bypass and Udhampur Bypass Phase - III Programme of North-South corridor Project, Contract Package C-2918

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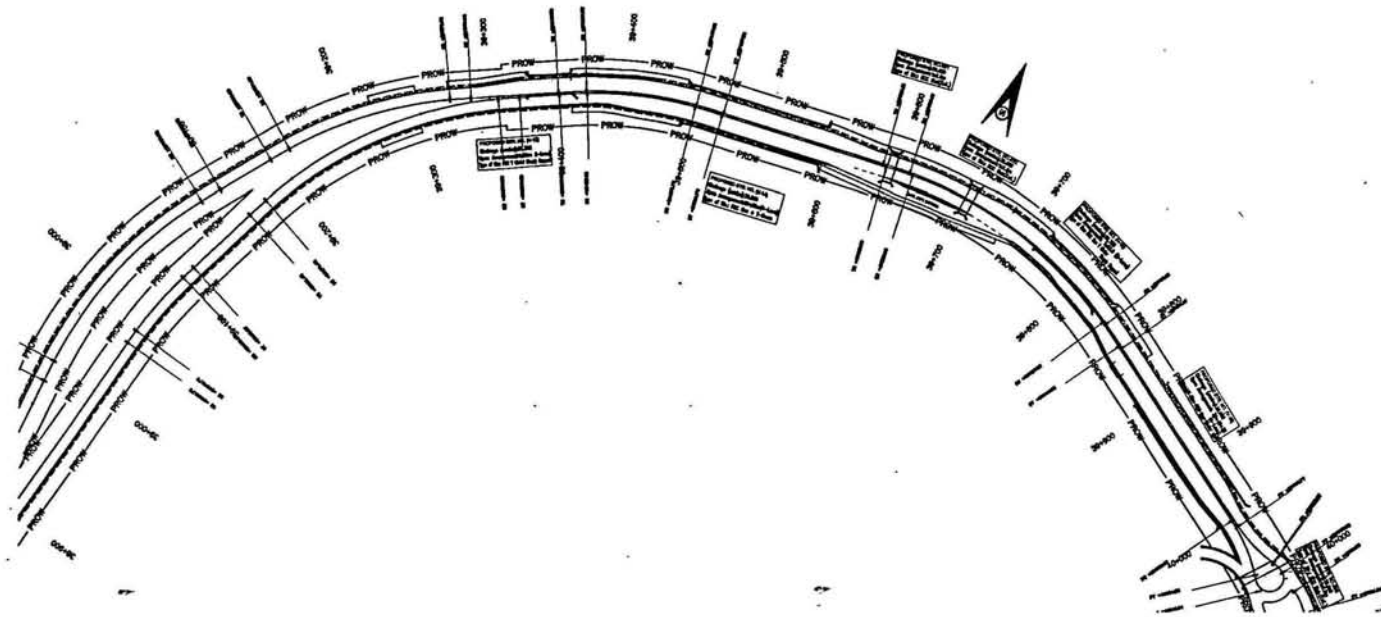
NATIONAL HIGHWAYS AUTHORITY OF INDIA
 (Ministry of Shipping, Road Transport & Highways)

Drawing Title: PLAN & PROFILE
Scale: HOR-1:2000 VER-1:200
Sheet: 2 of 2
Date: JUNE-2009

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← Jammu
103

Udhampur →



Level Datum =574.000

| |
|---|
| Proposed Level along Left Median Edge (m.) |
| Existing Level Below Left Median Edge (m.) |
| Proposed Level along Right Median Edge (m.) |
| Existing Level Below Right Median Edge (m.) |
| Horizontal Alignment Left |
| Horizontal Alignment Right |
| Vertical Alignment Left |
| Vertical Alignment Right |
| LHS Superelevation |
| RHS Superelevation |
| Left Profile Chainage |
| Right Profile Chainage |

| ITEM NO. | DESCRIPTION | QUANTITY | UNIT | REMARKS |
|----------|-------------|----------|------|---------|
| 1 | ... | ... | ... | ... |
| 2 | ... | ... | ... | ... |
| 3 | ... | ... | ... | ... |
| 4 | ... | ... | ... | ... |
| 5 | ... | ... | ... | ... |
| 6 | ... | ... | ... | ... |
| 7 | ... | ... | ... | ... |
| 8 | ... | ... | ... | ... |
| 9 | ... | ... | ... | ... |
| 10 | ... | ... | ... | ... |

Project Title
 Consultancy services for preparation of detailed project report for Rehabilitation and upgrading of existing 2-lane road to 4-lane divided carriageway configuration of Jammu - Udhampur section of NH-1A Bypass Phase - III Programme of North-South corridor Project, Contract Package C-0314

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Client
NATIONAL HIGHWAYS AUTHORITY OF INDIA
 (Ministry of Shipping, Road Transport & Highways)

Drawing Title: PLAN & PROFILE
 Km. 39+000 to 40+000

Drawing No.: SW/1019/ROADS/PP-37

Scale: HOR:-1:2000 VER:-1:200

Date: JUNE-2009

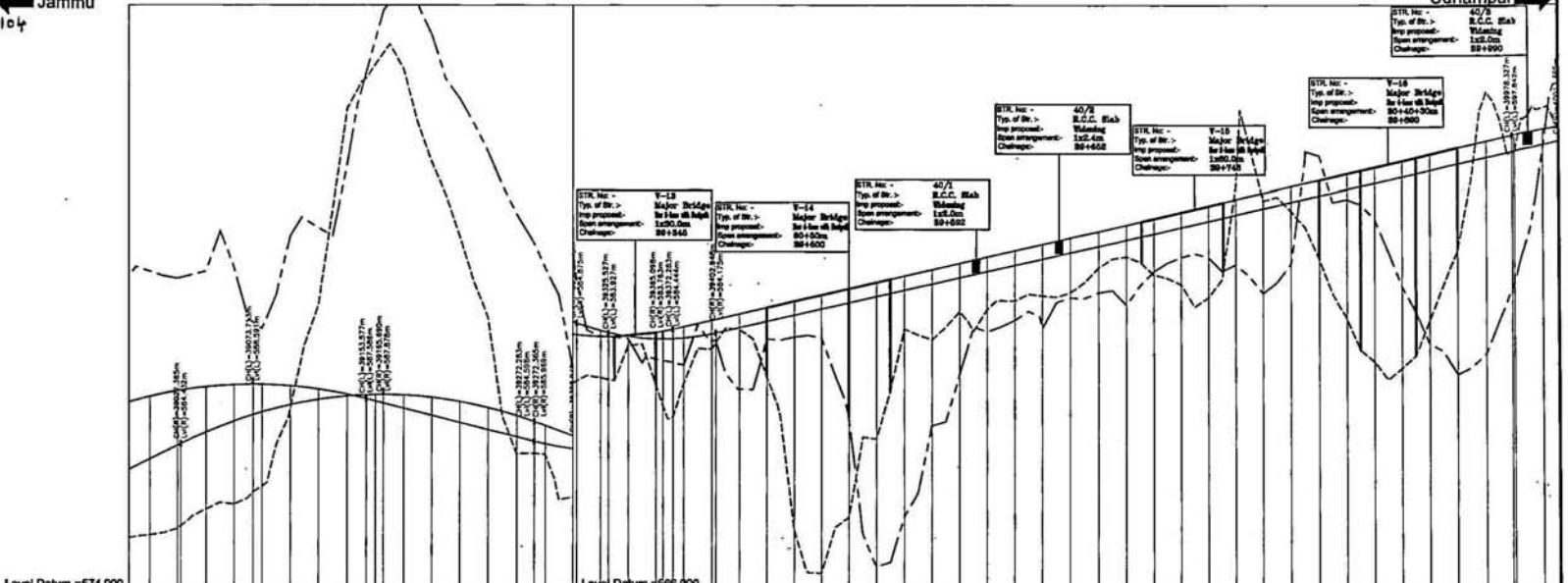
Drawn by: K.L.N. **Checked by:** K.PRASAD **Appd.:** BRG.C.D.PURI

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← Jammu
104

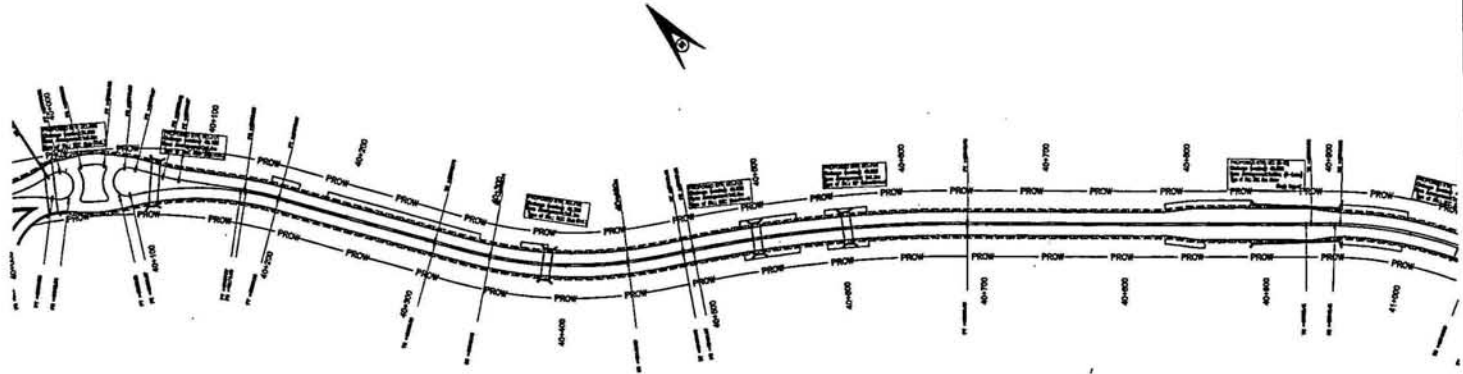
Udhampur →



| | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
|--|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|------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| Proposed Level along Left Median Edge (m.) | 396.481 | 396.527 | 396.573 | 396.619 | 396.665 | 396.711 | 396.757 | 396.803 | 396.849 | 396.895 | 396.941 | 396.987 | 397.033 | 397.079 | 397.125 | 397.171 | 397.217 | 397.263 | 397.309 | 397.355 | 397.401 | 397.447 | 397.493 | 397.539 | 397.585 | 397.631 | 397.677 | 397.723 | 397.769 | 397.815 | 397.861 | 397.907 | 397.953 | 397.999 | 398.045 | 398.091 | 398.137 | 398.183 | 398.229 | 398.275 | 398.321 | 398.367 | 398.413 | 398.459 | 398.505 | 398.551 | 398.597 | 398.643 | 398.689 | 398.735 | 398.781 | 398.827 | 398.873 | 398.919 | 398.965 | 399.011 | 399.057 | 399.103 | 399.149 | 399.195 | 399.241 | 399.287 | 399.333 | 399.379 | 399.425 | 399.471 | 399.517 | 399.563 | 399.609 | 399.655 | 399.701 | 399.747 | 399.793 | 399.839 | 399.885 | 399.931 | 399.977 | 400.023 | 400.069 | 400.115 | 400.161 | 400.207 | 400.253 | 400.299 | 400.345 | 400.391 | 400.437 | 400.483 | 400.529 | 400.575 | 400.621 | 400.667 | 400.713 | 400.759 | 400.805 | 400.851 | 400.897 | 400.943 | 400.989 | 401.035 | 401.081 | 401.127 | 401.173 | 401.219 | 401.265 | 401.311 | 401.357 | 401.403 | 401.449 | 401.495 | 401.541 | 401.587 | 401.633 | 401.679 | 401.725 | 401.771 | 401.817 | 401.863 | 401.909 | 401.955 | 402.001 | 402.047 | 402.093 | 402.139 | 402.185 | 402.231 | 402.277 | 402.323 | 402.369 | 402.415 | 402.461 | 402.507 | 402.553 | 402.599 | 402.645 | 402.691 | 402.737 | 402.783 | 402.829 | 402.875 | 402.921 | 402.967 | 403.013 | 403.059 | 403.105 | 403.151 | 403.197 | 403.243 | 403.289 | 403.335 | 403.381 | 403.427 | 403.473 | 403.519 | 403.565 | 403.611 | 403.657 | 403.703 | 403.749 | 403.795 | 403.841 | 403.887 | 403.933 | 403.979 | 404.025 | 404.071 | 404.117 | 404.163 | 404.209 | 404.255 | 404.301 | 404.347 | 404.393 | 404.439 | 404.485 | 404.531 | 404.577 | 404.623 | 404.669 | 404.715 | 404.761 | 404.807 | 404.853 | 404.899 | 404.945 | 404.991 | 405.037 | 405.083 | 405.129 | 405.175 | 405.221 | 405.267 | 405.313 | 405.359 | 405.405 | 405.451 | 405.497 | 405.543 | 405.589 | 405.635 | 405.681 | 405.727 | 405.773 | 405.819 | 405.865 | 405.911 | 405.957 | 406.003 | 406.049 | 406.095 | 406.141 | 406.187 | 406.233 | 406.279 | 406.325 | 406.371 | 406.417 | 406.463 | 406.509 | 406.555 | 406.601 | 406.647 | 406.693 | 406.739 | 406.785 | 406.831 | 406.877 | 406.923 | 406.969 | 407.015 | 407.061 | 407.107 | 407.153 | 407.199 | 407.245 | 407.291 | 407.337 | 407.383 | 407.429 | 407.475 | 407.521 | 407.567 | 407.613 | 407.659 | 407.705 | 407.751 | 407.797 | 407.843 | 407.889 | 407.935 | 407.981 | 408.027 | 408.073 | 408.119 | 408.165 | 408.211 | 408.257 | 408.303 | 408.349 | 408.395 | 408.441 | 408.487 | 408.533 | 408.579 | 408.625 | 408.671 | 408.717 | 408.763 | 408.809 | 408.855 | 408.901 | 408.947 | 408.993 | 409.039 | 409.085 | 409.131 | 409.177 | 409.223 | 409.269 | 409.315 | 409.361 | 409.407 | 409.453 | 409.499 | 409.545 | 409.591 | 409.637 | 409.683 | 409.729 | 409.775 | 409.821 | 409.867 | 409.913 | 409.959 | 410.005 | 410.051 | 410.097 | 410.143 | 410.189 | 410.235 | 410.281 | 410.327 | 410.373 | 410.419 | 410.465 | 410.511 | 410.557 | 410.603 | 410.649 | 410.695 | 410.741 | 410.787 | 410.833 | 410.879 | 410.925 | 410.971 | 411.017 | 411.063 | 411.109 | 411.155 | 411.201 | 411.247 | 411.293 | 411.339 | 411.385 | 411.431 | 411.477 | 411.523 | 411.569 | 411.615 | 411.661 | 411.707 | 411.753 | 411.799 | 411.845 | 411.891 | 411.937 | 411.983 | 412.029 | 412.075 | 412.121 | 412.167 | 412.213 | 412.259 | 412.305 | 412.351 | 412.397 | 412.443 | 412.489 | 412.535 | 412.581 | 412.627 | 412.673 | 412.719 | 412.765 | 412.811 | 412.857 | 412.903 | 412.949 | 412.995 | 413.041 | 413.087 | 413.133 | 413.179 | 413.225 | 413.271 | 413.317 | 413.363 | 413.409 | 413.455 | 413.501 | 413.547 | 413.593 | 413.639 | 413.685 | 413.731 | 413.777 | 413.823 | 413.869 | 413.915 | 413.961 | 414.007 | 414.053 | 414.099 | 414.145 | 414.191 | 414.237 | 414.283 | 414.329 | 414.375 | 414.421 | 414.467 | 414.513 | 414.559 | 414.605 | 414.651 | 414.697 | 414.743 | 414.789 | 414.835 | 414.881 | 414.927 | 414.973 | 415.019 | 415.065 | 415.111 | 415.157 | 415.203 | 415.249 | 415.295 | 415.341 | 415.387 | 415.433 | 415.479 | 415.525 | 415.571 | 415.617 | 415.663 | 415.709 | 415.755 | 415.801 | 415.847 | 415.893 | 415.939 | 415.985 | 416.031 | 416.077 | 416.123 | 416.169 | 416.215 | 416.261 | 416.307 | 416.353 | 416.399 | 416.445 | 416.491 | 416.537 | 416.583 | 416.629 | 416.675 | 416.721 | 416.767 | 416.813 | 416.859 | 416.905 | 416.951 | 416.997 | 417.043 | 417.089 | 417.135 | 417.181 | 417.227 | 417.273 | 417.319 | 417.365 | 417.411 | 417.457 | 417.503 | 417.549 | 417.595 | 417.641 | 417.687 | 417.733 | 417.779 | 417.825 | 417.871 | 417.917 | 417.963 | 418.009 | 418.055 | 418.101 | 418.147 | 418.193 | 418.239 | 418.285 | 418.331 | 418.377 | 418.423 | 418.469 | 418.515 | 418.561 | 418.607 | 418.653 | 418.699 | 418.745 | 418.791 | 418.837 | 418.883 | 418.929 | 418.975 | 419.021 | 419.067 | 419.113 | 419.159 | 419.205 | 419.251 | 419.297 | 419.343 | 419.389 | 419.435 | 419.481 | 419.527 | 419.573 | 419.619 | 419.665 | 419.711 | 419.757 | 419.803 | 419.849 | 419.895 | 419.941 | 419.987 | 420.033 | 420.079 | 420.125 | 420.171 | 420.217 | 420.263 | 420.309 | 420.355 | 420.401 | 420.447 | 420.493 | 420.539 | 420.585 | 420.631 | 420.677 | 420.723 | 420.769 | 420.815 | 420.861 | 420.907 | 420.953 | 420.999 | 421.045 | 421.091 | 421.137 | 421.183 | 421.229 | 421.275 | 421.321 | 421.367 | 421.413 | 421.459 | 421.505 | 421.551 | 421.597 | 421.643 | 421.689 | 421.735 | 421.781 | 421.827 | 421.873 | 421.919 | 421.965 | 422.011 | 422.057 | 422.103 | 422.149 | 422.195 | 422.241 | 422.287 | 422.333 | 422.379 | 422.425 | 422.471 | 422.517 | 422.563 | 422.609 | 422.655 | 422.701 | 422.747 | 422.793 | 422.839 | 422.885 | 422.931 | 422.977 | 423.023 | 423.069 | 423.115 | 423.161 | 423.207 | 423.253 | 423.299 | 423.345 | 423.391 | 423.437 | 423.483 | 423.529 | 423.575 | 423.621 | 423.667 | 423.713 | 423.759 | 423.805 | 423.851 | 423.897 | 423.943 | 423.989 | 424.035 | 424.081 | 424.127 | 424.173 | 424.219 | 424.265 | 424.311 | 424.357 | 424.403 | 424.449 | 424.495 | 424.541 | 424.587 | 424.633 | 424.679 | 424.725 | 424.771 | 424.817 | 424.863 | 424.909 | 424.955 | 425.001 | 425.047 | 425.093 | 425.139 | 425.185 | 425.231 | 425.277 | 425.323 | 425.369 | 425.415 | 425.461 | 425.507 | 425.553 | 425.599 | 425.645 | 425.691 | 425.737 | 425.783 | 425.829 | 425.875 | 425.921 | 425.967 | 426.013 | 426.059 | 426.105 | 426.151 | 426.197 | 426.243 | 426.289 | 426.335 | 426.381 | 426.427 | 426.473 | 426.519 | 426.565 | 426.611 | 426.657 | 426.703 | 426.749 | 426.795 | 426.841 | 426.887 | 426.933 | 426.979 | 427.025 | 427.071 | 427.117 | 427.163 | 427.209 | 427.255 | 427.301 | 427.347 | 427.393 | 427.439 | 427.485 | 427.531 | 427.577 | 427.623 | 427.669 | 427.715 | 427.761 | 427.807 | 427.853 | 427.899 | 427.945 | 427.991 | 428.037 | 428.083 | 428.129 | 428.175 | 428.221 | 428.267 | 428.313 | 428.359 | 428.405 | 428.451 | 428.497 | 428.543 | 428.589 | 428.635 | 428.681 | 428.727 | 428.773 | 428.819 | 428.865 | 428.911 | 428.957 | 429.003 | 429.049 | 429.095 | 429.141 | 429.187 | 429.233 | 429.279 | 429.325 | 429.371 | 429.417 | 429.463 | 429.509 | 429.555 | 429.601 | 429.647 | 429.693 | 429.739 | 429.785 | 429.831 | 429.877 | 429.923 | 429.969 | 430.015 | 430.061 | 430.107 | 430.153 | 430.199 | 430.245 | 430.291 | 430.337 | 430.383 | 430.429 | 430.475 | 430.521 | 430.567 | 430.613 | 430.659 | 430.705 | 430.751 | 430.797 | 430.843 | 430.889 | 430.935 | 430.981 | 431.027 | 431.073 | 431.119 | 431.165 | 431.211 | 431.257 | 431.303 | 431.349 | 431.395 | 431.441 | 431.487 | 431.533 | 431.579 | 431.625 | 431.671 | 431.717 | 431.763 | 431.809 | 431.855 | 431.901 | 431.947 | 431.993 | 432.039 | 432.085 | 432.131 | 432.177 | 432.223 | 432.269 | 432.315 | 432.361 | 432.407 | 432.453 | 432.499 | 432.545 | 432.591 | 432.637 | 432.683 | 432.729 | 432.775 | 432.821 | 432.867 | 432.913 | 432.959 | 433.005 | 433.051 | 433.097 | 433.143 | 433.189 | 433.235 | 433.281 | 433.327 | 433.373 | 433.419 | 433.465 | 433.511 | 433.557 | 433.603 | 433.649 | 433.695 | 433.741 | 433.787 | 433.833 | 433.879 | 433.925 | 433.971 | 434.017 | 434.063 | 434.109 | 434.155 | 434.201 | 434.247 | 434.293 | 434.339 | 434.385 | 434.431 | 434.477 | 434.523 | 434.569 | 434.615 | 434.661 | 434.707 | 434.753 | 434.799 | 434.845 | 434.891 | 434.937 | 434.983 | 435.029 | 435.075 | 435.121 | 435.167 | 435.213 | 435.259 | 435.305 | 435.351 | 435.397 | 435.443 | 435.489 | 435.535 | 435.581 | 435.627 | 435.673 | 435.719 | 435.765 | 435.811 | 435.857 | 435.903 | 435.949 | 435.995 | 436.041 | 436.087 | 436.133 | 436.179 | 436.225 | 436.271 | 436.317 | 436.363 | 436.409 | 436.455 | 436.501 | 436.547 | 436.593 | 436.639 | 436.685 | 436.731 | 436.777 | 436.823 | 436.869 | 436.915 | 436.961 | 437.007 | 437.053 | 437.099 | 437.145 | 437.191 | 437.237 | 437.283 | 437.329 | 437.375 | 437.421 | 437.467 | 437.513 | 437.559 | 437.605 | 437.651 | 437.697 | 437.743 | 437.789 | 437.835 | 437.881 | 437.927 | 437.973 | 438.019 | 438.065 | 438.111 | 438.157 | 438.203 | 438.249 | 438.295 | 438.341 | 438.387 | 438.433 | 438.479 | 438.525 | 438.571 | 438.617 | 438.663 | 438.709 | 438.755 | 438.801 | 438.847 | 438.893 | 438.939 | 438.985 | 439.031 | 439.077 | 439.123 | 439.169 | 439.215 | 439.261 | 439.307 | 439.353 | 439.399 | 439.445 | 439.491 | 439.537 | 439.583 | 439.629 | 439.675 | 439.721 | 439.767 | 439.813 | 439.859 | 439.905 | 439.951 | 439.997 | 440.043 | 440.089 | 440.135 | 440.181 | 440.227 | 440.273 | 440.319 | 440.365 |
|--|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|------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Jammu
105

Udhampur



| |
|---|
| Level Datum = 568.000 |
| Proposed Level along Left Median Edge (m.) |
| Existing Level Below Left Median Edge (m.) |
| Proposed Level along Right Median Edge (m.) |
| Existing Level Below Right Median Edge (m.) |
| Horizontal Alignment Left |
| Horizontal Alignment Right |
| Vertical Alignment Left |
| Vertical Alignment Right |
| LHS Superelevation |
| RHS Superelevation |
| Left Profile Chainage |
| Right Profile Chainage |

| Sl. No. | Part | Legend |
|---------|----------------------|--------|
| 1. | Proposed Road Limits | ————— |
| 2. | Existing Road Limits | ————— |
| 3. | Right of Way | ————— |
| 4. | Proposed Road | ————— |
| 5. | Existing Road | ————— |
| 6. | Proposed L&T | ————— |
| 7. | Existing L&T | ————— |
| 8. | Proposed L&T | ————— |
| 9. | Existing L&T | ————— |
| 10. | Proposed L&T | ————— |
| 11. | Existing L&T | ————— |

Project Title
 Consultancy services for preparation of detailed project report for Rehabilitation and upgrading of existing 2-lane road to 4-lane divided carriageway modification of Jammu - Udhampur section of NH-1 A (Km 0.00 to Km 64.00) including Jammu/Dun Nigra and Udhampur Bypass Phase - II. Progression of North - South - corridor Project, Contract Package C-0274

Client
 NATIONAL HIGHWAYS AUTHORITY OF INDIA
 (Ministry of Shipping, Road Transport & Highways)

Drawing Title: PLAN & PROFILE
 Km. 40+000 to 41+000

Drawing No.: SWH/1019/ROADS/PP-38

Scale: —:—
 HOR:-1:2000 VER:-1:200

Drawn by: K.L.N.
Checked by: K.PRASAD
Appr. by: BRG.C.D.PURI
Date: JUNE-2009

Sheet No.: 1 of 2

Scale: —:—

Date: JUNE-2009

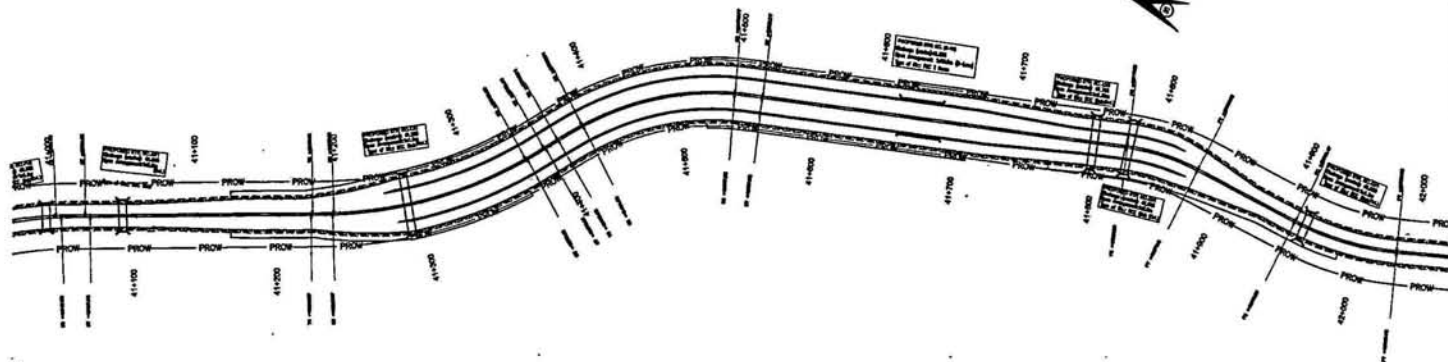
Client: Wilson India Private Limited
 4/374, MOHAN CO-OPERATIVE IND. ESTATE
 SAHIBZADA ROAD
 NEW DELHI - 110044
 Tele: (011) 41679340 - 49
 Fax: (011) 41679350
 E-mail: info@wilson.com



← Jammu

Udhampur →

1b7



Level Datum = 590.000

- Proposed Level along Left Median Edge (m.)
- Existing Level Below Left Median Edge (m.)
- Proposed Level along Right Median Edge (m.)
- Existing Level Below Right Median Edge (m.)
- Horizontal Alignment Left
- Horizontal Alignment Right
- Vertical Alignment Left
- Vertical Alignment Right
- LHS Superelevation
- RHS Superelevation
- Left Profile Chainage
- Right Profile Chainage

| LEGEND: | |
|-----------------------------------|-----------------------------------|
| For Proposed Four Lanes | |
| 1. Existing R.O. alignment | 1. Proposed Left Hand Side Slope |
| 2. Proposed Right Hand Side Slope | 2. Proposed Right Hand Side Slope |
| 3. Proposed Left Hand Side Slope | 3. Existing Left Hand Side Slope |
| 4. Proposed Right Hand Side Slope | 4. Existing Right Hand Side Slope |
| 5. Proposed L.S. | 5. Existing L.S. |
| 6. Proposed R.S. | 6. Existing R.S. |
| 7. Proposed L.S. & R.S. | 7. Existing L.S. & R.S. |
| 8. Proposed L.S. & R.S. | 8. Existing L.S. & R.S. |

Project Title
 Consultancy services for preparation of detailed project report for Rehabilitation and upgrading of existing 2-lane road to 4-lane divided carriageway configuration of Jammu - Udhampur section of NH-1A (Km 0.00 to Km 64.00) including Jammu(Ur) Bypass and Udhampur Bypass Phase - II Progression of North - South - corridor Project. Contract Package C-0211

Site situated in the vicinity of Jammu Udhampur section NH-1A and road will be widened on the existing alignment and will be provided with 4 lanes divided carriageway configuration and other facilities as per the requirements of the design. The road will be widened on the existing alignment and will be provided with 4 lanes divided carriageway configuration and other facilities as per the requirements of the design.

Client
NATIONAL HIGHWAYS AUTHORITY OF INDIA
 (Ministry of Shipping, Road Transport & Highways)

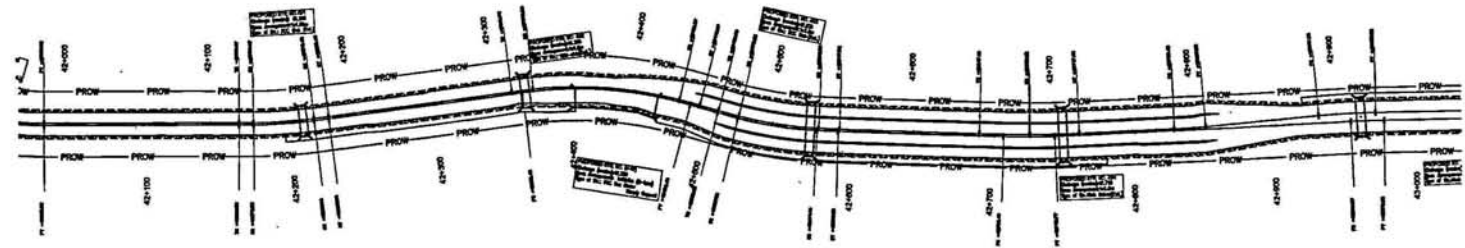
Drawing Title: PLAN & PROFILE
 Km. 41+000 to 42+000
Drawing No.: SW/101/ROADS/PP - 39
Scale: HOR:-1:2000 VEC:-1:200
Date: JUNE-2009
Drawn by: K.L.N. **Checked by:** K.PRASAD **Appd. by:** BRG.C.D.PUR

Scott Wilson India Private Limited
 M-25/4, MOHAN CO-OPERATIVE RD. ESTATE
 MAYAPUR ROAD
 NEW DELHI - 110044
 Tel: (011) 41679340 - 49
 Fax: (011) 41679350
 E-mail: swinfo@swi.com



← Jammu
1b9

Udhampur →



Level Datum =604.000

| |
|---|
| Proposed Level along Left Median Edge (m.) |
| Existing Level Below Left Median Edge (m.) |
| Proposed Level along Right Median Edge (m.) |
| Existing Level Below Right Median Edge (m.) |
| Horizontal Alignment Left |
| Horizontal Alignment Right |
| Vertical Alignment Left |
| Vertical Alignment Right |
| LHS Superelevation |
| RHS Superelevation |
| Left Profile Chainage |
| Right Profile Chainage |

| Rev | Proposed Year | Leaving |
|-----|---------------|--|
| 1 | 2011 | 1. Existing 4L of roadway |
| 2 | 2011 | 2. Proposed 4L along Left Median Edge |
| 3 | 2011 | 3. Proposed 4R along Right Median Edge |
| 4 | 2011 | 4. Existing 4L along Left Median Edge |
| 5 | 2011 | 5. Existing 4R along Right Median Edge |
| 6 | 2011 | 6. Road 4L & 4R |
| 7 | 2011 | 7. Road 4L & 4R |

Project Title
 Consultancy services for preparation of detailed project report for rehabilitation and upgrading of existing 2-lane road to 4-lane divided carriageway consisting of Jammu - Udhampur section of NH-1A (km 0.00 to km 64.00) including Jammu(Ur) Nagrova and Udhampur bypass Phase - II Progression of North-South corridor Project, Contract Package C-2318

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Notes:
 1. Refer to the contract documents for all details.
 2. The drawings shall be in 1:2000 scale unless otherwise stated.
 3. The drawings shall be in 1:200 scale unless otherwise stated.
 4. The drawings shall be in 1:200 scale unless otherwise stated.
 5. The drawings shall be in 1:200 scale unless otherwise stated.
 6. The drawings shall be in 1:200 scale unless otherwise stated.
 7. The drawings shall be in 1:200 scale unless otherwise stated.

Client
NATIONAL HIGHWAYS AUTHORITY OF INDIA
 (Ministry of Shipping, Road Transport & Highways)

Drawing Title: PLAN & PROFILE
 Km. 42+000 to 43+000

Drawing No.: SW/1018/ROADS/PP-40

Scale: HOR:-1:2000 VER:-1:200

Dwn: K.L.N
Dgn: K.PRASAD
Appd: BRIG.C.D.PURI
Date: JUNE-2009

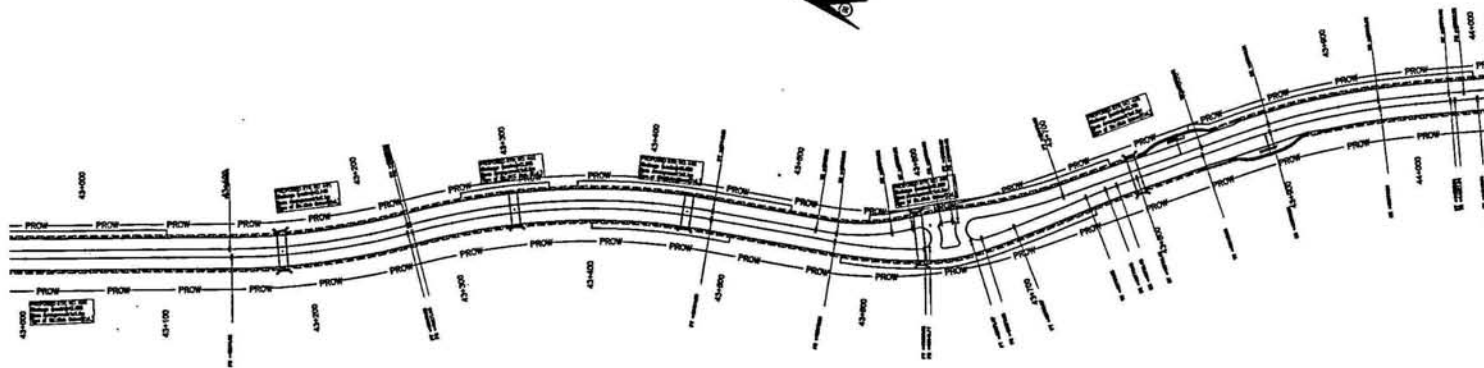


Scott Wilson India Private Limited
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 Fax: (011) 41679350
 E-mail: s.w.in@scottwilson.com



← Jammu
|||

Udhampur →



Level Datum = 618.000

| |
|---|
| Proposed Level along Left Median Edge (m.) |
| Existing Level Below Left Median Edge (m.) |
| Proposed Level along Right Median Edge (m.) |
| Existing Level Below Right Median Edge (m.) |
| Horizontal Alignment Left |
| Horizontal Alignment Right |
| Vertical Alignment Left |
| Vertical Alignment Right |
| LHS Super-elevation |
| RHS Super-elevation |
| Left Profile Chainage |
| Right Profile Chainage |

| | |
|----------|---------------------------------|
| REVISION | |
| No. | Description |
| 1 | As per approved Tender Envelope |
| 2 | As per approved Tender Envelope |
| 3 | As per approved Tender Envelope |
| 4 | As per approved Tender Envelope |
| 5 | As per approved Tender Envelope |
| 6 | As per approved Tender Envelope |
| 7 | As per approved Tender Envelope |
| 8 | As per approved Tender Envelope |
| 9 | As per approved Tender Envelope |
| 10 | As per approved Tender Envelope |

Project Title
Consultancy services for preparation of detailed project report for Rehabilitation and upgrading of existing 2-lane road to 4-lane divided carriageway configuration of Jammu - Udhampur section of NH-1A (km 0.00 to km 64.00) including Jammu(Ur) Nagrota and Udhampur Bypass Phase - III Programme of North-South corridor Project. Contract Package C-2011

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Author: K. Prasad
Checked: K. Prasad
Drawn: K. Prasad
Scale: As shown
Date: June-2009

Client
NATIONAL HIGHWAYS AUTHORITY OF INDIA
(Ministry of Shipping, Road Transport & Highways)

Drawing Title: PLAN & PROFILE
Km. 43+000 to 44+000

Drawing No.: SW/1019/ROADS/PP-41

Scale: HOR:-1:2000 VER:-1:200

Sheet No.: 1 of 2

Date: JUNE-2009

Drn.: K.L.N
Dgn.: K.PRASAD
Appd.: BRG.C.D.PURI

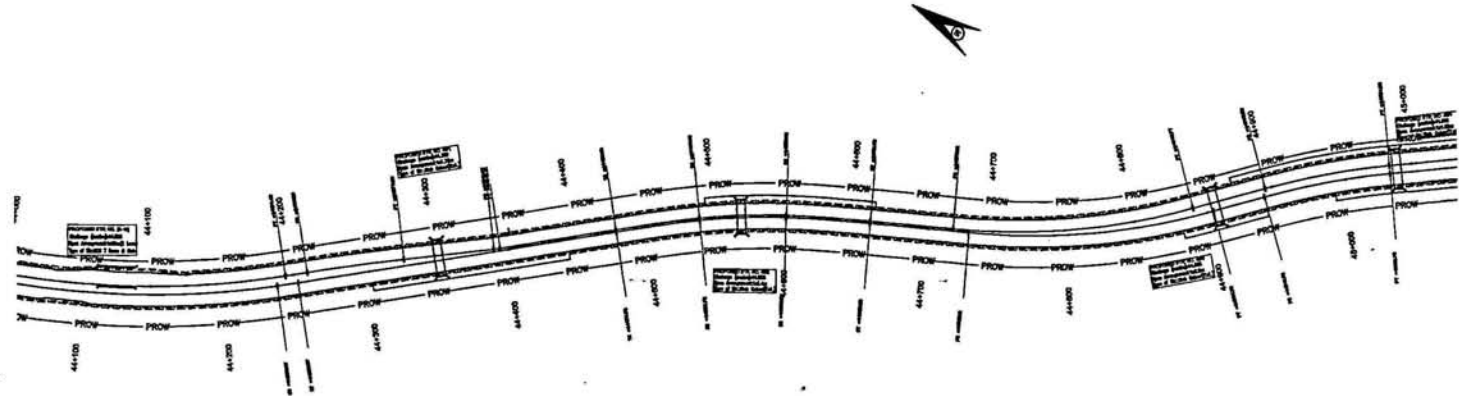
Client: Wilson India Private Limited
MONAN CO-OPERATIVE IND. ESTATE
MATHURA ROAD
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E-mail: prindia@rediffmail.com

Scott Wilson



← Jammu
13

Udhampur →



Level Datum = 630.000

| |
|---|
| Proposed Level along Left Median Edge (m.) |
| Existing Level Below Left Median Edge (m.) |
| Proposed Level along Right Median Edge (m.) |
| Existing Level Below Right Median Edge (m.) |
| Horizontal Alignment Left |
| Horizontal Alignment Right |
| Vertical Alignment Left |
| Vertical Alignment Right |
| LHS Superelevation |
| RHS Superelevation |
| Left Profile Chainage |
| Right Profile Chainage |

| LEGEND | |
|------------------------|--------|
| For | Symbol |
| Proposed Level | — |
| Existing Level | - - - |
| Horizontal Alignment | — |
| Vertical Alignment | — |
| Left Median Edge | — |
| Right Median Edge | — |
| Left Shoulder | — |
| Right Shoulder | — |
| Left Side Slope | — |
| Right Side Slope | — |
| Left Profile Chainage | — |
| Right Profile Chainage | — |
| Level Datum | — |
| Scale | — |

Project Title
 Consultancy services for preparation of detailed project report for Rehabilitation and upgrading of existing 2-lane road to 4-lane divided Carriageway configuration of Jammu - Udhampur section of NH-1 A (Km 0.00 to Km 64.00) including Jammu (Part) Nagrova and Udhampur Bypass Phase - II Programme of North-South-corridor Project, Contract Package C-02/18

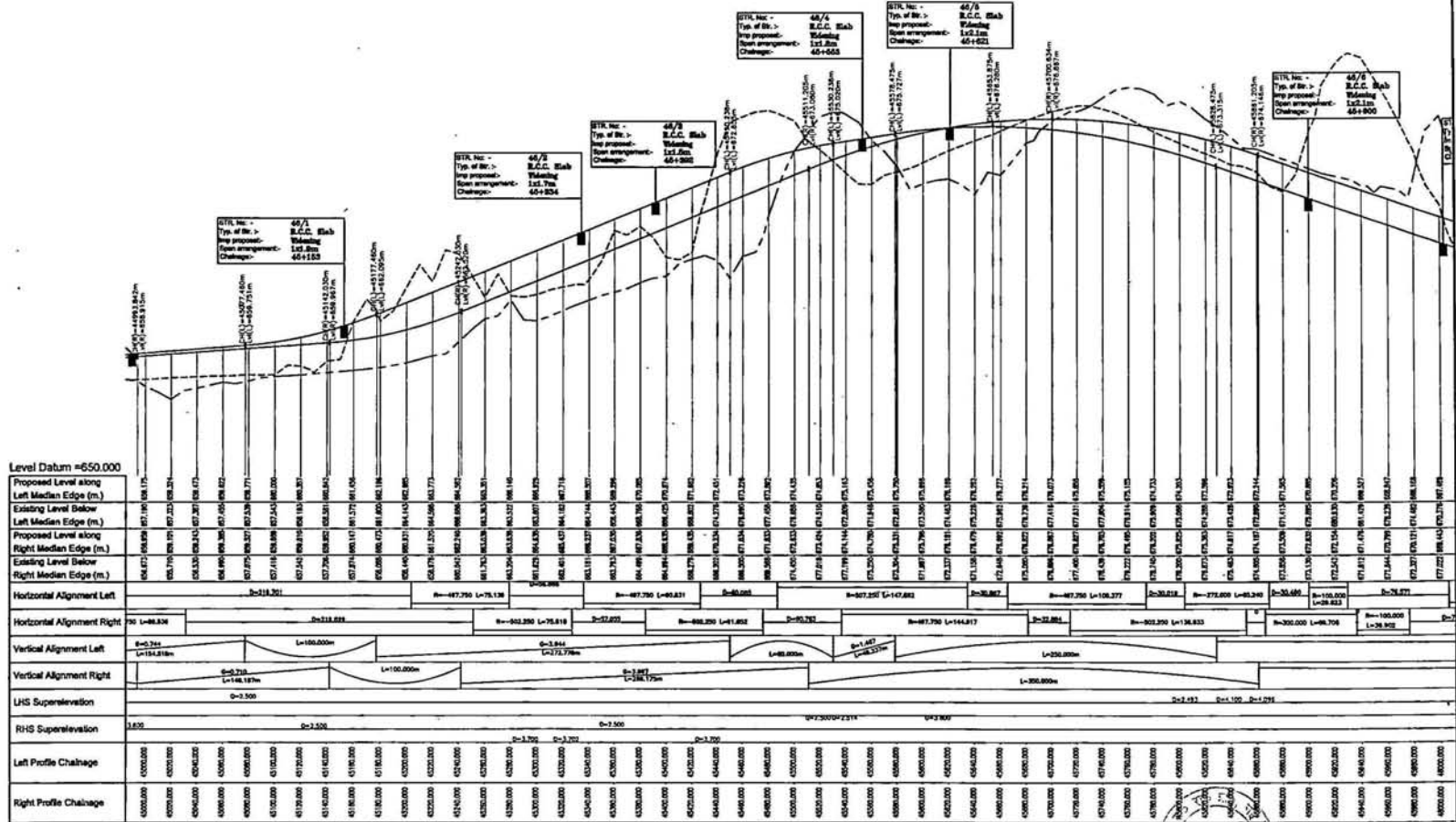
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Client
NATIONAL HIGHWAYS AUTHORITY OF INDIA
 (Ministry of Shipping, Road Transport & Highways)

Drawing Title: PLAN & PROFILE
 Km. 44+000 to 45+000
Drawing No.: SW/1019/ROADS/PP-42
 Scale: — HOR: 1:2000 VER: 1:200
 Dn: KLN Dgn: K.PRASAD Apod: BRG.C.D.PUR Date: JUNE-2009

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 NEW DELHI - 110044
 Tel: (011) 41679340 - 49
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 E-mail: scw@scw.com





LEGEND

| For | Profile | Plan |
|--------------|--------------|--------------|
| 1. Existing | 1. Existing | 1. Existing |
| 2. Proposed | 2. Proposed | 2. Proposed |
| 3. Proposed | 3. Proposed | 3. Proposed |
| 4. Proposed | 4. Proposed | 4. Proposed |
| 5. Proposed | 5. Proposed | 5. Proposed |
| 6. Proposed | 6. Proposed | 6. Proposed |
| 7. Proposed | 7. Proposed | 7. Proposed |
| 8. Proposed | 8. Proposed | 8. Proposed |
| 9. Proposed | 9. Proposed | 9. Proposed |
| 10. Proposed | 10. Proposed | 10. Proposed |

Project Title
 Consultancy services for preparation of detailed project report for rehabilitation and upgrading of existing 2-lane road to 4-lane divided carriageway configuration of Jammu - Udhampur section of NH-1A (Km 0+00 to Km 66+00) including Jammu(Ur) Nagrova and Udhampur Bypass Phase - III Programme of North-South corridor Project, Contract Package C-0018

Client
NATIONAL HIGHWAYS AUTHORITY OF INDIA
 (Ministry of Shipping, Road Transport & Highways)

Drawing Title: PLAN & PROFILE
 Km. 45+000 to 46+000

Drawing No.: SW/1019/ROADS/PP-43

Scale: HOR:-1:2000 VER:-1:200

Date: JUNE-2009

Prepared by: BRG.C.D.PUR

Checked by: K.PRASAD

Approved by: BRG.C.D.PUR

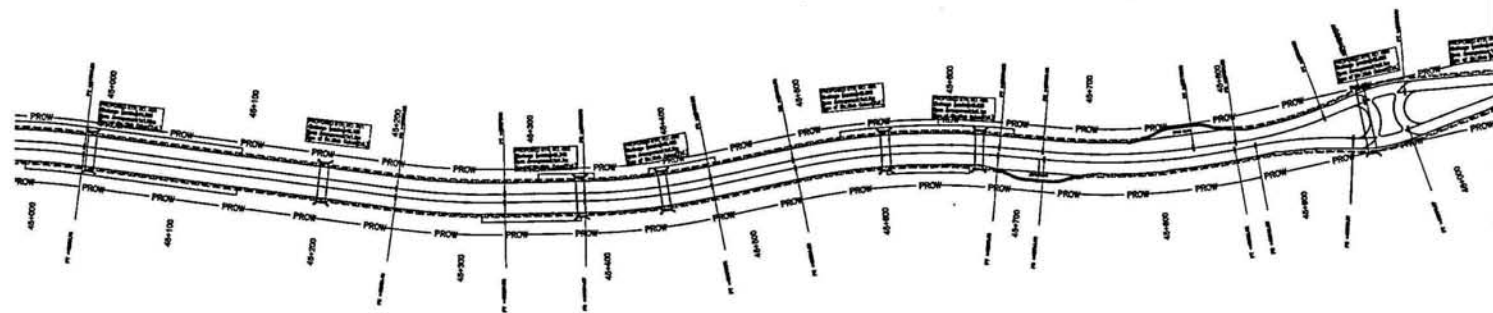
Client: National Highways Authority of India

Consultant: Scott Wilson India Private Limited
 MOHAN CO-OPERATIVE IND. ESTATE
 WDHARA ROAD
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 Fax: (011) 41679350
 E-mail: rpi@swi.com



← Jammu
115

Udhampur →



Level Datum = 650.000

| |
|---|
| Proposed Level along Left Median Edge (m.) |
| Existing Level Below Left Median Edge (m.) |
| Proposed Level along Right Median Edge (m.) |
| Existing Level Below Right Median Edge (m.) |
| Horizontal Alignment Left |
| Horizontal Alignment Right |
| Vertical Alignment Left |
| Vertical Alignment Right |
| LHS Superelevation |
| RHS Superelevation |
| Left Profile Chainage |
| Right Profile Chainage |

| LEGEND | |
|--|--|
| For | Proposed / Four Lanes |
| 1. Existing C.C. of carriageway | 1. Existing C.C. of carriageway |
| 2. Existing Right Side | 2. Proposed Left Side Left Median Edge |
| 3. Existing Left Side | 3. Proposed Left Side Right Median Edge |
| 4. Existing Right Side Right Median Edge | 4. Existing Left Side Left Median Edge |
| 5. Existing Left Side | 5. Existing Left Side Right Median Edge |
| 6. Existing Right Side | 6. Existing Left Side Right Median Edge |
| 7. Existing Left Side | 7. Existing Left Side Right Median Edge |
| 8. Existing Right Side | 8. Existing Left Side Right Median Edge |
| 9. Existing Left Side | 9. Existing Left Side Right Median Edge |
| 10. Existing Right Side | 10. Existing Left Side Right Median Edge |

Project Title
 Consultancy services for preparation of detailed project report for rehabilitation and upgrading of existing 2-lane road to 4-lane divided carriageway configuration of Jammu - Udhampur section of NH-1A. (Km 0.00 to km 64.90) including Jammu/Var/ Nagrota and Udhampur Bypass Phase - III Programme of North-South corridor Project. Contract Package C-0718

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Scale :- HORIZ - 1:2000 VERT - 1:200

Date: JUNE-2009

Client
NATIONAL HIGHWAYS AUTHORITY OF INDIA
 (Ministry of Shipping, Road Transport & Highways)

Drawing Title: PLAN & PROFILE
 Km. 45+000 to 46+000

Drawing No.: SW/1019/ROADS/PP-43

Scale :- HORIZ - 1:2000 VERT - 1:200

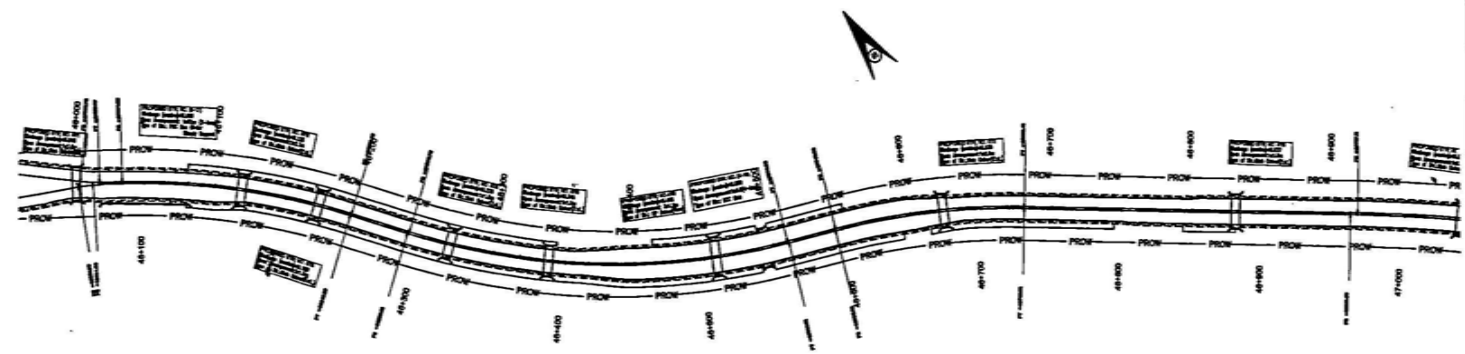
Date: JUNE-2009

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← Jammu
11 →

Udhampur →



Level Datum = 644.000

| |
|---|
| Proposed Level along Left Median Edge (m.) |
| Existing Level Below Left Median Edge (m.) |
| Proposed Level along Right Median Edge (m.) |
| Existing Level Below Right Median Edge (m.) |
| Horizontal Alignment Left |
| Horizontal Alignment Right |
| Vertical Alignment Left |
| Vertical Alignment Right |
| LHS Superelevation |
| RHS Superelevation |
| Left Profile Chainage |
| Right Profile Chainage |

| LEGEND | |
|--------------|--------------|
| For Proposed | For Existing |
| 1. Road | 1. Road |
| 2. Footpath | 2. Footpath |
| 3. Drainage | 3. Drainage |
| 4. Utility | 4. Utility |
| 5. Boundary | 5. Boundary |
| 6. Survey | 6. Survey |
| 7. Proposed | 7. Existing |
| 8. Proposed | 8. Existing |
| 9. Proposed | 9. Existing |
| 10. Proposed | 10. Existing |
| 11. Proposed | 11. Existing |
| 12. Proposed | 12. Existing |
| 13. Proposed | 13. Existing |
| 14. Proposed | 14. Existing |
| 15. Proposed | 15. Existing |
| 16. Proposed | 16. Existing |
| 17. Proposed | 17. Existing |
| 18. Proposed | 18. Existing |
| 19. Proposed | 19. Existing |
| 20. Proposed | 20. Existing |

Project Title
 Consultancy services for preparation of detailed project report for rehabilitation and upgrading of existing 2-lane road to 4-lane divided category configuration of Jammu - Udhampur section of NH-1A (km 0+00 to km 66+00) including Jammu(Pur) Nagar and Udhampur District Phase - III Programme of North-South corridor Project. Contract Package C/0718

Client
NATIONAL HIGHWAYS AUTHORITY OF INDIA
 (Ministry of Shipping, Road Transport & Highways)

Drawing Title: PLAN & PROFILE
 Km. 46+000 to 47+000
Drawing No.: SW/1015/ROADS/PP-44
Scale: -/-
Drawn by: K.L.N
Checked by: K.PRASAD
Approved by: BRIG.C.D.PURI
Date: JUNE-2009

Sheet: 1 of 2
Scale: NSR-1:2000 VER-1:200
Date: JUNE-2009

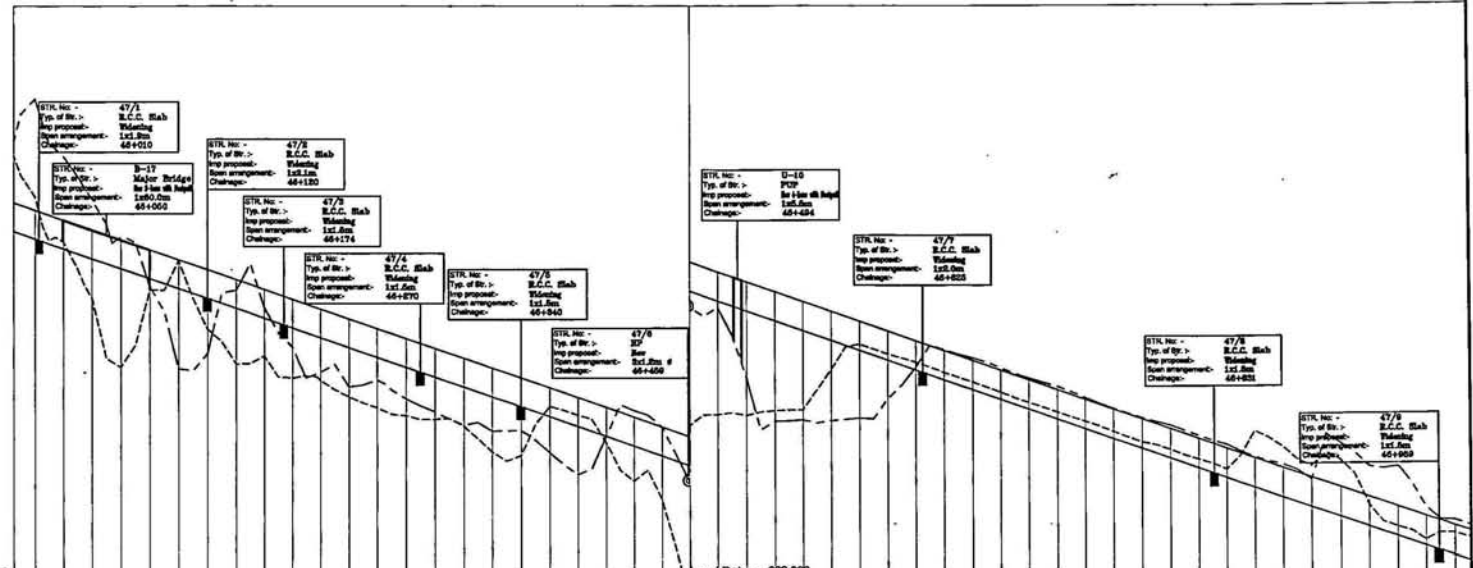


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 E-mail: scw@scwpl.com



← Jammu
11B

Udhampur →



| | | | |
|---|---------------------|------------------------|---------|
| Level Datum = +544.000 | | Level Datum = +632.000 | |
| Proposed Level along Left Median Edge (m.) | 617.752 | 617.752 | 617.752 |
| Existing Level Below Left Median Edge (m.) | 617.752 | 617.752 | 617.752 |
| Proposed Level along Right Median Edge (m.) | 617.752 | 617.752 | 617.752 |
| Existing Level Below Right Median Edge (m.) | 617.752 | 617.752 | 617.752 |
| Horizontal Alignment Left | B=525.350 L=183.828 | | |
| Horizontal Alignment Right | B=525.350 L=183.828 | | |
| Vertical Alignment Left | L=1903.870m | | |
| Vertical Alignment Right | L=1903.870m | | |
| LHS Superelevation | e=2.500 | | |
| RHS Superelevation | e=2.500 | | |
| Left Profile Chainage | 40000.000 | | |
| Right Profile Chainage | 40000.000 | | |

| Rev. | Description |
|------|--------------------------|
| 1 | As per approved & issued |
| 2 | As per approved & issued |
| 3 | As per approved & issued |
| 4 | As per approved & issued |
| 5 | As per approved & issued |
| 6 | As per approved & issued |
| 7 | As per approved & issued |
| 8 | As per approved & issued |
| 9 | As per approved & issued |
| 10 | As per approved & issued |

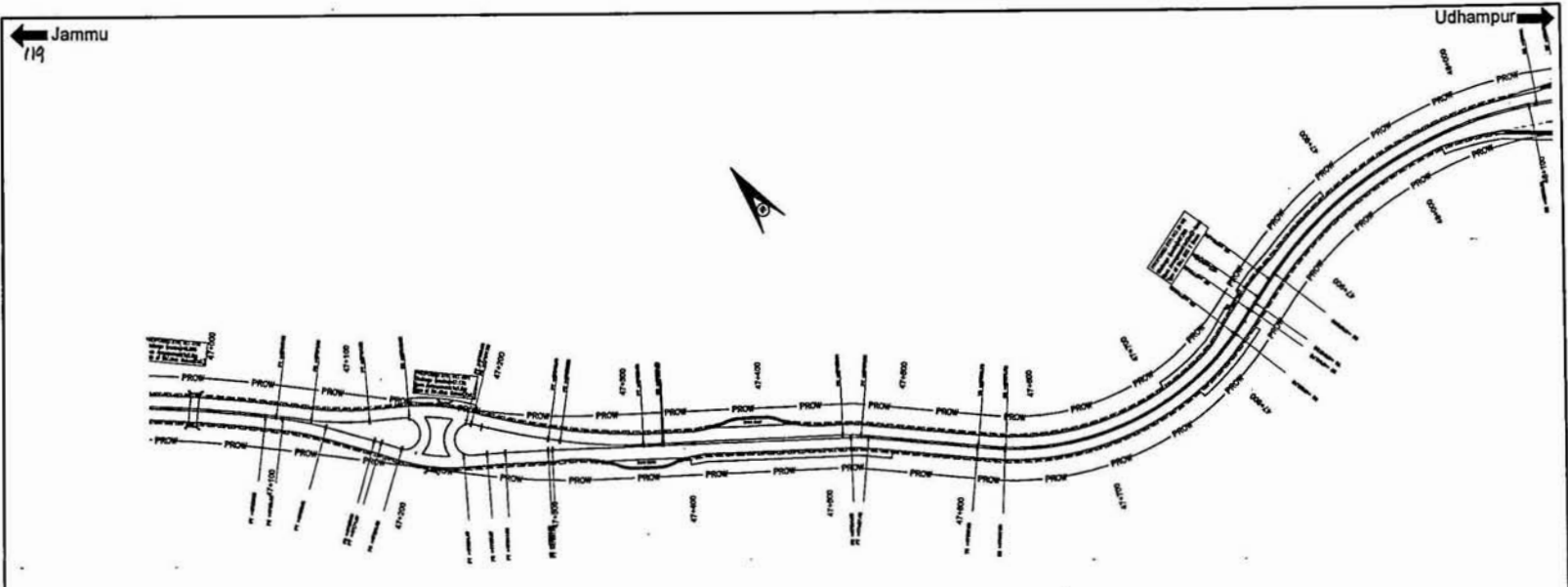
Project Title
 Rehabilitation and upgrading of existing 2-lane road to 4-lane divided carriageway and provision of Jammu - Udhampur section of NH-1A Class 0 to km 66.00 (including Jammu-Udhampur) bypass and Udhampur Bypass Phase - II Programme of North-South corridor Project, Contract Package C-018

NATIONAL HIGHWAYS AUTHORITY OF INDIA
 (Ministry of Shipping, Road Transport & Highways)

Drawing Title: PLAN & PROFILE
 Km. 46+000 to 47+000
Drawing No.: SW/1018/ROADS/PP-44
Scale: HOR:- 1:2000 VER:- 1:200
Date: JUNE-2009

Wilson India Private Limited
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 Fax: (011) 41679350
 E-mail: wipro@wipro.com





Level Datum = 598.000

| |
|---|
| Proposed Level along Left Median Edge (m.) |
| Existing Level Below Left Median Edge (m.) |
| Proposed Level along Right Median Edge (m.) |
| Existing Level Below Right Median Edge (m.) |
| Horizontal Alignment Left |
| Horizontal Alignment Right |
| Vertical Alignment Left |
| Vertical Alignment Right |
| LHS Superelevation |
| RHS Superelevation |
| Left Profile Chainage |
| Right Profile Chainage |

| LEGEND: | |
|-------------------|-------------------------------|
| For | Symbol/Line Style |
| 1. Title | 1. Solid line |
| 2. Subtitle | 2. Dashed line |
| 3. Proposed level | 3. Solid line with dots |
| 4. Existing level | 4. Dashed line with dots |
| 5. Proposed L.S. | 5. Solid line with triangles |
| 6. Existing L.S. | 6. Dashed line with triangles |
| 7. Proposed R.S. | 7. Solid line with squares |
| 8. Existing R.S. | 8. Dashed line with squares |
| 9. Proposed C.S. | 9. Solid line with circles |
| 10. Existing C.S. | 10. Dashed line with circles |
| 11. Proposed P.C. | 11. Solid line with diamonds |
| 12. Existing P.C. | 12. Dashed line with diamonds |

Project Title
 Consultancy services for preparation of detailed project report for Rehabilitation and upgrading of existing 2-lane road to 4-lane divided carriageway and geometrics of Jammu - Udhampur section of NH-1A (km 0.00 to km 46.00) including Jammu (Pvt) Bypass and Udhampur Bypass Phase - III Programme of North-South corridor Project, Contract Package C-019

Client
 NATIONAL HIGHWAYS AUTHORITY OF INDIA
 (Ministry of Shipping, Road Transport & Highways)

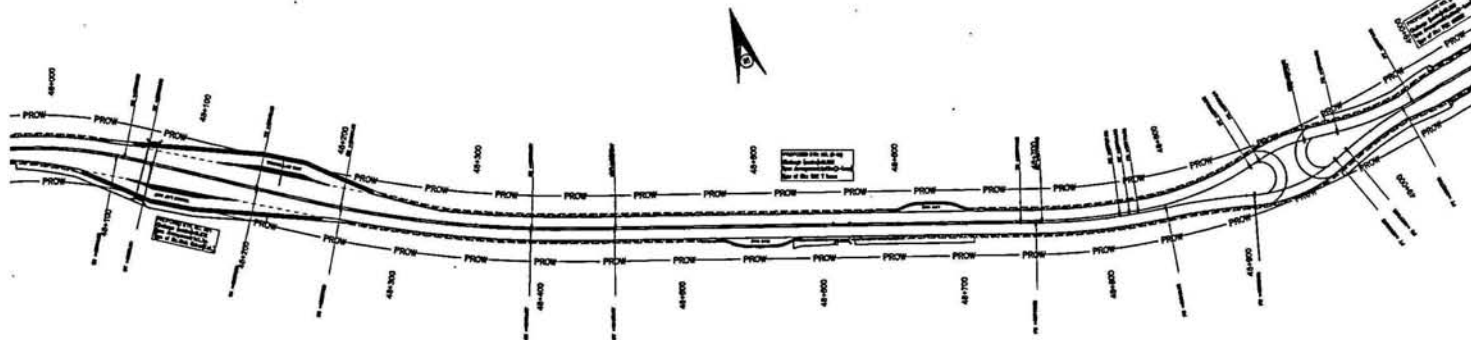
Drawing Title: PLAN & PROFILE
 Km. 47+000 to 48+000
Drawing No.: SW/1018/ROADS/PP-45
Scale: ---
Drawn by: K.L.N
Checked by: K.PRASAD
Approved by: BRO.C.D.PURI
Date: JUNE-2009

Client: Scott Wilson India Private Limited
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 E-mail: swhindia@swi.com



← Jammu
12.1

Udhampur →



Level Datum = 574.000

| |
|---|
| Proposed Level along Left Median Edge (m.) |
| Existing Level Below Left Median Edge (m.) |
| Proposed Level along Right Median Edge (m.) |
| Existing Level Below Right Median Edge (m.) |
| Horizontal Alignment Left |
| Horizontal Alignment Right |
| Vertical Alignment Left |
| Vertical Alignment Right |
| LHS Superelevation |
| RHS Superelevation |
| Left Profile Chainage |
| Right Profile Chainage |

| LEGEND | |
|-----------------------------|-----------------------------|
| For Proposed | For Existing |
| 1. Right | 1. Right |
| 2. Left | 2. Left |
| 3. Center Line | 3. Center Line |
| 4. Proposed Left Hand Side | 4. Existing Left Hand Side |
| 5. Proposed Right Hand Side | 5. Existing Right Hand Side |
| 6. Proposed Left Hand Side | 6. Existing Left Hand Side |
| 7. Proposed Right Hand Side | 7. Existing Right Hand Side |
| 8. Street Set to File | 8. Street Set to File |
| 9. Street Set to File | 9. Street Set to File |

Project Title
 Consultancy services for preparation of detailed project report for Rehabilitation and upgrading of existing 2-lane road to 4-lane divided carriageway configuration of Jammu - Udhampur section of NH-1A (Km 0.00 to Km 66.00) including Jammu(Pur) Nagrova and Udhampur Bypass Phase - III Programme of North - South corridor Project, Contract Package C-02/18

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 4. The drawings shall be in SI units and shall conform to the Indian Standards (IS) and other relevant standards.
 5. The drawings shall be in SI units and shall conform to the Indian Standards (IS) and other relevant standards.

Client
NATIONAL HIGHWAYS AUTHORITY OF INDIA
 (Ministry of Shipping, Road Transport & Highways)

Drawing Title: PLAN & PROFILE
 Km. 48+000 to 49+000
Drawing No.: SW/1019/ROADS/PP-46
 Scale: --- HOR:-1:2000 VER:-1:200
 Dn: KLN Dgn: K.PRASAD Appd: BRG.C.D.PURI Date: JUNE-2009



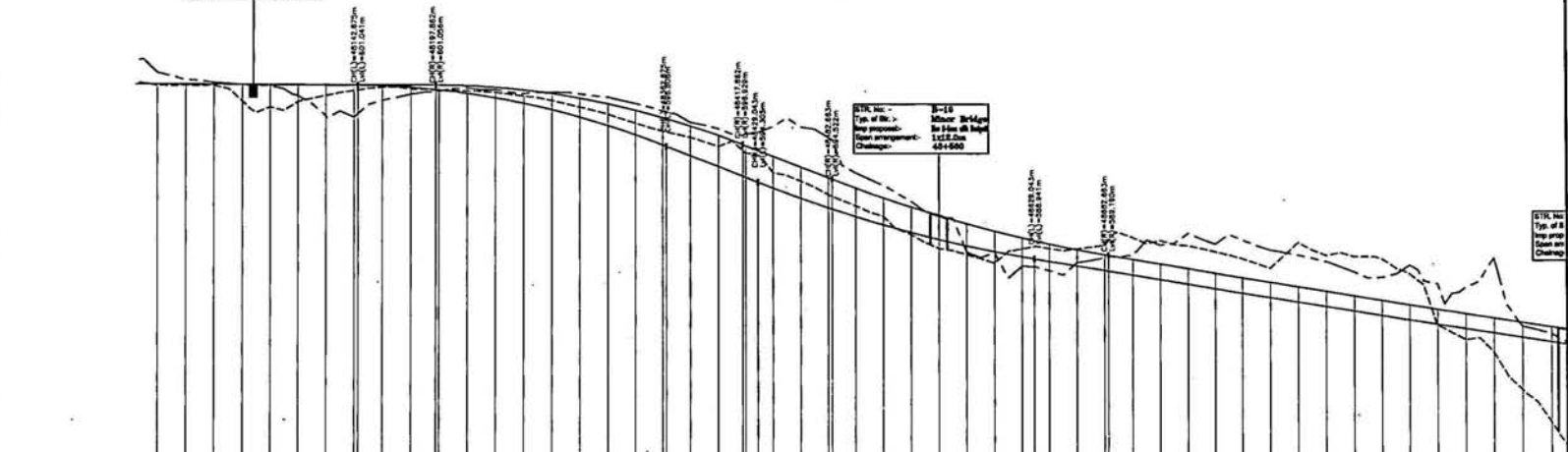
Scot Wilson India Private Limited
 C-26/A, MOHAN CO-OPERATIVE IND. ESTATE
 NEHRU ROAD
 NEW DELHI - 110094
 Tel: (011) 41679340 - 49
 Fax: (011) 41679350
 E-mail: scotwilson.com



STR. No. - 49/1
Type of Br. > R.C.C. Slab
Slope proposed - 1:15
Slope arrangement - 45+550
Challenges -

STR. No. - B-14
Type of Br. > Stone
Slope proposed - 1:15
Slope arrangement - 45+550
Challenges -

STR. No. >
Type of Br. >
Slope proposed >
Slope arrangement >
Challenges >

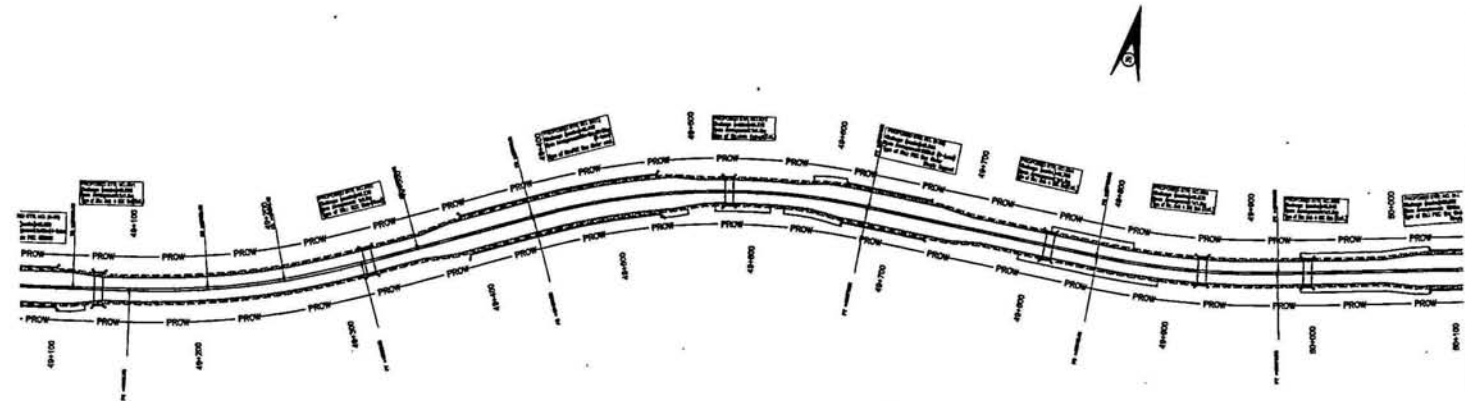


Level Datum = 574.000

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| Proposed Level along Left Median Edge (m.) | 481.050 | 481.120 | 481.200 | 481.280 | 481.360 | 481.440 | 481.520 | 481.600 | 481.680 | 481.760 | 481.840 | 481.920 | 482.000 | 482.080 | 482.160 | 482.240 | 482.320 | 482.400 | 482.480 | 482.560 | 482.640 | 482.720 | 482.800 | 482.880 | 482.960 | 483.040 | 483.120 | 483.200 | 483.280 | 483.360 | 483.440 | 483.520 | 483.600 | 483.680 | 483.760 | 483.840 | 483.920 | 484.000 | 484.080 | 484.160 | 484.240 | 484.320 | 484.400 | 484.480 | 484.560 | 484.640 | 484.720 | 484.800 | 484.880 | 484.960 | 485.040 | 485.120 | 485.200 | 485.280 | 485.360 | 485.440 | 485.520 | 485.600 | 485.680 | 485.760 | 485.840 | 485.920 | 486.000 | 486.080 | 486.160 | 486.240 | 486.320 | 486.400 | 486.480 | 486.560 | 486.640 | 486.720 | 486.800 | 486.880 | 486.960 | 487.040 | 487.120 | 487.200 | 487.280 | 487.360 | 487.440 | 487.520 | 487.600 | 487.680 | 487.760 | 487.840 | 487.920 | 488.000 | 488.080 | 488.160 | 488.240 | 488.320 | 488.400 | 488.480 | 488.560 | 488.640 | 488.720 | 488.800 | 488.880 | 488.960 | 489.040 | 489.120 | 489.200 | 489.280 | 489.360 | 489.440 | 489.520 | 489.600 | 489.680 | 489.760 | 489.840 | 489.920 | 490.000 | 490.080 | 490.160 | 490.240 | 490.320 | 490.400 | 490.480 | 490.560 | 490.640 | 490.720 | 490.800 | 490.880 | 490.960 | 491.040 | 491.120 | 491.200 | 491.280 | 491.360 | 491.440 | 491.520 | 491.600 | 491.680 | 491.760 | 491.840 | 491.920 | 492.000 | 492.080 | 492.160 | 492.240 | 492.320 | 492.400 | 492.480 | 492.560 | 492.640 | 492.720 | 492.800 | 492.880 | 492.960 | 493.040 | 493.120 | 493.200 | 493.280 | 493.360 | 493.440 | 493.520 | 493.600 | 493.680 | 493.760 | 493.840 | 493.920 | 494.000 | 494.080 | 494.160 | 494.240 | 494.320 | 494.400 | 494.480 | 494.560 | 494.640 | 494.720 | 494.800 | 494.880 | 494.960 | 495.040 | 495.120 | 495.200 | 495.280 | 495.360 | 495.440 | 495.520 | 495.600 | 495.680 | 495.760 | 495.840 | 495.920 | 496.000 | 496.080 | 496.160 | 496.240 | 496.320 | 496.400 | 496.480 | 496.560 | 496.640 | 496.720 | 496.800 | 496.880 | 496.960 | 497.040 | 497.120 | 497.200 | 497.280 | 497.360 | 497.440 | 497.520 | 497.600 | 497.680 | 497.760 | 497.840 | 497.920 | 498.000 | 498.080 | 498.160 | 498.240 | 498.320 | 498.400 | 498.480 | 498.560 | 498.640 | 498.720 | 498.800 | 498.880 | 498.960 | 499.040 | 499.120 | 499.200 | 499.280 | 499.360 | 499.440 | 499.520 | 499.600 | 499.680 | 499.760 | 499.840 | 499.920 | 500.000 | 500.080 | 500.160 | 500.240 | 500.320 | 500.400 | 500.480 | 500.560 | 500.640 | 500.720 | 500.800 | 500.880 | 500.960 | 501.040 | 501.120 | 501.200 | 501.280 | 501.360 | 501.440 | 501.520 | 501.600 | 501.680 | 501.760 | 501.840 | 501.920 | 502.000 | 502.080 | 502.160 | 502.240 | 502.320 | 502.400 | 502.480 | 502.560 | 502.640 | 502.720 | 502.800 | 502.880 | 502.960 | 503.040 | 503.120 | 503.200 | 503.280 | 503.360 | 503.440 | 503.520 | 503.600 | 503.680 | 503.760 | 503.840 | 503.920 | 504.000 | 504.080 | 504.160 | 504.240 | 504.320 | 504.400 | 504.480 | 504.560 | 504.640 | 504.720 | 504.800 | 504.880 | 504.960 | 505.040 | 505.120 | 505.200 | 505.280 | 505.360 | 505.440 | 505.520 | 505.600 | 505.680 | 505.760 | 505.840 | 505.920 | 506.000 | 506.080 | 506.160 | 506.240 | 506.320 | 506.400 | 506.480 | 506.560 | 506.640 | 506.720 | 506.800 | 506.880 | 506.960 | 507.040 | 507.120 | 507.200 | 507.280 | 507.360 | 507.440 | 507.520 | 507.600 | 507.680 | 507.760 | 507.840 | 507.920 | 508.000 | 508.080 | 508.160 | 508.240 | 508.320 | 508.400 | 508.480 | 508.560 | 508.640 | 508.720 | 508.800 | 508.880 | 508.960 | 509.040 | 509.120 | 509.200 | 509.280 | 509.360 | 509.440 | 509.520 | 509.600 | 509.680 | 509.760 | 509.840 | 509.920 | 510.000 | 510.080 | 510.160 | 510.240 | 510.320 | 510.400 | 510.480 | 510.560 | 510.640 | 510.720 | 510.800 | 510.880 | 510.960 | 511.040 | 511.120 | 511.200 | 511.280 | 511.360 | 511.440 | 511.520 | 511.600 | 511.680 | 511.760 | 511.840 | 511.920 | 512.000 | 512.080 | 512.160 | 512.240 | 512.320 | 512.400 | 512.480 | 512.560 | 512.640 | 512.720 | 512.800 | 512.880 | 512.960 | 513.040 | 513.120 | 513.200 | 513.280 | 513.360 | 513.440 | 513.520 | 513.600 | 513.680 | 513.760 | 513.840 | 513.920 | 514.000 | 514.080 | 514.160 | 514.240 | 514.320 | 514.400 | 514.480 | 514.560 | 514.640 | 514.720 | 514.800 | 514.880 | 514.960 | 515.040 | 515.120 | 515.200 | 515.280 | 515.360 | 515.440 | 515.520 | 515.600 | 515.680 | 515.760 | 515.840 | 515.920 | 516.000 | 516.080 | 516.160 | 516.240 | 516.320 | 516.400 | 516.480 | 516.560 | 516.640 | 516.720 | 516.800 | 516.880 | 516.960 | 517.040 | 517.120 | 517.200 | 517.280 | 517.360 | 517.440 | 517.520 | 517.600 | 517.680 | 517.760 | 517.840 | 517.920 | 518.000 | 518.080 | 518.160 | 518.240 | 518.320 | 518.400 | 518.480 | 518.560 | 518.640 | 518.720 | 518.800 | 518.880 | 518.960 | 519.040 | 519.120 | 519.200 | 519.280 | 519.360 | 519.440 | 519.520 | 519.600 | 519.680 | 519.760 | 519.840 | 519.920 | 520.000 | 520.080 | 520.160 | 520.240 | 520.320 | 520.400 | 520.480 | 520.560 | 520.640 | 520.720 | 520.800 | 520.880 | 520.960 | 521.040 | 521.120 | 521.200 | 521.280 | 521.360 | 521.440 | 521.520 | 521.600 | 521.680 | 521.760 | 521.840 | 521.920 | 522.000 | 522.080 | 522.160 | 522.240 | 522.320 | 522.400 | 522.480 | 522.560 | 522.640 | 522.720 | 522.800 | 522.880 | 522.960 | 523.040 | 523.120 | 523.200 | 523.280 | 523.360 | 523.440 | 523.520 | 523.600 | 523.680 | 523.760 | 523.840 | 523.920 | 524.000 | 524.080 | 524.160 | 524.240 | 524.320 | 524.400 | 524.480 | 524.560 | 524.640 | 524.720 | 524.800 | 524.880 | 524.960 | 525.040 | 525.120 | 525.200 | 525.280 | 525.360 | 525.440 | 525.520 | 525.600 | 525.680 | 525.760 | 525.840 | 525.920 | 526.000 | 526.080 | 526.160 | 526.240 | 526.320 | 526.400 | 526.480 | 526.560 | 526.640 | 526.720 | 526.800 | 526.880 | 526.960 | 527.040 | 527.120 | 527.200 | 527.280 | 527.360 | 527.440 | 527.520 | 527.600 | 527.680 | 527.760 | 527.840 | 527.920 | 528.000 | 528.080 | 528.160 | 528.240 | 528.320 | 528.400 | 528.480 | 528.560 | 528.640 | 528.720 | 528.800 | 528.880 | 528.960 | 529.040 | 529.120 | 529.200 | 529.280 | 529.360 | 529.440 | 529.520 | 529.600 | 529.680 | 529.760 | 529.840 | 529.920 | 530.000 | 530.080 | 530.160 | 530.240 | 530.320 | 530.400 | 530.480 | 530.560 | 530.640 | 530.720 | 530.800 | 530.880 | 530.960 | 531.040 | 531.120 | 531.200 | 531.280 | 531.360 | 531.440 | 531.520 | 531.600 | 531.680 | 531.760 | 531.840 | 531.920 | 532.000 | 532.080 | 532.160 | 532.240 | 532.320 | 532.400 | 532.480 | 532.560 | 532.640 | 532.720 | 532.800 | 532.880 | 532.960 | 533.040 | 533.120 | 533.200 | 533.280 | 533.360 | 533.440 | 533.520 | 533.600 | 533.680 | 533.760 | 533.840 | 533.920 | 534.000 | 534.080 | 534.160 | 534.240 | 534.320 | 534.400 | 534.480 | 534.560 | 534.640 | 534.720 | 534.800 | 534.880 | 534.960 | 535.040 | 535.120 | 535.200 | 535.280 | 535.360 | 535.440 | 535.520 | 535.600 | 535.680 | 535.760 | 535.840 | 535.920 | 536.000 | 536.080 | 536.160 | 536.240 | 536.320 | 536.400 | 536.480 | 536.560 | 536.640 | 536.720 | 536.800 | 536.880 | 536.960 | 537.040 | 537.120 | 537.200 | 537.280 | 537.360 | 537.440 | 537.520 | 537.600 | 537.680 | 537.760 | 537.840 | 537.920 | 538.000 | 538.080 | 538.160 | 538.240 | 538.320 | 538.400 | 538.480 | 538.560 | 538.640 | 538.720 | 538.800 | 538.880 | 538.960 | 539.040 | 539.120 | 539.200 | 539.280 | 539.360 | 539.440 | 539.520 | 539.600 | 539.680 | 539.760 | 539.840 | 539.920 | 540.000 | 540.080 | 540.160 | 540.240 | 540.320 | 540.400 | 540.480 | 540.560 | 540.640 | 540.720 | 540.800 | 540.880 | 540.960 | 541.040 | 541.120 | 541.200 | 541.280 | 541.360 | 541.440 | 541.520 | 541.600 | 541.680 | 541.760 | 541.840 | 541.920 | 542.000 | 542.080 | 542.160 | 542.240 | 542.320 | 542.400 | 542.480 | 542.560 | 542.640 | 542.720 | 542.800 | 542.880 | 542.960 | 543.040 | 543.120 | 543.200 | 543.280 | 543.360 | 543.440 | 543.520 | 543.600 | 543.680 | 543.760 | 543.840 | 543.920 | 544.000 | 544.080 | 544.160 | 544.240 | 544.320 | 544.400 | 544.480 | 544.560 | 544.640 | 544.720 | 544.800 | 544.880 | 544.960 | 545.040 | 545.120 | 545.200 | 545.280 | 545.360 | 545.440 | 545.520 | 545.600 | 545.680 | 545.760 | 545.840 | 545.920 | 546.000 | 546.080 | 546.160 | 546.240 | 546.320 | 546.400 | 546.480 | 546.560 | 546.640 | 546.720 | 546.800 | 546.880 | 546.960 | 547.040 | 547.120 | 547.200 | 547.280 | 547.360 | 547.440 | 547.520 | 547.600 | 547.680 | 547.760 | 547.840 | 547.920 | 548.000 | 548.080 | 548.160 | 548.240 | 548.320 | 548.400 | 548.480 | 548.560 | 548.640 | 548.720 | 548.800 | 548.880 | 548.960 | 549.040 | 549.120 | 549.200 | 549.280 | 549.360 | 549.440 | 549.520 | 549.600 | 549.680 | 549.760 | 549.840 | 549.920 | 550.000 | 550.080 | 550.160 | 550.240 | 550.320 | 550.400 | 550.480 | 550.560 | 550.640 | 550.720 | 550.800 | 550.880 | 550.960 | 551.040 | 551.120 | 551.200 | 551.280 | 551.360 | 551.440 | 551.520 | 551.600 | 551.680 | 551.760 | 551.840 | 551.920 | 552.000 | 552.080 | 552.160 | 552.240 | 552.320 | 552.400 | 552.480 | 552.560 | 552.640 | 552.720 | 552.800 | 552.880 | 552.960 | 553.040 | 553.120 | 553.200 | 553.280 | 553.360 | 553.440 | 553.520 | 553.600 | 553.680 | 553.760 | 553.840 | 553.920 | 554.000 | 554.080 | 554.160 | 554.240 | 554.320 | 554.400 | 554.480 | 554.560 | 554.640 | 554.720 | 554.800 | 554.880 | 554.960 | 555.040 | 555.120 | 555.200 | 555.280 | 555.360 | 555.440 | 555.520 | 555.600 | 555.680 | 555.760 | 555.840 | 555.920 | 556.000 | 556.080 | 556.160 | 556.240 | 556.320 | 556.400 | 556.480 | 556.560 | 556.640 | 556.720 | 556.800 | 556.880 | 556.960 | 557.040 | 557.120 | 557.200 | 557.280 | 557.360 | 557.440 | 557.520 | 557.600 | 557.680 | 557.760 | 557.840 | 557.920 | 558.000 | 558.080 | 558.160 | 558.240 | 558.320 | 558.400 | 558.480 | 558.560 | 558.640 | 558.720 | 558.800 | 558.880 | 558.960 | 559.040 | 559.120 | 559.200 | 559.280 | 559.360 | 559.440 | 559.520 | 559.600 | 559.680 | 559.760 | 559.840 | 559.920 | 560.000 | 560.080 | 560.160 | |
|--|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|------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← Jammu
133

Udhampur →



Level Datum = 562.000

| |
|---|
| Proposed Level along Left Median Edge (m.) |
| Existing Level Below Left Median Edge (m.) |
| Proposed Level along Right Median Edge (m.) |
| Existing Level Below Right Median Edge (m.) |
| Horizontal Alignment Left |
| Horizontal Alignment Right |
| Vertical Alignment Left |
| Vertical Alignment Right |
| LHS Superelevation |
| RHS Superelevation |
| Left Profile Chainage |
| Right Profile Chainage |

| REV | DESCRIPTION |
|-----|----------------|
| 1 | As per drawing |
| 2 | As per drawing |
| 3 | As per drawing |
| 4 | As per drawing |
| 5 | As per drawing |
| 6 | As per drawing |
| 7 | As per drawing |
| 8 | As per drawing |
| 9 | As per drawing |
| 10 | As per drawing |
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| 96 | As per drawing |
| 97 | As per drawing |
| 98 | As per drawing |
| 99 | As per drawing |
| 100 | As per drawing |

Project Title
 Consultancy services for preparation of detailed project report for Rehabilitation and upgrading of existing 3-lane road to 4-lane divided carriageway with provision of Jammu - Udhampur section of NH 1A (Km 0.00 to Km 64.00) including Jammu/Parit/Nigra and Udhampur Bypass Phase - III Programme of North - South corridor Project. Contract Package-C-02/18

Client
NATIONAL HIGHWAYS AUTHORITY OF INDIA
 (Ministry of Shipping, Road Transport & Highways)

Drawing Title: PLAN & PROFILE
 Km. 49+000 to 50+000

Drawing No.: SW/1019/ROADS/PP-47

Scale: HOR:-1:2000 VER:-1:200

Drawn by: K.L.M

Checked by: K.PRASAD

Appd.: BRG.C.D.PURI

Date: JUNE-2009

Sheet No.: 1 of 2

Scale: HOR:-1:2000 VER:-1:200

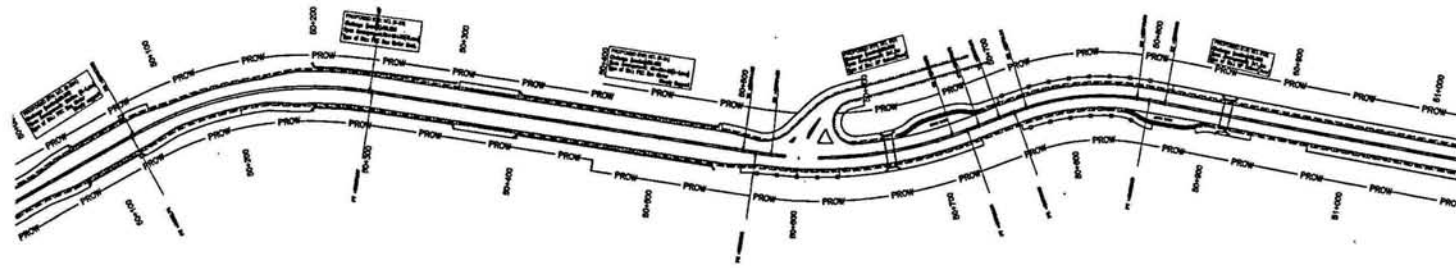
Date: JUNE-2009

Scott Wilson India Private Limited
 4, MOHAN CO-OPERATIVE IND. ESTATE
 WATHERA ROAD
 NEW DELHI - 110044
 Tele: (011) 41679340 - 49
 Fax: (011) 41679350
 E-mail: rai@swi.com



← Jammu
125

Udhampur →



| |
|---|
| Level Datum =620.000 |
| Proposed Level along Left Median Edge (m.) |
| Existing Level Below Left Median Edge (m.) |
| Proposed Level along Right Median Edge (m.) |
| Existing Level Below Right Median Edge (m.) |
| Horizontal Alignment Left |
| Horizontal Alignment Right |
| Vertical Alignment Left |
| Vertical Alignment Right |
| LHS Superelevation |
| RHS Superelevation |
| Left Profile Chainage |
| Right Profile Chainage |

| LEGEND | |
|------------------------|--------|
| For | Symbol |
| Proposed Level | — |
| Existing Level | --- |
| Horizontal Alignment | — |
| Vertical Alignment | — |
| Superelevation | — |
| Profile Chainage | — |
| Right Profile Chainage | — |

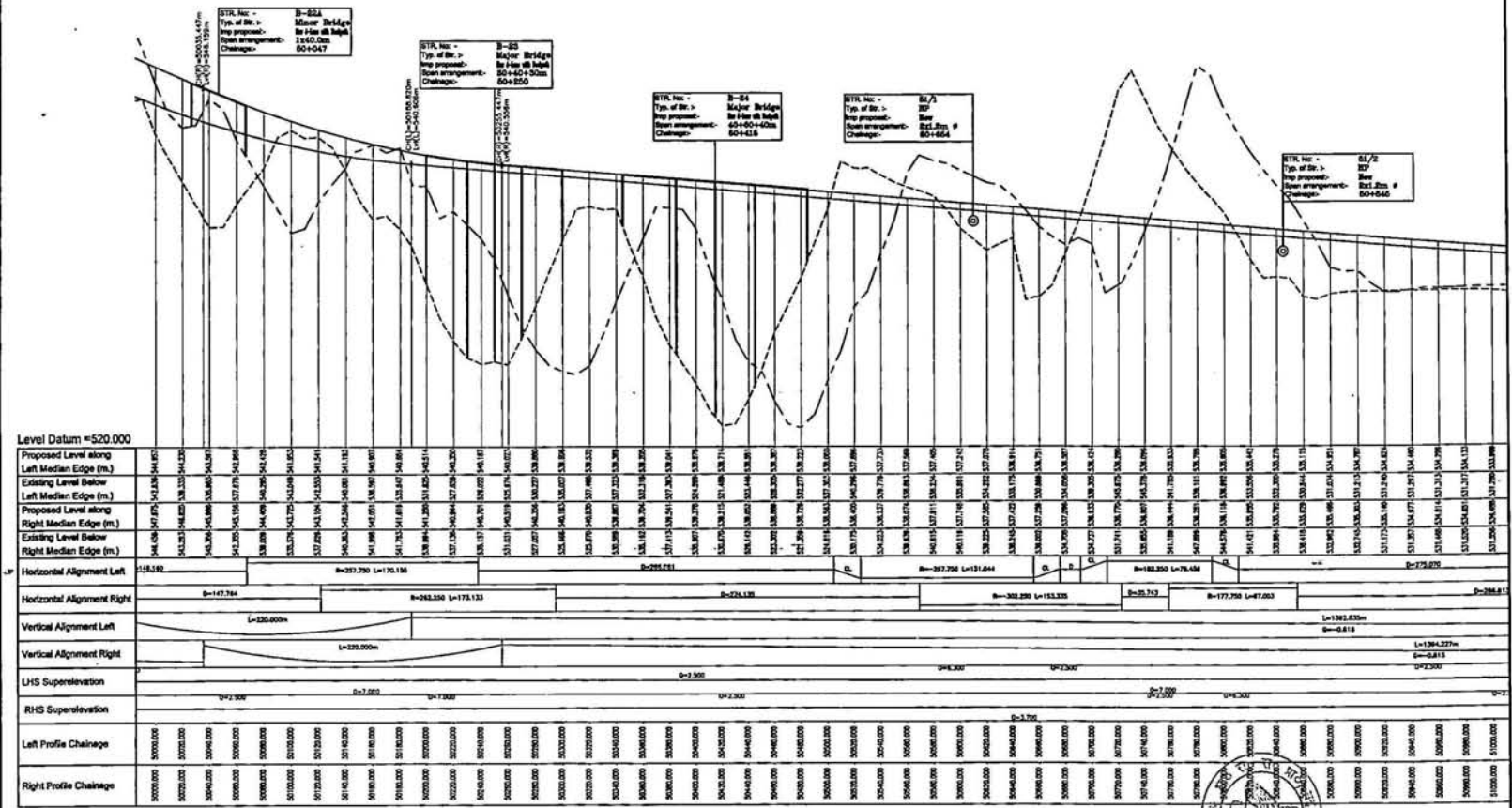
Project Title
 Consultancy services for preparation of detailed project report for Rehabilitation and upgrading of existing 3-lane road to 4-lane divided carriageway configuration of Jammu - Udhampur section of NH-1 A
 (on 0.00 to Km 66.00) including Jammu (Part) Nagrota and Udhampur Bypass Phase - III Programme of North - South - corridor Project, Contract Package C-02/18

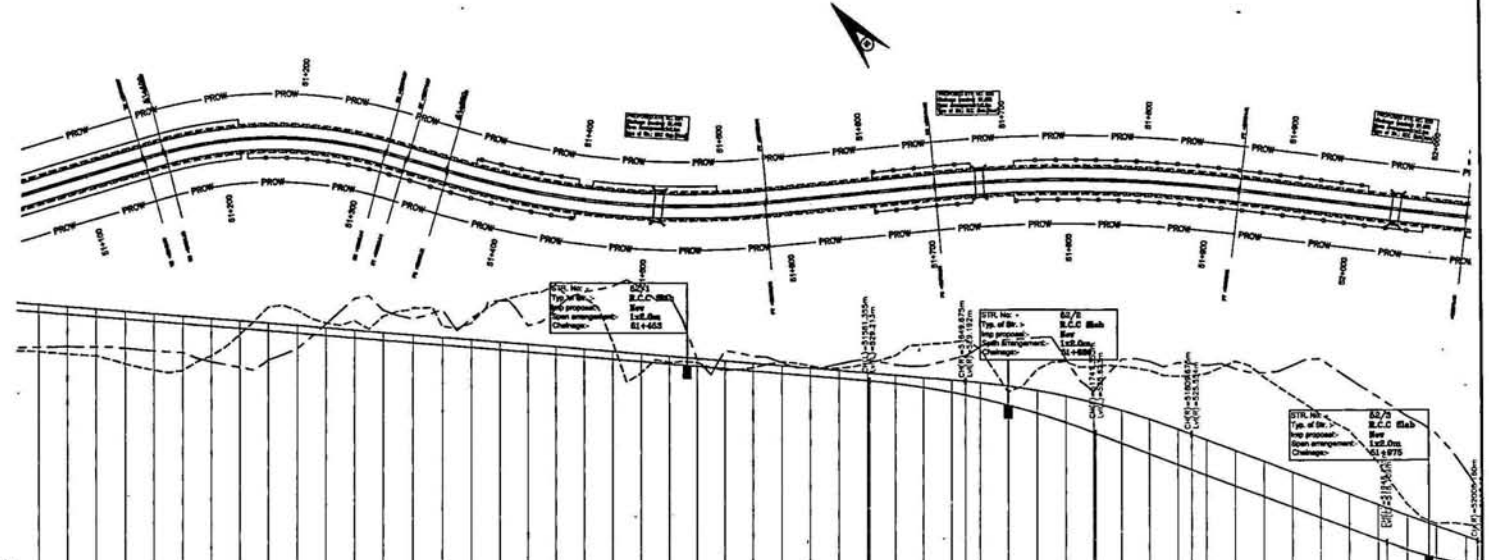
Client
NATIONAL HIGHWAYS AUTHORITY OF INDIA
 (Ministry of Shipping, Road Transport & Highways)

Drawing Title: PLAN & PROFILE
 Km. 50+000 to 51+000
Drawing No.: SW/1018/ROADS/PP-48
Scale: HOR:-1:2000 VER:-1:200
Date: JUNE-2009
Sheet: 1 of 2

Client: Scott Wilson India Private Limited
 A-26/4, MOHAN CO-OPERATIVE IND. ESTATE
 MATRUJARA ROAD
 NEW DELHI - 110044
 Tel: (011) 41679340 - 49
 Fax: (011) 41679350
 E-mail: scw@scw.com







Level Datum =516.000

| | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
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| Proposed Level along Left Median Edge (m.) | 517.550 | 517.630 | 517.700 | 517.750 | 517.800 | 517.850 | 517.900 | 517.950 | 518.000 | 518.050 | 518.100 | 518.150 | 518.200 | 518.250 | 518.300 | 518.350 | 518.400 | 518.450 | 518.500 | 518.550 | 518.600 | 518.650 | 518.700 | 518.750 | 518.800 | 518.850 | 518.900 | 518.950 | 519.000 | 519.050 | 519.100 | 519.150 | 519.200 | 519.250 | 519.300 | 519.350 | 519.400 | 519.450 | 519.500 | 519.550 | 519.600 | 519.650 | 519.700 | 519.750 | 519.800 | 519.850 | 519.900 | 519.950 | 520.000 | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| Existing Level Below Left Median Edge (m.) | 517.450 | 517.530 | 517.600 | 517.650 | 517.700 | 517.750 | 517.800 | 517.850 | 517.900 | 517.950 | 518.000 | 518.050 | 518.100 | 518.150 | 518.200 | 518.250 | 518.300 | 518.350 | 518.400 | 518.450 | 518.500 | 518.550 | 518.600 | 518.650 | 518.700 | 518.750 | 518.800 | 518.850 | 518.900 | 518.950 | 519.000 | 519.050 | 519.100 | 519.150 | 519.200 | 519.250 | 519.300 | 519.350 | 519.400 | 519.450 | 519.500 | 519.550 | 519.600 | 519.650 | 519.700 | 519.750 | 519.800 | 519.850 | 519.900 | 519.950 | 520.000 | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| Proposed Level along Right Median Edge (m.) | 517.550 | 517.630 | 517.700 | 517.750 | 517.800 | 517.850 | 517.900 | 517.950 | 518.000 | 518.050 | 518.100 | 518.150 | 518.200 | 518.250 | 518.300 | 518.350 | 518.400 | 518.450 | 518.500 | 518.550 | 518.600 | 518.650 | 518.700 | 518.750 | 518.800 | 518.850 | 518.900 | 518.950 | 519.000 | 519.050 | 519.100 | 519.150 | 519.200 | 519.250 | 519.300 | 519.350 | 519.400 | 519.450 | 519.500 | 519.550 | 519.600 | 519.650 | 519.700 | 519.750 | 519.800 | 519.850 | 519.900 | 519.950 | 520.000 | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| Existing Level Below Right Median Edge (m.) | 517.450 | 517.530 | 517.600 | 517.650 | 517.700 | 517.750 | 517.800 | 517.850 | 517.900 | 517.950 | 518.000 | 518.050 | 518.100 | 518.150 | 518.200 | 518.250 | 518.300 | 518.350 | 518.400 | 518.450 | 518.500 | 518.550 | 518.600 | 518.650 | 518.700 | 518.750 | 518.800 | 518.850 | 518.900 | 518.950 | 519.000 | 519.050 | 519.100 | 519.150 | 519.200 | 519.250 | 519.300 | 519.350 | 519.400 | 519.450 | 519.500 | 519.550 | 519.600 | 519.650 | 519.700 | 519.750 | 519.800 | 519.850 | 519.900 | 519.950 | 520.000 | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| Horizontal Alignment Left | B=302.250 L=157.296 | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | B=217.750 L=114.723 | | | B=209.470 | | | B=407.750 L=227.421 | | | D=118.827 | | | B=1000.000 L=418.758 | | | B=497.750 L=318.373 | | | L=354.780m C=3.248 | | | L=185.485m C=3.723 | | | L=130 | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| Horizontal Alignment Right | D=281.612 | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | B=217.750 L=114.723 | | | B=209.470 | | | B=407.750 L=227.421 | | | D=118.827 | | | B=1000.000 L=418.758 | | | B=497.750 L=318.373 | | | L=354.780m C=3.248 | | | L=185.485m C=3.723 | | | L=130 | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| Vertical Alignment Left | D=281.612 | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | B=217.750 L=114.723 | | | B=209.470 | | | B=407.750 L=227.421 | | | D=118.827 | | | B=1000.000 L=418.758 | | | B=497.750 L=318.373 | | | L=354.780m C=3.248 | | | L=185.485m C=3.723 | | | L=130 | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| Vertical Alignment Right | D=281.612 | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | B=217.750 L=114.723 | | | B=209.470 | | | B=407.750 L=227.421 | | | D=118.827 | | | B=1000.000 L=418.758 | | | B=497.750 L=318.373 | | | L=354.780m C=3.248 | | | L=185.485m C=3.723 | | | L=130 | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| LHS Superelevation | D=281.612 | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | B=217.750 L=114.723 | | | B=209.470 | | | B=407.750 L=227.421 | | | D=118.827 | | | B=1000.000 L=418.758 | | | B=497.750 L=318.373 | | | L=354.780m C=3.248 | | | L=185.485m C=3.723 | | | L=130 | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| RHS Superelevation | D=281.612 | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | B=217.750 L=114.723 | | | B=209.470 | | | B=407.750 L=227.421 | | | D=118.827 | | | B=1000.000 L=418.758 | | | B=497.750 L=318.373 | | | L=354.780m C=3.248 | | | L=185.485m C=3.723 | | | L=130 | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| Left Profile Chainage | 51000.000 | 51050.000 | 51100.000 | 51150.000 | 51200.000 | 51250.000 | 51300.000 | 51350.000 | 51400.000 | 51450.000 | 51500.000 | 51550.000 | 51600.000 | 51650.000 | 51700.000 | 51750.000 | 51800.000 | 51850.000 | 51900.000 | 51950.000 | 52000.000 | 52050.000 | 52100.000 | 52150.000 | 52200.000 | 52250.000 | 52300.000 | 52350.000 | 52400.000 | 52450.000 | 52500.000 | 52550.000 | 52600.000 | 52650.000 | 52700.000 | 52750.000 | 52800.000 | 52850.000 | 52900.000 | 52950.000 | 53000.000 | 53050.000 | 53100.000 | 53150.000 | 53200.000 | 53250.000 | 53300.000 | 53350.000 | 53400.000 | 53450.000 | 53500.000 | 53550.000 | 53600.000 | 53650.000 | 53700.000 | 53750.000 | 53800.000 | 53850.000 | 53900.000 | 53950.000 | 54000.000 | 54050.000 | 54100.000 | 54150.000 | 54200.000 | 54250.000 | 54300.000 | 54350.000 | 54400.000 | 54450.000 | 54500.000 | 54550.000 | 54600.000 | 54650.000 | 54700.000 | 54750.000 | 54800.000 | 54850.000 | 54900.000 | 54950.000 | 55000.000 | 55050.000 | 55100.000 | 55150.000 | 55200.000 | 55250.000 | 55300.000 | 55350.000 | 55400.000 | 55450.000 | 55500.000 | 55550.000 | 55600.000 | 55650.000 | 55700.000 | 55750.000 | 55800.000 | 55850.000 | 55900.000 | 55950.000 | 56000.000 | 56050.000 | 56100.000 | 56150.000 | 56200.000 | 56250.000 | 56300.000 | 56350.000 | 56400.000 | 56450.000 | 56500.000 | 56550.000 | 56600.000 | 56650.000 | 56700.000 | 56750.000 | 56800.000 | 56850.000 | 56900.000 | 56950.000 | 57000.000 | 57050.000 | 57100.000 | 57150.000 | 57200.000 | 57250.000 | 57300.000 | 57350.000 | 57400.000 | 57450.000 | 57500.000 | 57550.000 | 57600.000 | 57650.000 | 57700.000 | 57750.000 | 57800.000 | 57850.000 | 57900.000 | 57950.000 | 58000.000 | 58050.000 | 58100.000 | 58150.000 | 58200.000 | 58250.000 | 58300.000 | 58350.000 | 58400.000 | 58450.000 | 58500.000 | 58550.000 | 58600.000 | 58650.000 | 58700.000 | 58750.000 | 58800.000 | 58850.000 | 58900.000 | 58950.000 | 59000.000 | 59050.000 | 59100.000 | 59150.000 | 59200.000 | 59250.000 | 59300.000 | 59350.000 | 59400.000 | 59450.000 | 59500.000 | 59550.000 | 59600.000 | 59650.000 | 59700.000 | 59750.000 | 59800.000 | 59850.000 | 59900.000 | 59950.000 | 60000.000 |
| Right Profile Chainage | 51000.000 | 51050.000 | 51100.000 | 51150.000 | 51200.000 | 51250.000 | 51300.000 | 51350.000 | 51400.000 | 51450.000 | 51500.000 | 51550.000 | 51600.000 | 51650.000 | 51700.000 | 51750.000 | 51800.000 | 51850.000 | 51900.000 | 51950.000 | 52000.000 | 52050.000 | 52100.000 | 52150.000 | 52200.000 | 52250.000 | 52300.000 | 52350.000 | 52400.000 | 52450.000 | 52500.000 | 52550.000 | 52600.000 | 52650.000 | 52700.000 | 52750.000 | 52800.000 | 52850.000 | 52900.000 | 52950.000 | 53000.000 | 53050.000 | 53100.000 | 53150.000 | 53200.000 | 53250.000 | 53300.000 | 53350.000 | 53400.000 | 53450.000 | 53500.000 | 53550.000 | 53600.000 | 53650.000 | 53700.000 | 53750.000 | 53800.000 | 53850.000 | 53900.000 | 53950.000 | 54000.000 | 54050.000 | 54100.000 | 54150.000 | 54200.000 | 54250.000 | 54300.000 | 54350.000 | 54400.000 | 54450.000 | 54500.000 | 54550.000 | 54600.000 | 54650.000 | 54700.000 | 54750.000 | 54800.000 | 54850.000 | 54900.000 | 54950.000 | 55000.000 | 55050.000 | 55100.000 | 55150.000 | 55200.000 | 55250.000 | 55300.000 | 55350.000 | 55400.000 | 55450.000 | 55500.000 | 55550.000 | 55600.000 | 55650.000 | 55700.000 | 55750.000 | 55800.000 | 55850.000 | 55900.000 | 55950.000 | 56000.000 | 56050.000 | 56100.000 | 56150.000 | 56200.000 | 56250.000 | 56300.000 | 56350.000 | 56400.000 | 56450.000 | 56500.000 | 56550.000 | 56600.000 | 56650.000 | 56700.000 | 56750.000 | 56800.000 | 56850.000 | 56900.000 | 56950.000 | 57000.000 | 57050.000 | 57100.000 | 57150.000 | 57200.000 | 57250.000 | 57300.000 | 57350.000 | 57400.000 | 57450.000 | 57500.000 | 57550.000 | 57600.000 | 57650.000 | 57700.000 | 57750.000 | 57800.000 | 57850.000 | 57900.000 | 57950.000 | 58000.000 | 58050.000 | 58100.000 | 58150.000 | 58200.000 | 58250.000 | 58300.000 | 58350.000 | 58400.000 | 58450.000 | 58500.000 | 58550.000 | 58600.000 | 58650.000 | 58700.000 | 58750.000 | 58800.000 | 58850.000 | 58900.000 | 58950.000 | 59000.000 | 59050.000 | 59100.000 | 59150.000 | 59200.000 | 59250.000 | 59300.000 | 59350.000 | 59400.000 | 59450.000 | 59500.000 | 59550.000 | 59600.000 | 59650.000 | 59700.000 | 59750.000 | 59800.000 | 59850.000 | 59900.000 | 59950.000 | 60000.000 |

| | |
|----------------------------|----------------------------------|
| LEGEND | FOR PRELIMINARY WORK ONLY |
| 1. Existing Road | 2. Proposed Road |
| 3. Existing Median | 4. Proposed Median |
| 5. Existing Shoulder | 6. Proposed Shoulder |
| 7. Existing Drainage | 8. Proposed Drainage |
| 9. Existing Level | 10. Proposed Level |
| 11. Existing Chainage | 12. Proposed Chainage |
| 13. Existing Profile | 14. Proposed Profile |
| 15. Existing Cross-section | 16. Proposed Cross-section |

Project Title
 Consultancy services for preparation of detailed project report for Rehabilitation and upgrading of existing 3 lane road to 4 lane divided Carriageway configuration of Jammu - Udhampur section of NH-64 A (Div 0.00 to box 66.00) including Jammu(Ur) Highways and Udhampur Bypass Phase - II Programme of North-South corridor Project, Contract Package C-02/18

Client
 NATIONAL HIGHWAYS AUTHORITY OF INDIA
 (Ministry of Shipping, Road Transport & Highways)

Client
NATIONAL HIGHWAYS AUTHORITY OF INDIA
 (Ministry of Shipping, Road Transport & Highways)

Drawing Title: PLAN & PROFILE
 Km. 51+000 to 52+000

Drawing No.: SW/H/1019/ROADS/PP-49

Scale 1:- HOR:-1:2000 VDR:-1:200

Date: 06/06/2009

Drawn by: K.L.N

Checked by: K.PRASAD

Appd: BRIG.C.D.PURI

Date: JUNE-2009

Project: NEW DELHI

Scale: 1:10000

Sheet No.: 49/1

Scale: (1:11) 41679340 - 49

Scale: (1:11) 41679350

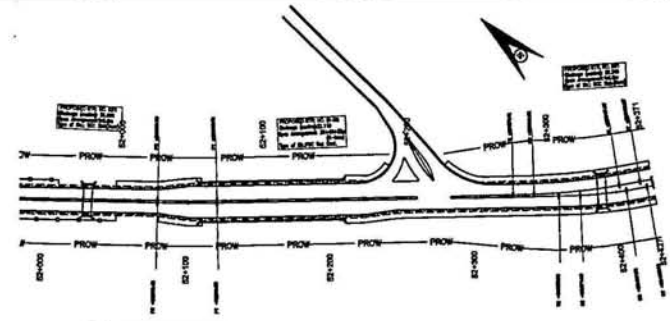
Scale: E-mail : enr@nhai.org

Scale: Scott Wilson India Private Limited
 MOHAN CO-OPERATIVE IND. ESTATE
 GATEWAY ROAD
 NEW DELHI - 110044
 Tel: (011) 41679340 - 49
 Fax: (011) 41679350



← Jammu
12B

Udhampur →



Level Datum = 506.000

| | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
|---|--------------------------|-----------|-----------|-----------|-----------|-----------|-----------|-----------|-----------|-----------|-----------|-----------|-----------|-----------|-----------|-----------|-----------|-----------|-----------|-----------|-----------|-----------|-----------|-----------|-----------|-----------|-----------|-----------|-----------|-----------|-----------|-----------|-----------|-----------|-----------|-----------|-----------|-----------|-----------|-----------|-----------|
| Proposed Level along Left Median Edge (m.) | 522.960 | 523.000 | 523.040 | 523.080 | 523.120 | 523.160 | 523.200 | 523.240 | 523.280 | 523.320 | 523.360 | 523.400 | 523.440 | 523.480 | 523.520 | 523.560 | 523.600 | 523.640 | 523.680 | 523.720 | 523.760 | 523.800 | 523.840 | 523.880 | 523.920 | 523.960 | 524.000 | | | | | | | | | | | | | | |
| Existing Level Below Left Median Edge (m.) | 522.960 | 523.000 | 523.040 | 523.080 | 523.120 | 523.160 | 523.200 | 523.240 | 523.280 | 523.320 | 523.360 | 523.400 | 523.440 | 523.480 | 523.520 | 523.560 | 523.600 | 523.640 | 523.680 | 523.720 | 523.760 | 523.800 | 523.840 | 523.880 | 523.920 | 523.960 | 524.000 | | | | | | | | | | | | | | |
| Proposed Level along Right Median Edge (m.) | 522.960 | 523.000 | 523.040 | 523.080 | 523.120 | 523.160 | 523.200 | 523.240 | 523.280 | 523.320 | 523.360 | 523.400 | 523.440 | 523.480 | 523.520 | 523.560 | 523.600 | 523.640 | 523.680 | 523.720 | 523.760 | 523.800 | 523.840 | 523.880 | 523.920 | 523.960 | 524.000 | | | | | | | | | | | | | | |
| Existing Level Below Right Median Edge (m.) | 522.960 | 523.000 | 523.040 | 523.080 | 523.120 | 523.160 | 523.200 | 523.240 | 523.280 | 523.320 | 523.360 | 523.400 | 523.440 | 523.480 | 523.520 | 523.560 | 523.600 | 523.640 | 523.680 | 523.720 | 523.760 | 523.800 | 523.840 | 523.880 | 523.920 | 523.960 | 524.000 | | | | | | | | | | | | | | |
| Horizontal Alignment Left | L=1000.000 L=1.247 | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| Horizontal Alignment Right | L=1000.000 L=1.247 | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| Vertical Alignment Left | L=120.000m L=304.825m | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| Vertical Alignment Right | L=120.000m L=288.625m | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| LHS Superelevation | 0+2.300 0+2.937 | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| RHS Superelevation | 0+2.300 0+2.937 | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| Left Profile Chainage | 52000.000 | 52050.000 | 52100.000 | 52150.000 | 52200.000 | 52250.000 | 52300.000 | 52350.000 | 52400.000 | 52450.000 | 52500.000 | 52550.000 | 52600.000 | 52650.000 | 52700.000 | 52750.000 | 52800.000 | 52850.000 | 52900.000 | 52950.000 | 53000.000 | 53050.000 | 53100.000 | 53150.000 | 53200.000 | 53250.000 | 53300.000 | 53350.000 | 53400.000 | 53450.000 | 53500.000 | 53550.000 | 53600.000 | 53650.000 | 53700.000 | 53750.000 | 53800.000 | 53850.000 | 53900.000 | 53950.000 | 54000.000 |
| Right Profile Chainage | 52000.000 | 52050.000 | 52100.000 | 52150.000 | 52200.000 | 52250.000 | 52300.000 | 52350.000 | 52400.000 | 52450.000 | 52500.000 | 52550.000 | 52600.000 | 52650.000 | 52700.000 | 52750.000 | 52800.000 | 52850.000 | 52900.000 | 52950.000 | 53000.000 | 53050.000 | 53100.000 | 53150.000 | 53200.000 | 53250.000 | 53300.000 | 53350.000 | 53400.000 | 53450.000 | 53500.000 | 53550.000 | 53600.000 | 53650.000 | 53700.000 | 53750.000 | 53800.000 | 53850.000 | 53900.000 | 53950.000 | 54000.000 |

LEGEND

| For Proposed Year | Existing |
|---|---|
| 1. Existing R.O. of carriageway | 1. Existing R.O. of carriageway |
| 2. Proposed level along Left Median Edge | 2. Proposed level along Left Median Edge |
| 3. Proposed level along Right Median Edge | 3. Proposed level along Right Median Edge |
| 4. Existing level below Left Median Edge | 4. Existing level below Left Median Edge |
| 5. Existing level below Right Median Edge | 5. Existing level below Right Median Edge |
| 6. Road Bed & Top | 6. Road Bed & Top |

Project Title
 Consultancy services for preparation of detailed project report for Rehabilitation and upgrading of existing 2-lane road to 4-lane divided Carriageway and upgrading of existing 2-lane road to 4-lane divided Carriageway and upgrading of existing 2-lane road to 4-lane divided Carriageway of Jammu - Udhampur section of NH-1A from Km 0+00 to Km 54+00 (including Jammu/Part) Jammu and Udhampur Region, Phase - IV, Programme of North-South-eastern Project, Contract Package C-0218

Scale: 1:1000
 Date: JUNE-2009
 Author: K.L.N.
 Checker: K.PRASAD
 Appr: BRG.C.P.PURI

NATIONAL HIGHWAYS AUTHORITY OF INDIA
 (Ministry of Shipping, Road Transport & Highways)

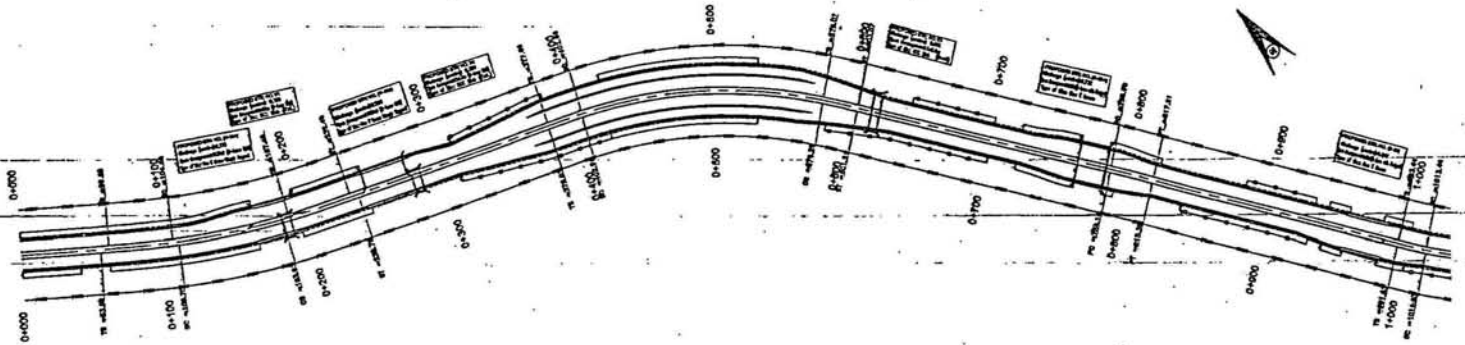
Drawing Title: **PLAN & PROFILE**
 Km. 52+000 to 52+409
 Drawing No.: **SW/H019/ROADS/PP-50**
 Scale: 1:1000
 Date: JUNE-2009

Scott Wilson India Private Limited
 A-26/A, MOHAN CO-OPERATIVE IND. ESTATE
 MATHERA ROAD
 NEW DELHI - 110044
 Tele: (011) 41679340 - 49
 Fax: (011) 41679350
 E-mail: rajiv@swi.com



← Jammu
129

Udhampur →



Level Datum =504.000

| |
|---|
| Proposed Level along Left Median Edge (m.) |
| Existing Level Below Left Median Edge (m.) |
| Proposed Level along Right Median Edge (m.) |
| Existing Level Below Right Median Edge (m.) |
| Horizontal Alignment Left |
| Horizontal Alignment Right |
| Vertical Alignment Left |
| Vertical Alignment Right |
| LHS Superelevation |
| RHS Superelevation |
| Left Profile Chainage |
| Right Profile Chainage |

| REV | DESCRIPTION |
|-----|-------------|
| 1 | As Per |
| 2 | As Per |
| 3 | As Per |
| 4 | As Per |
| 5 | As Per |
| 6 | As Per |
| 7 | As Per |
| 8 | As Per |
| 9 | As Per |
| 10 | As Per |
| 11 | As Per |
| 12 | As Per |
| 13 | As Per |
| 14 | As Per |
| 15 | As Per |
| 16 | As Per |
| 17 | As Per |
| 18 | As Per |
| 19 | As Per |
| 20 | As Per |

Project Title
 Consultancy services for preparation of detailed project report for
 2. Rehabilitation and upgrading of existing 2-lane road to 4-lane divided
 highway configuration of Jammu - Udhampur section at NH-1A
 Km 0+00 to Km 64+00 including Jammu (P) Bypass and Udhampur
 Bypass Phase - II. Programme of North - South corridor Project
 Contract Package C-021A

Client
 NATIONAL HIGHWAYS
 AUTHORITY OF INDIA
 (Ministry of Shipping, Road Transport & Highways)

Object
 NATIONAL HIGHWAYS
 AUTHORITY OF INDIA
 (Ministry of Shipping, Road Transport & Highways)

Drawing Title: PLAN & PROFILE
 Km. 0+000 to 1+000 (UDHAMPUR BYPASS)

Drawing No.: SW/1019/ROADS/PP-51

Scale: HOR:-1:2000 VER:-1:200

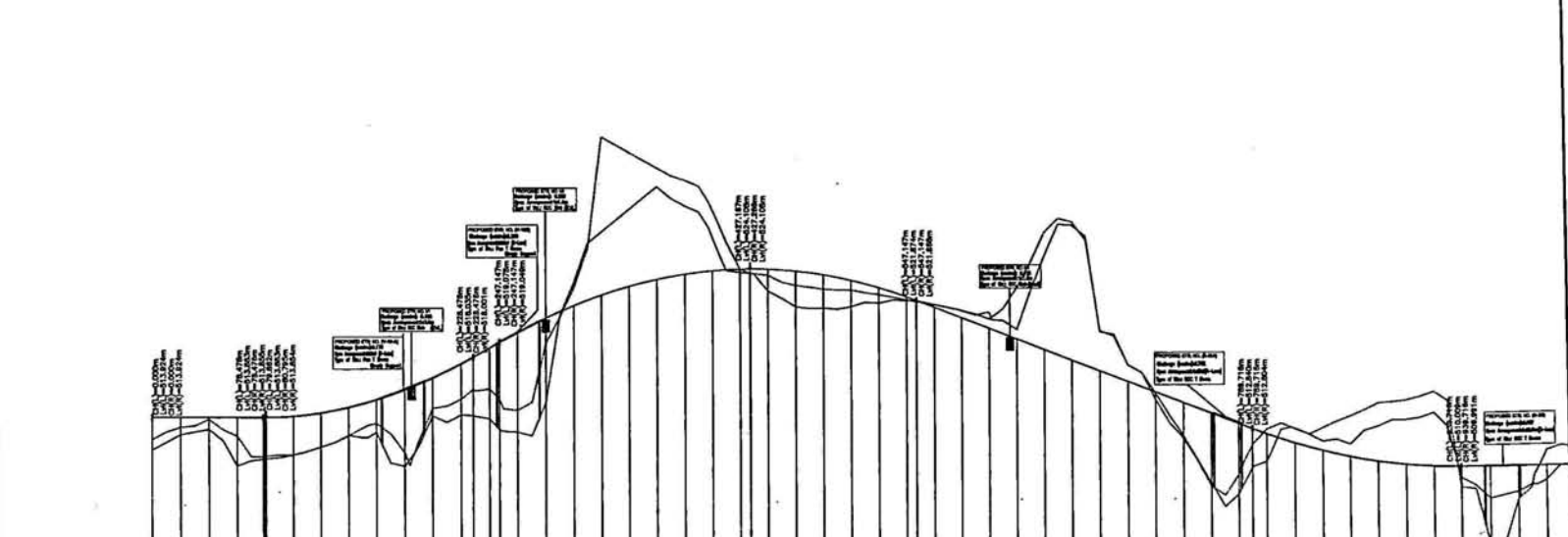
Sheet: 1 of 2

Date: JUNE-2005

Drawn by: KLN
Checked by: K.PRASAD
Approved by: BRIG.C.D.PURI
Date: JUNE-2005

Client Logo: Jammu Udhampur Highway Authority

Scott Wilson India Private Limited
 A-26/4, MGNM CO-OPERATIVE IND. ESTATE,
 MUKHERJI ROAD,
 NEW DELHI - 110044
 Tele: (011) 41679340 - 49
 Fax: (011) 41679350
 E-mail: swinfo@swi.com



| | |
|---|--|
| Level Datum = 504.000 | |
| Proposed Level along Left Median Edge (m.) | 511.2779151879511627518187 |
| Existing Level Below Left Median Edge (m.) | 511.555333500511896431311 |
| Proposed Level along Right Median Edge (m.) | 511.2779151879511627518187 |
| Existing Level Below Right Median Edge (m.) | 511.555333500511896431311 |
| Horizontal Alignment Left | D=58.819, CL=43.000, R=497.790, L=82.877, CL=43.000, D=144.176, R=362.250, L=175.408, D=182.837, L=150.000, D=178.150 |
| Horizontal Alignment Right | D=53.678, CL=55.124, R=602.250, L=74.624, CL=55.124, D=136.384, R=367.790, L=173.334, D=182.825, L=150.000, D=178.150 |
| Vertical Alignment Left | L=150.000m, L=300.000m, L=150.000m, L=150.000m, L=150.000m |
| Vertical Alignment Right | L=150.000m, L=300.000m, L=150.000m, L=150.000m, L=150.000m |
| LHS Super-elevation | 0% to 3.4% |
| RHS Super-elevation | 3.4% to 0% |
| Left Profile Chainage | 0.000, 20.000, 40.000, 60.000, 80.000, 100.000, 120.000, 140.000, 160.000, 180.000, 200.000, 220.000, 240.000, 260.000, 280.000, 300.000, 320.000, 340.000, 360.000, 380.000, 400.000, 420.000, 440.000, 460.000, 480.000, 500.000, 520.000, 540.000, 560.000, 580.000, 600.000, 620.000, 640.000, 660.000, 680.000, 700.000, 720.000, 740.000, 760.000, 780.000, 800.000, 820.000, 840.000, 860.000, 880.000, 900.000, 920.000, 940.000, 960.000, 980.000, 1000.000 |
| Right Profile Chainage | 0.000, 20.000, 40.000, 60.000, 80.000, 100.000, 120.000, 140.000, 160.000, 180.000, 200.000, 220.000, 240.000, 260.000, 280.000, 300.000, 320.000, 340.000, 360.000, 380.000, 400.000, 420.000, 440.000, 460.000, 480.000, 500.000, 520.000, 540.000, 560.000, 580.000, 600.000, 620.000, 640.000, 660.000, 680.000, 700.000, 720.000, 740.000, 760.000, 780.000, 800.000, 820.000, 840.000, 860.000, 880.000, 900.000, 920.000, 940.000, 960.000, 980.000, 1000.000 |

| | |
|---|---|
| 1. Safety 10 | 1. Safety 10 |
| 2. Proposed Level along Left Median Edge | 2. Proposed Level along Left Median Edge |
| 3. Proposed Level along Right Median Edge | 3. Proposed Level along Right Median Edge |
| 4. Existing Level Below Left Median Edge | 4. Existing Level Below Left Median Edge |
| 5. Existing Level Below Right Median Edge | 5. Existing Level Below Right Median Edge |
| 6. Chainage | 6. Chainage |
| 7. Horizontal Alignment | 7. Horizontal Alignment |
| 8. Vertical Alignment | 8. Vertical Alignment |
| 9. Super-elevation | 9. Super-elevation |
| 10. Profile | 10. Profile |

Project Title: Consultancy services for preparation of detailed project report for Rehabilitation and upgrading of existing 2-lane road to 4-lane divided carriageway configuration of Jammu - Udhampur section of NH-1A (from 0+00 to last 64.70) including Jammu-Udhampur and Udhampur-Baramulla Phase - III Programme of North-South corridor Project. Contract Package C-0118

Client: NATIONAL HIGHWAYS AUTHORITY OF INDIA (Ministry of Shipping, Road Transport & Highways)

Drawing Title: PLAN & PROFILE Km. 0+000 to 1+000 (UDHAMPUR BYPASS)

Drawing No.: SWM/019/ROADS/PP-51

Scale: 1:2000

Drn: K.L.N. Dgn: K.PRASAD Appd: BRIG.C.D.PURI Date: JUNE-2009

Scott Wilson India Private Limited K-26/4, MOHAN CO-OPERATIVE IND. ESTATE SATAPTA ROAD NEW DELHI - 110044

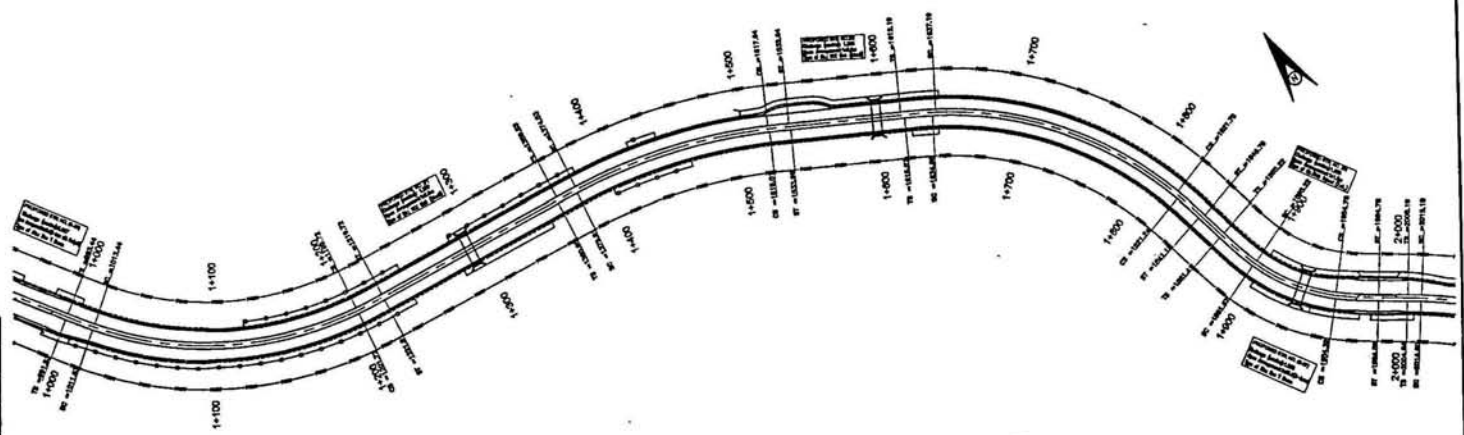
Tel: (011) 41679340 - 49 Fax: (011) 41679350 E-mail: swindia@rediffmail.com



← Jammu
131

Udhampur →

| |
|---|
| Level Datum = 473.000 |
| Proposed Level along Left Median Edge (m.) |
| Existing Level Below Left Median Edge (m.) |
| Proposed Level along Right Median Edge (m.) |
| Existing Level Below Right Median Edge (m.) |
| Horizontal Alignment Left |
| Horizontal Alignment Right |
| Vertical Alignment Left |
| Vertical Alignment Right |
| LHS Superelevation |
| RHS Superelevation |
| Left Profile Chainage |
| Right Profile Chainage |



| LEGEND | |
|--------------|--------------|
| For | Symbol |
| 1. Existing | 1. Existing |
| 2. Proposed | 2. Proposed |
| 3. Proposed | 3. Proposed |
| 4. Proposed | 4. Proposed |
| 5. Proposed | 5. Proposed |
| 6. Proposed | 6. Proposed |
| 7. Proposed | 7. Proposed |
| 8. Proposed | 8. Proposed |
| 9. Proposed | 9. Proposed |
| 10. Proposed | 10. Proposed |

Project Title
 Consultancy services for preparation of detailed project report for Rehabilitation and upgrading of existing 2-lane road to 4-lane divided carriageway configuration of Jammu - Udhampur section of NH-1A (Km 0.00 to Km 66.00) including Jammu/Chowk/Nigrota and Udhampur Bypass Phase - III Programme of North-South corridor Project Contract Package C-0313

Client
NATIONAL HIGHWAYS AUTHORITY OF INDIA
 (Ministry of Shipping, Road Transport & Highways)

Drawing Title: PLAN & PROFILE
Km. 1+000 to 2+000 (UDHAMPUR BYPASS)

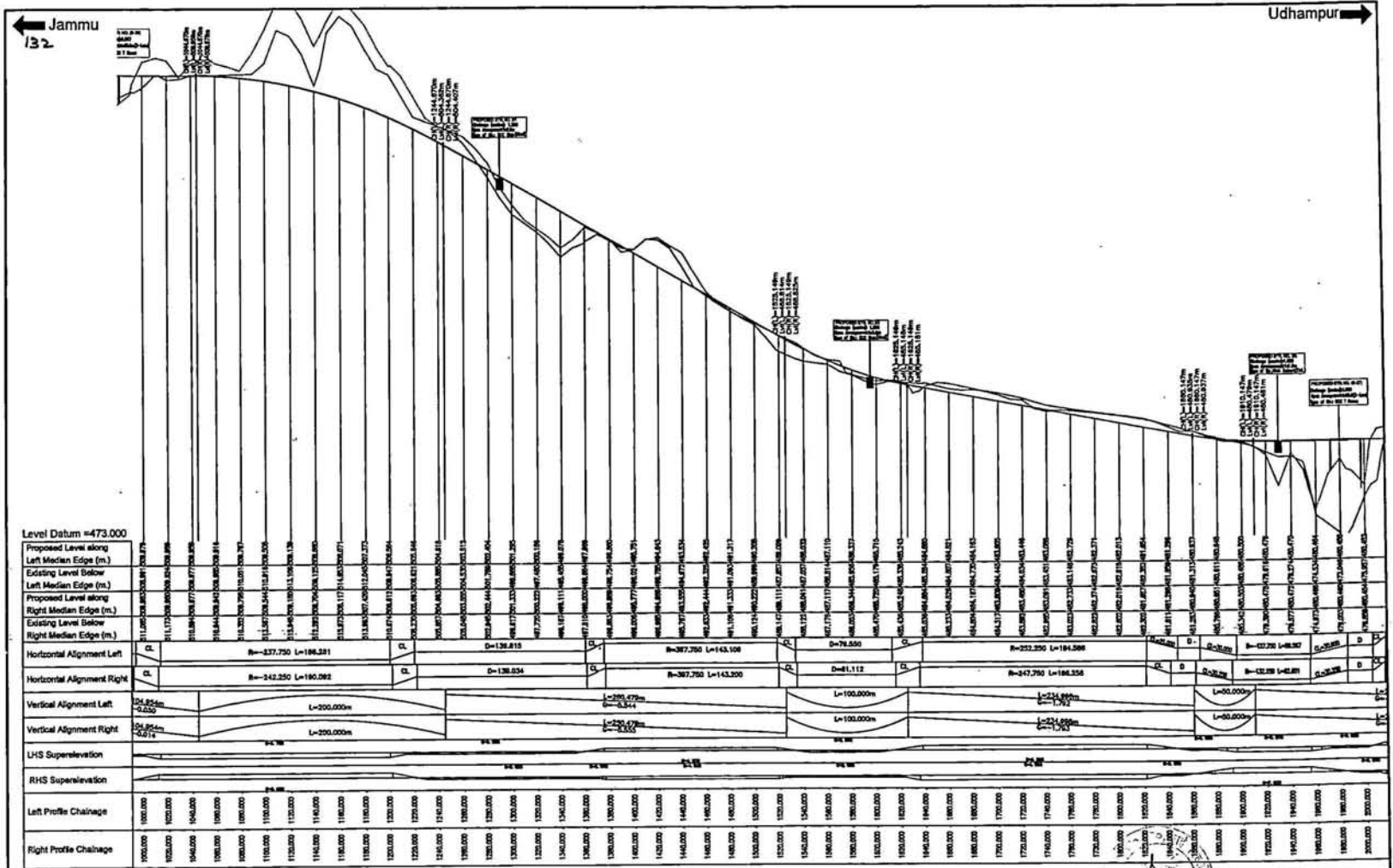
Drawing No.: SW/1019/ROADS/PP-52

Scale: HOR-1:2000 VER-1:200

Dr: K.L.N. **Dy:** K.PRASAD **Appd:** BRIG.C.D.PURI **Date:** JUNE-2009

Scott Wilson India Private Limited
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 Tel: (011) 41679340 - 49
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 E-mail: scw@scwpl.com





| | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
|---|---------------------|------------|------------|------------|---------------------|------------|------------|------------|---------------------|------------|---------------------|------------|---------------------|------------|---------------------|------------|---------------------|------------|---------------------|------------|---------------------|------------|---------------------|------------|---------------------|------------|---------------------|------------|---------------------|------------|---------------------|------------|---------------------|------------|---------------------|------------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|
| Level Datum =473.000 | 111.250000 | 112.000000 | 112.750000 | 113.500000 | 114.250000 | 115.000000 | 115.750000 | 116.500000 | 117.250000 | 118.000000 | 118.750000 | 119.500000 | 120.250000 | 121.000000 | 121.750000 | 122.500000 | 123.250000 | 124.000000 | 124.750000 | 125.500000 | 126.250000 | 127.000000 | 127.750000 | 128.500000 | 129.250000 | 130.000000 | 130.750000 | 131.500000 | 132.250000 | 133.000000 | 133.750000 | 134.500000 | 135.250000 | 136.000000 | 136.750000 | 137.500000 | | | | | | | | | | | | | | | |
| Proposed Level along Left Median Edge (m.) | 111.172000 | 111.920000 | 112.670000 | 113.420000 | 114.170000 | 114.920000 | 115.670000 | 116.420000 | 117.170000 | 117.920000 | 118.670000 | 119.420000 | 120.170000 | 120.920000 | 121.670000 | 122.420000 | 123.170000 | 123.920000 | 124.670000 | 125.420000 | 126.170000 | 126.920000 | 127.670000 | 128.420000 | 129.170000 | 129.920000 | 130.670000 | 131.420000 | 132.170000 | 132.920000 | 133.670000 | 134.420000 | 135.170000 | 135.920000 | 136.670000 | 137.420000 | | | | | | | | | | | | | | | |
| Existing Level Below Left Median Edge (m.) | 111.250000 | 112.000000 | 112.750000 | 113.500000 | 114.250000 | 115.000000 | 115.750000 | 116.500000 | 117.250000 | 118.000000 | 118.750000 | 119.500000 | 120.250000 | 121.000000 | 121.750000 | 122.500000 | 123.250000 | 124.000000 | 124.750000 | 125.500000 | 126.250000 | 127.000000 | 127.750000 | 128.500000 | 129.250000 | 130.000000 | 130.750000 | 131.500000 | 132.250000 | 133.000000 | 133.750000 | 134.500000 | 135.250000 | 136.000000 | 136.750000 | 137.500000 | | | | | | | | | | | | | | | |
| Proposed Level along Right Median Edge (m.) | 111.250000 | 112.000000 | 112.750000 | 113.500000 | 114.250000 | 115.000000 | 115.750000 | 116.500000 | 117.250000 | 118.000000 | 118.750000 | 119.500000 | 120.250000 | 121.000000 | 121.750000 | 122.500000 | 123.250000 | 124.000000 | 124.750000 | 125.500000 | 126.250000 | 127.000000 | 127.750000 | 128.500000 | 129.250000 | 130.000000 | 130.750000 | 131.500000 | 132.250000 | 133.000000 | 133.750000 | 134.500000 | 135.250000 | 136.000000 | 136.750000 | 137.500000 | | | | | | | | | | | | | | | |
| Existing Level Below Right Median Edge (m.) | 111.250000 | 112.000000 | 112.750000 | 113.500000 | 114.250000 | 115.000000 | 115.750000 | 116.500000 | 117.250000 | 118.000000 | 118.750000 | 119.500000 | 120.250000 | 121.000000 | 121.750000 | 122.500000 | 123.250000 | 124.000000 | 124.750000 | 125.500000 | 126.250000 | 127.000000 | 127.750000 | 128.500000 | 129.250000 | 130.000000 | 130.750000 | 131.500000 | 132.250000 | 133.000000 | 133.750000 | 134.500000 | 135.250000 | 136.000000 | 136.750000 | 137.500000 | | | | | | | | | | | | | | | |
| Horizontal Alignment Left | R=337.750 L=186.231 | | D=138.615 | | R=387.750 L=143.108 | | D=76.550 | | R=252.250 L=184.269 | | R=427.250 L=148.957 | | R=432.850 L=142.851 | | R=432.850 L=142.851 | | R=432.850 L=142.851 | | R=432.850 L=142.851 | | R=432.850 L=142.851 | | R=432.850 L=142.851 | | R=432.850 L=142.851 | | R=432.850 L=142.851 | | R=432.850 L=142.851 | | R=432.850 L=142.851 | | R=432.850 L=142.851 | | | | | | | | | | | | | | | | | | |
| Horizontal Alignment Right | R=342.250 L=180.282 | | D=138.634 | | R=387.750 L=143.200 | | D=81.112 | | R=247.750 L=184.254 | | R=427.250 L=148.957 | | R=432.850 L=142.851 | | R=432.850 L=142.851 | | R=432.850 L=142.851 | | R=432.850 L=142.851 | | R=432.850 L=142.851 | | R=432.850 L=142.851 | | R=432.850 L=142.851 | | R=432.850 L=142.851 | | R=432.850 L=142.851 | | R=432.850 L=142.851 | | R=432.850 L=142.851 | | R=432.850 L=142.851 | | | | | | | | | | | | | | | | |
| Vertical Alignment Left | L=200.000m | | L=200.000m | | L=100.000m | | L=100.000m | | L=200.000m | | L=200.000m | | L=200.000m | | L=200.000m | | L=200.000m | | L=200.000m | | L=200.000m | | L=200.000m | | L=200.000m | | L=200.000m | | L=200.000m | | L=200.000m | | L=200.000m | | L=200.000m | | | | | | | | | | | | | | | | |
| Vertical Alignment Right | L=200.000m | | L=200.000m | | L=100.000m | | L=100.000m | | L=200.000m | | L=200.000m | | L=200.000m | | L=200.000m | | L=200.000m | | L=200.000m | | L=200.000m | | L=200.000m | | L=200.000m | | L=200.000m | | L=200.000m | | L=200.000m | | L=200.000m | | L=200.000m | | | | | | | | | | | | | | | | |
| LHS Superelevation | 0.0000 | | 0.0000 | | 0.0000 | | 0.0000 | | 0.0000 | | 0.0000 | | 0.0000 | | 0.0000 | | 0.0000 | | 0.0000 | | 0.0000 | | 0.0000 | | 0.0000 | | 0.0000 | | 0.0000 | | 0.0000 | | 0.0000 | | 0.0000 | | | | | | | | | | | | | | | | |
| RHS Superelevation | 0.0000 | | 0.0000 | | 0.0000 | | 0.0000 | | 0.0000 | | 0.0000 | | 0.0000 | | 0.0000 | | 0.0000 | | 0.0000 | | 0.0000 | | 0.0000 | | 0.0000 | | 0.0000 | | 0.0000 | | 0.0000 | | 0.0000 | | 0.0000 | | | | | | | | | | | | | | | | |
| Left Profile Chainage | 100.000 | 102.000 | 104.000 | 106.000 | 108.000 | 110.000 | 112.000 | 114.000 | 116.000 | 118.000 | 120.000 | 122.000 | 124.000 | 126.000 | 128.000 | 130.000 | 132.000 | 134.000 | 136.000 | 138.000 | 140.000 | 142.000 | 144.000 | 146.000 | 148.000 | 150.000 | 152.000 | 154.000 | 156.000 | 158.000 | 160.000 | 162.000 | 164.000 | 166.000 | 168.000 | 170.000 | 172.000 | 174.000 | 176.000 | 178.000 | 180.000 | 182.000 | 184.000 | 186.000 | 188.000 | 190.000 | 192.000 | 194.000 | 196.000 | 198.000 | 200.000 |
| Right Profile Chainage | 100.000 | 102.000 | 104.000 | 106.000 | 108.000 | 110.000 | 112.000 | 114.000 | 116.000 | 118.000 | 120.000 | 122.000 | 124.000 | 126.000 | 128.000 | 130.000 | 132.000 | 134.000 | 136.000 | 138.000 | 140.000 | 142.000 | 144.000 | 146.000 | 148.000 | 150.000 | 152.000 | 154.000 | 156.000 | 158.000 | 160.000 | 162.000 | 164.000 | 166.000 | 168.000 | 170.000 | 172.000 | 174.000 | 176.000 | 178.000 | 180.000 | 182.000 | 184.000 | 186.000 | 188.000 | 190.000 | 192.000 | 194.000 | 196.000 | 198.000 | 200.000 |

LEGEND:

For Preparation of Profile

- 1. Existing G.L. of existing road
- 2. Proposed Level along Left Median Edge
- 3. Proposed Level along Right Median Edge
- 4. Existing Level Below Left Median Edge
- 5. Existing Level Below Right Median Edge
- 6. Level Datum
- 7. Road Bed

Project Title

Consultancy services for preparation of detailed project report for Rehabilitation and upgrading of existing 3-lane road to 4-lane divided carriageway configuration of Jammu - Udhampur section of NH-1 A (Sta 0+00 to Sta 66+90) including Jammu (Part) Bypass and Udhampur Bypass. Phase - II: Programme of North-South corridor Project, Central Package C-251A

Client

NATIONAL HIGHWAYS AUTHORITY OF INDIA
(Ministry of Shipping, Road Transport & Highways)

Drawing Title: PLAN & PROFILE
Km. 1+000 to 2+000 (UDHAMPUR BYPASS)

Drawing No.: SW/1919/ROADS/PP-S2

Scale: HOR: 1:2000 VER: 1:200

Drn: K.L.N **Dgn:** K.PRASAD **Appd:** BRG.C.D.PURI **Date:** JUNE-2009

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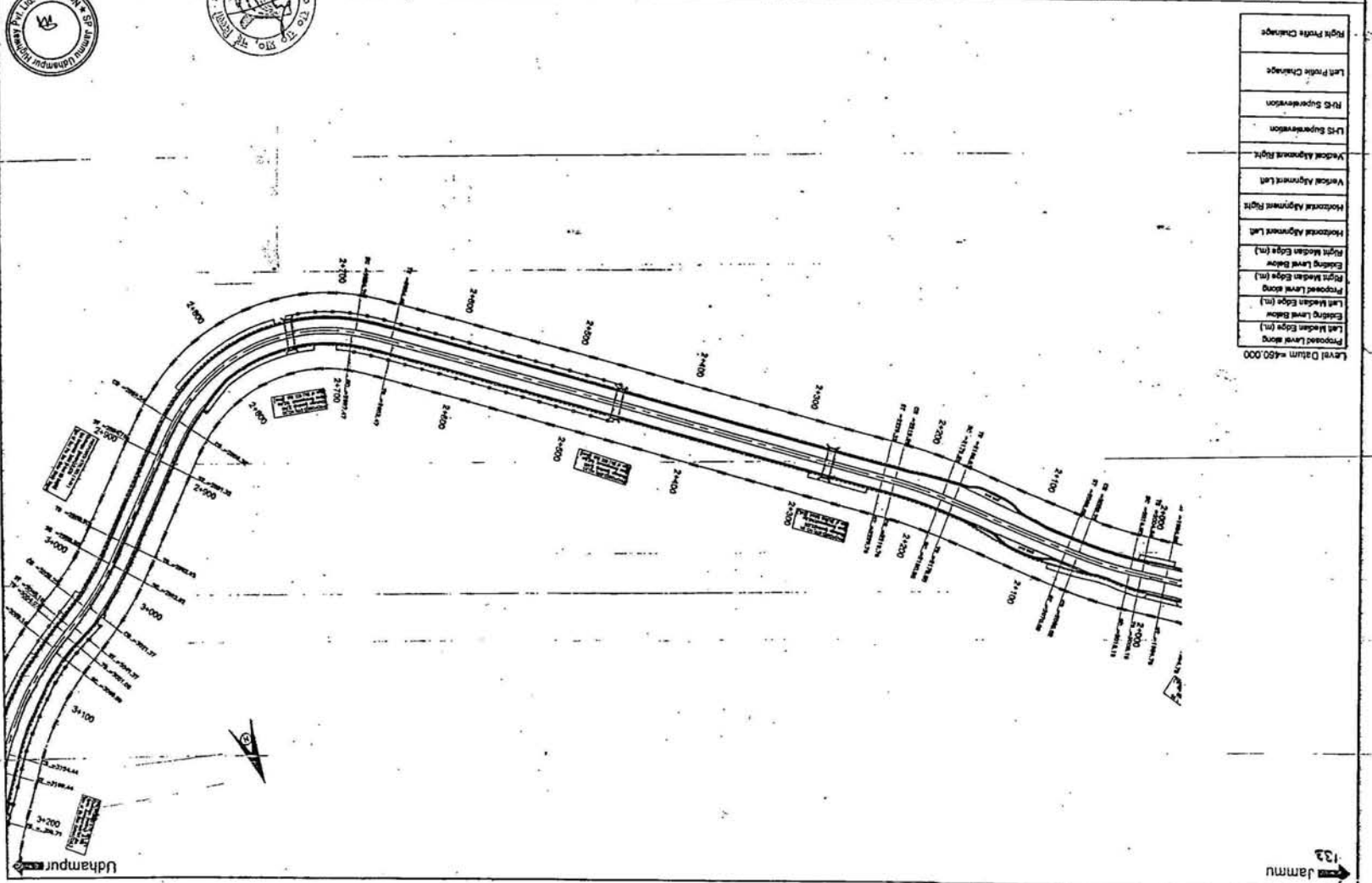


- Level Datum =450.000
- Proposed Lane Edge (m)
- Existing Lane Edge (m)
- Proposed Lane Edge (m)
- Existing Lane Edge (m)
- Proposed Lane Edge (m)
- Existing Lane Edge (m)
- Horizontal Alignment Left
- Horizontal Alignment Right
- Vertical Alignment Left
- Vertical Alignment Right
- US Super-elevation
- RTS Super-elevation
- Left Profile Change
- Right Profile Change

Project Title
 Contract Package 02/15
 Project Phase - 02 Progression of Plans - South - sections Project
 The Road is for 4x2 lanes (medium speed) and 2 lanes for 4x2 lanes
 Category - construction of access - interchange section of 101.1 A
 Construction and opening of existing lanes need to have divided
 Section of the road to be opened for traffic and the road to be
 opened for traffic and the road to be opened for traffic and the road to be
 opened for traffic and the road to be opened for traffic and the road to be

NATIONAL HIGHWAYS
AUTHORITY OF INDIA
 (Ministry of Shipping, Road Transport & Highways)

Contract No: SW/1019/Roads/PP-15
 Drawing No: SW/1019/Roads/PP-15/1
 Drawing Title: PLAN & PROFILE
 Km. 2+000 to 3+000 (UDHAMPUR BYPASS)

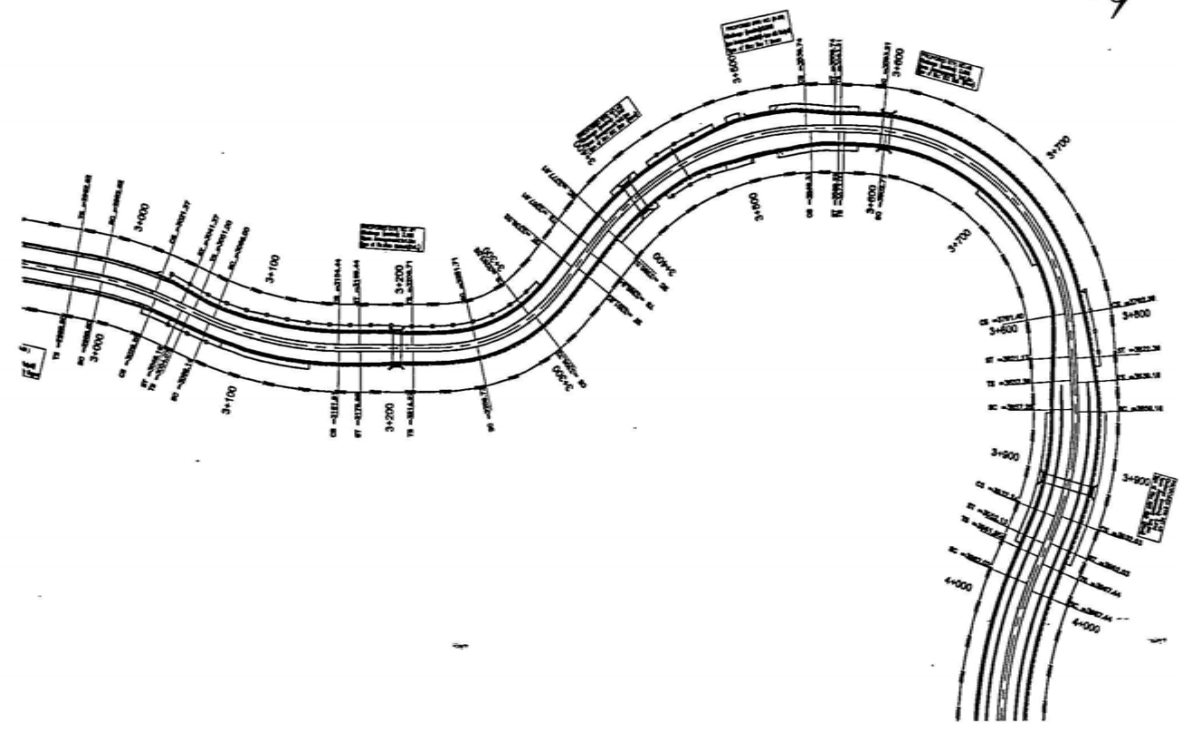


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 E-mail: info@swindia.com

133
 → Jammu

← Jammu
135

Udhampur →



| |
|---|
| Level Datum = 450.000 |
| Proposed Level along Left Median Edge (m.) |
| Existing Level Below Left Median Edge (m.) |
| Proposed Level along Right Median Edge (m.) |
| Existing Level Below Right Median Edge (m.) |
| Horizontal Alignment Left |
| Horizontal Alignment Right |
| Vertical Alignment Left |
| Vertical Alignment Right |
| LHS Superelevation |
| RHS Superelevation |
| Left Profile Chainage |
| Right Profile Chainage |

| REV. | DESCRIPTION | DATE |
|------|------------------|------|
| 1 | Issue for tender | |
| 2 | As per comments | |
| 3 | As per comments | |
| 4 | As per comments | |
| 5 | As per comments | |
| 6 | As per comments | |
| 7 | As per comments | |
| 8 | As per comments | |
| 9 | As per comments | |
| 10 | As per comments | |
| 11 | As per comments | |
| 12 | As per comments | |
| 13 | As per comments | |
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| 40 | As per comments | |
| 41 | As per comments | |
| 42 | As per comments | |
| 43 | As per comments | |
| 44 | As per comments | |
| 45 | As per comments | |
| 46 | As per comments | |
| 47 | As per comments | |
| 48 | As per comments | |
| 49 | As per comments | |
| 50 | As per comments | |

Project Title
 Consultancy services for preparation of detailed project report for Rehabilitation and upgrading of existing 3-lane road to 4-lane divided. Contiguity construction of drainage - Udhampur section of NH-44 A (km 0.20 to km 0.60) including Jammu-Port Nagar and Udhampur Bypass - 02 Programme of North-South corridor Project, Contract Package C-0218

Client
NATIONAL HIGHWAYS AUTHORITY OF INDIA
 (Ministry of Shipping, Road Transport & Highways)

Drawing Title: PLAN & PROFILE
 Km. 3+000 to 4+000 (UDHAMPUR-BYPASS)

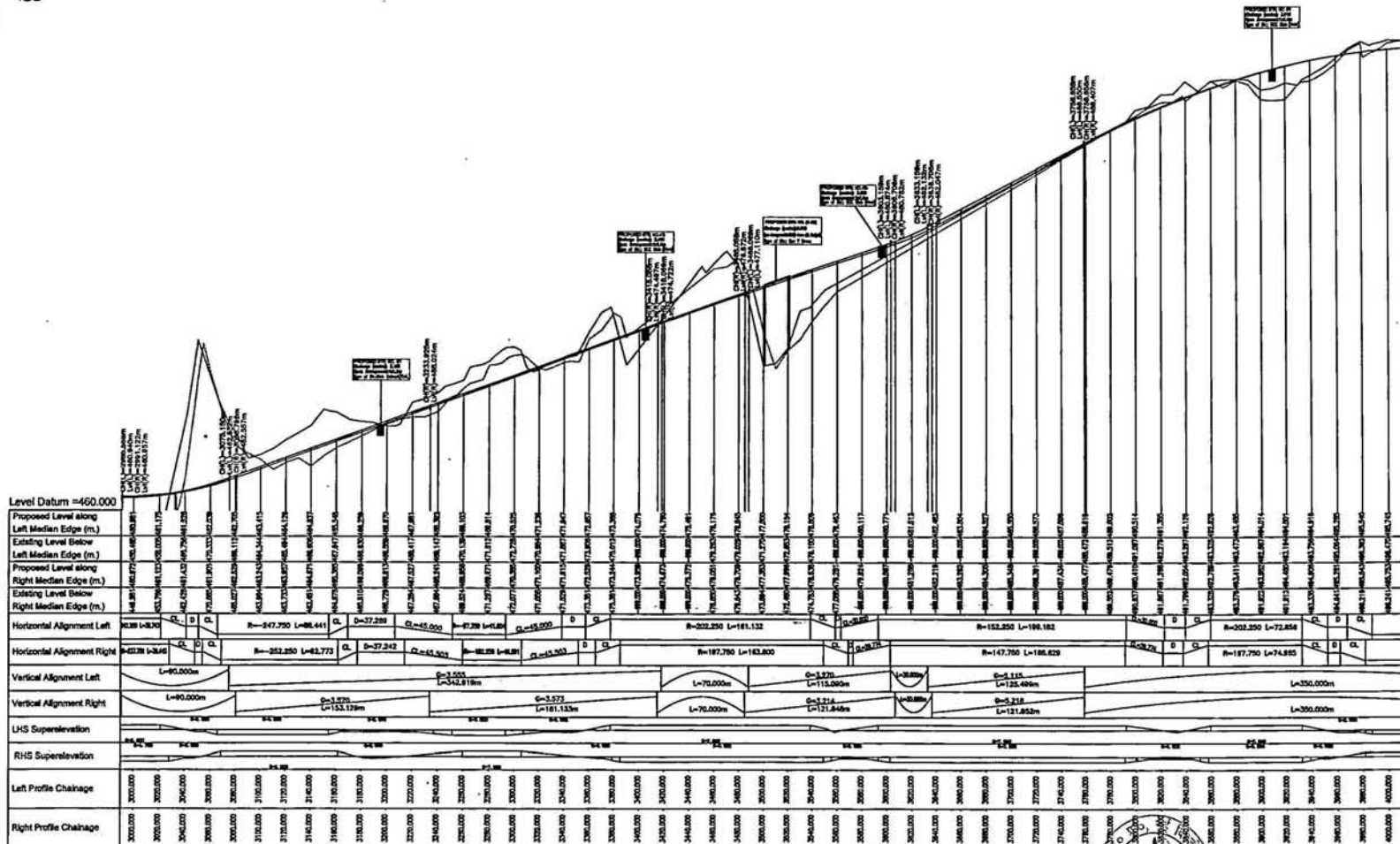
Drawing No.: SW/1019/ROADS/PP-54

Scale: HOR-1:2000 VER-1:200

Dr.: K.L.N. **Dgn.:** K.PRASAD **Appd:** BRIG.C.D.PUR **Scale:** 4566.1

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| For | ES&MP | Project Title |
|--------------------------|-------|---|
| 1. Proposed Road Layout | | Construction services for preparation of detailed project report for rehabilitation and upgrading of existing 3-lane road to 4-lane divided carriageway and junction of Jammu - Udhampur section of NH-1 A. (San 5.00 to last 56.00) including Jammu(Udh) Bypass and Udhampur Bypass Phase - III Programme of North-South corridor Project. Contract Package C-0918 |
| 2. Proposed Road Layout | | |
| 3. Proposed Road Layout | | |
| 4. Proposed Road Layout | | |
| 5. Proposed Road Layout | | |
| 6. Proposed Road Layout | | |
| 7. Proposed Road Layout | | |
| 8. Proposed Road Layout | | |
| 9. Proposed Road Layout | | |
| 10. Proposed Road Layout | | |

**NATIONAL HIGHWAYS
AUTHORITY OF INDIA**
(Ministry of Shipping, Road Transport & Highways)

Drawing Title: **PLAN & PROFILE**
Km. 3+000 to 4+000 (UDHAMPUR BYPASS)

Drawing No.: **SW/1018/ROADS/PP-54**

Scale: **HOR:-1:2000 VER:-1:200**

Date: **JUNE-2009**

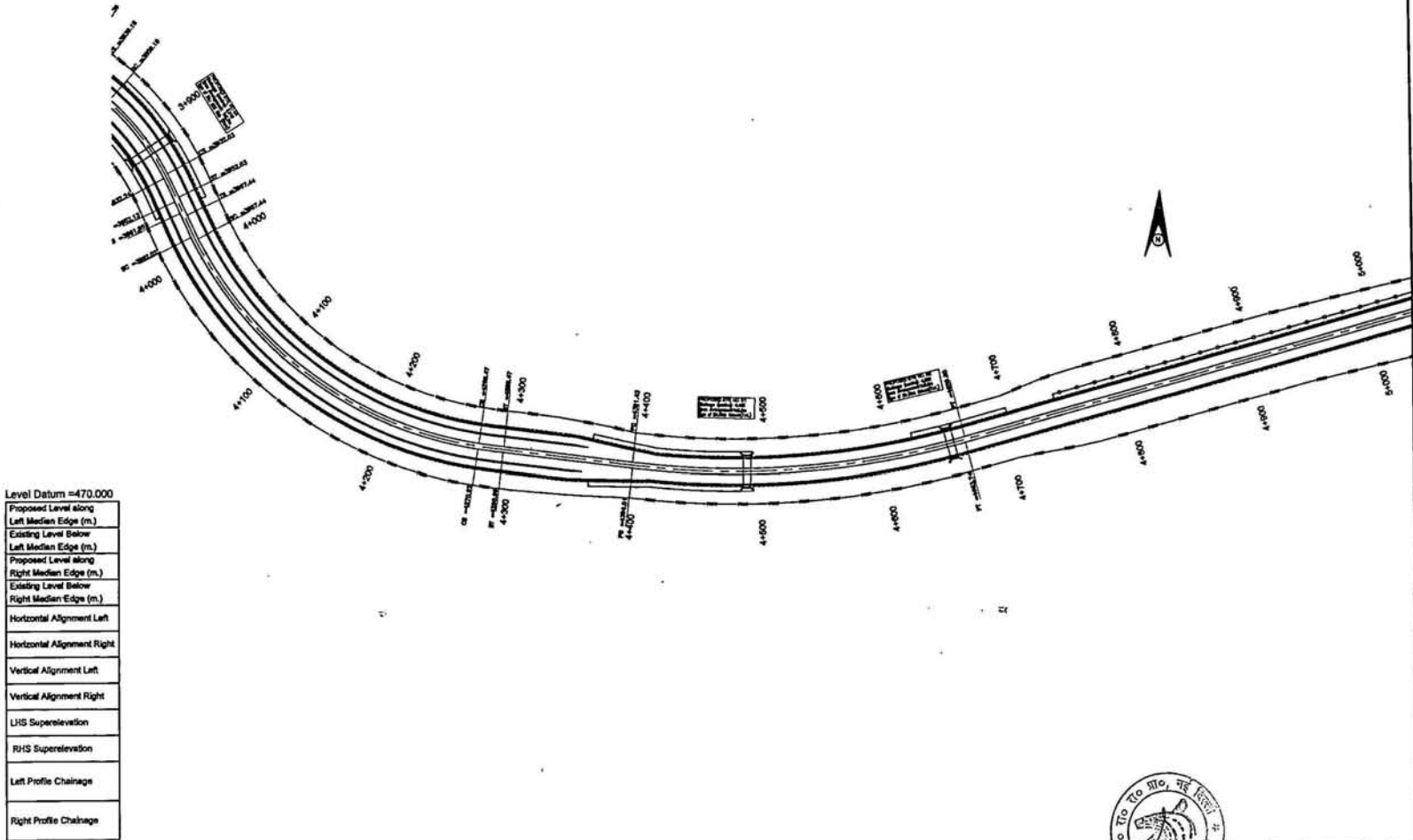
Sheet: **3 of 4**

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Jammu
13.7

Udhampur



Level Datum = 470.000

| |
|---|
| Proposed Level along Left Median Edge (m.) |
| Existing Level Below Left Median Edge (m.) |
| Proposed Level along Right Median Edge (m.) |
| Existing Level Below Right Median Edge (m.) |
| Horizontal Alignment Left |
| Horizontal Alignment Right |
| Vertical Alignment Left |
| Vertical Alignment Right |
| LHS Superelevation |
| RHS Superelevation |
| Left Profile Chainage |
| Right Profile Chainage |

| NO. | DESCRIPTION | DATE |
|-----|--------------------|------|
| 1 | Issue for approval | |
| 2 | Issue for approval | |
| 3 | Issue for approval | |
| 4 | Issue for approval | |
| 5 | Issue for approval | |
| 6 | Issue for approval | |
| 7 | Issue for approval | |
| 8 | Issue for approval | |
| 9 | Issue for approval | |
| 10 | Issue for approval | |

Project Title
 Consultancy services for preparation of detailed project report for Rehabilitation and upgrading of existing 2-lane road to 4-lane divided carriageway configuration of Jammu - Udhampur section of NH-1 A (km 0.00 to km 66.00) including Jammu(Ur)/Nagrin and Udhampur Bypass. Phase - III. Programme of North - South - corridor Project, Contract Package C-0214

Client
NATIONAL HIGHWAYS AUTHORITY OF INDIA
 (Ministry of Shipping, Road Transport & Highways)

Drawing Title: PLAN & PROFILE
 Km 4+000 to 5+000 (UDHAMPUR BYPASS)

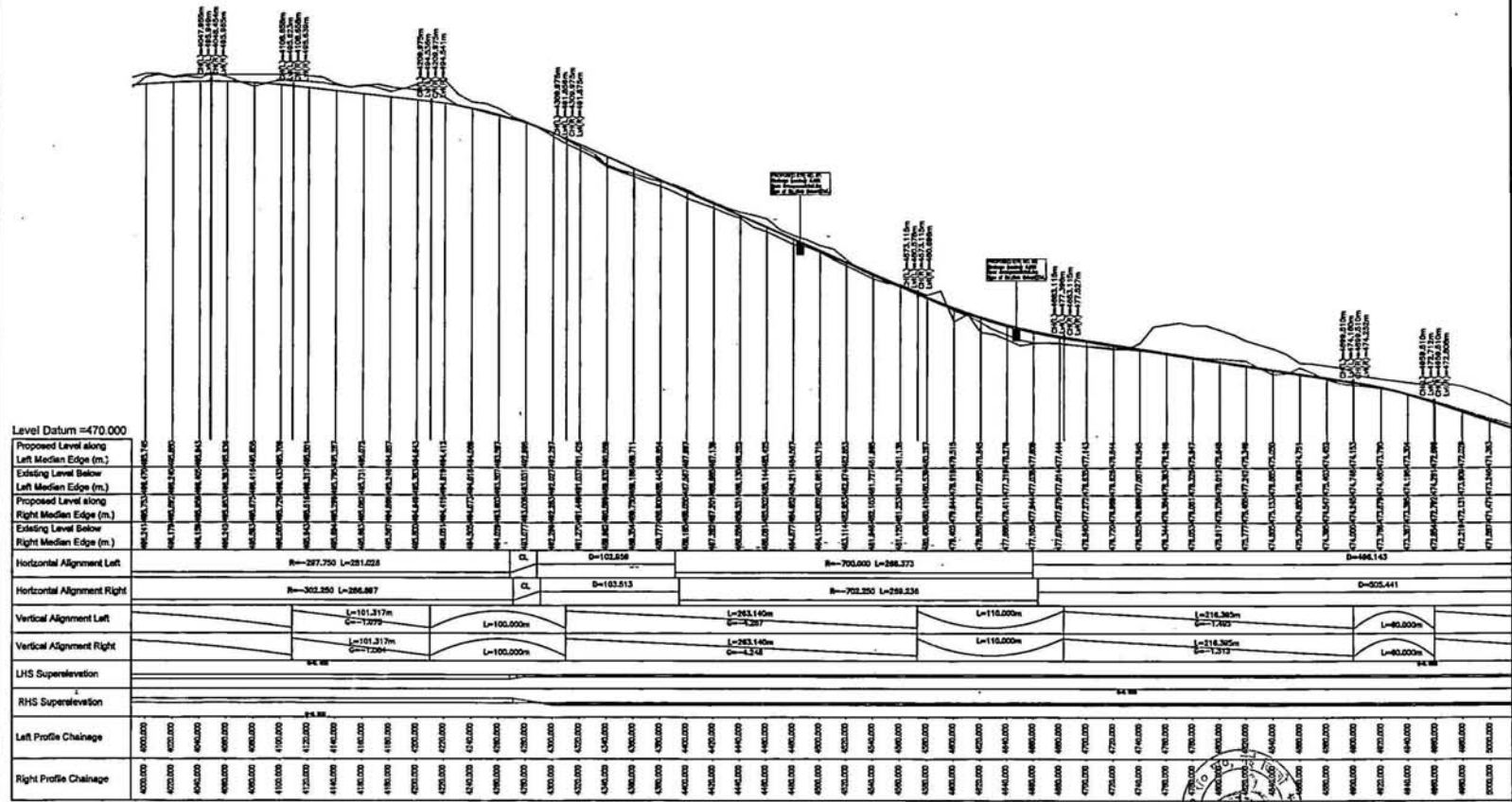
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 Scale: -/- HOR:-1:2000 VER:-1:200
 Date: JUNE-2009

Sheet No.: 1 of 2

Author: K.L.N.
Checked: K.PRASAD
Approved: BRIG.C.D.PURI

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 e-mail: swinfo@swindia.com





| Stationing | Proposed Level along Left Median Edge (m) | Existing Level Below Left Median Edge (m) | Proposed Level along Right Median Edge (m) | Existing Level Below Right Median Edge (m) |
|------------|---|---|--|--|
| 400.000 | 400.000 | 400.000 | 400.000 | 400.000 |
| 402.000 | 402.000 | 402.000 | 402.000 | 402.000 |
| 404.000 | 404.000 | 404.000 | 404.000 | 404.000 |
| 406.000 | 406.000 | 406.000 | 406.000 | 406.000 |
| 408.000 | 408.000 | 408.000 | 408.000 | 408.000 |
| 410.000 | 410.000 | 410.000 | 410.000 | 410.000 |
| 412.000 | 412.000 | 412.000 | 412.000 | 412.000 |
| 414.000 | 414.000 | 414.000 | 414.000 | 414.000 |
| 416.000 | 416.000 | 416.000 | 416.000 | 416.000 |
| 418.000 | 418.000 | 418.000 | 418.000 | 418.000 |
| 420.000 | 420.000 | 420.000 | 420.000 | 420.000 |
| 422.000 | 422.000 | 422.000 | 422.000 | 422.000 |
| 424.000 | 424.000 | 424.000 | 424.000 | 424.000 |
| 426.000 | 426.000 | 426.000 | 426.000 | 426.000 |
| 428.000 | 428.000 | 428.000 | 428.000 | 428.000 |
| 430.000 | 430.000 | 430.000 | 430.000 | 430.000 |
| 432.000 | 432.000 | 432.000 | 432.000 | 432.000 |
| 434.000 | 434.000 | 434.000 | 434.000 | 434.000 |
| 436.000 | 436.000 | 436.000 | 436.000 | 436.000 |
| 438.000 | 438.000 | 438.000 | 438.000 | 438.000 |
| 440.000 | 440.000 | 440.000 | 440.000 | 440.000 |
| 442.000 | 442.000 | 442.000 | 442.000 | 442.000 |
| 444.000 | 444.000 | 444.000 | 444.000 | 444.000 |
| 446.000 | 446.000 | 446.000 | 446.000 | 446.000 |
| 448.000 | 448.000 | 448.000 | 448.000 | 448.000 |
| 450.000 | 450.000 | 450.000 | 450.000 | 450.000 |
| 452.000 | 452.000 | 452.000 | 452.000 | 452.000 |
| 454.000 | 454.000 | 454.000 | 454.000 | 454.000 |
| 456.000 | 456.000 | 456.000 | 456.000 | 456.000 |
| 458.000 | 458.000 | 458.000 | 458.000 | 458.000 |
| 460.000 | 460.000 | 460.000 | 460.000 | 460.000 |
| 462.000 | 462.000 | 462.000 | 462.000 | 462.000 |
| 464.000 | 464.000 | 464.000 | 464.000 | 464.000 |
| 466.000 | 466.000 | 466.000 | 466.000 | 466.000 |
| 468.000 | 468.000 | 468.000 | 468.000 | 468.000 |
| 470.000 | 470.000 | 470.000 | 470.000 | 470.000 |
| 472.000 | 472.000 | 472.000 | 472.000 | 472.000 |
| 474.000 | 474.000 | 474.000 | 474.000 | 474.000 |
| 476.000 | 476.000 | 476.000 | 476.000 | 476.000 |
| 478.000 | 478.000 | 478.000 | 478.000 | 478.000 |
| 480.000 | 480.000 | 480.000 | 480.000 | 480.000 |
| 482.000 | 482.000 | 482.000 | 482.000 | 482.000 |
| 484.000 | 484.000 | 484.000 | 484.000 | 484.000 |
| 486.000 | 486.000 | 486.000 | 486.000 | 486.000 |
| 488.000 | 488.000 | 488.000 | 488.000 | 488.000 |
| 490.000 | 490.000 | 490.000 | 490.000 | 490.000 |
| 492.000 | 492.000 | 492.000 | 492.000 | 492.000 |
| 494.000 | 494.000 | 494.000 | 494.000 | 494.000 |
| 496.000 | 496.000 | 496.000 | 496.000 | 496.000 |
| 498.000 | 498.000 | 498.000 | 498.000 | 498.000 |
| 500.000 | 500.000 | 500.000 | 500.000 | 500.000 |

- LEGEND:**
- For Proposed Four Lanes**
- 1. Existing R.O. alignment
 - 2. Proposed Left Hand Side Edge
 - 3. Proposed Right Hand Side Edge
 - 4. Proposed Left Hand Side Edge
 - 5. Proposed Right Hand Side Edge
 - 6. Existing Left Hand Side Edge
 - 7. Existing Right Hand Side Edge

Project Title
 Consultancy services for preparation of detailed project report for Rehabilitation and upgrading of existing 2-lane road to 4-lane divided carriageway and provision of Jammu - Udhampur section of NH-1A (from 0+00 to km 66.00) including Jammu(Ut) Nagrota and Udhampur Bypass Phase - III Progress of North-South corridor Project, Contract Package C-2318

Client
NATIONAL HIGHWAYS AUTHORITY OF INDIA
 (Ministry of Shipping, Road Transport & Highways)

Drawing Title: PLAN & PROFILE
 Km. 4+000 to 5+000 (UDHAMPUR BYPASS)

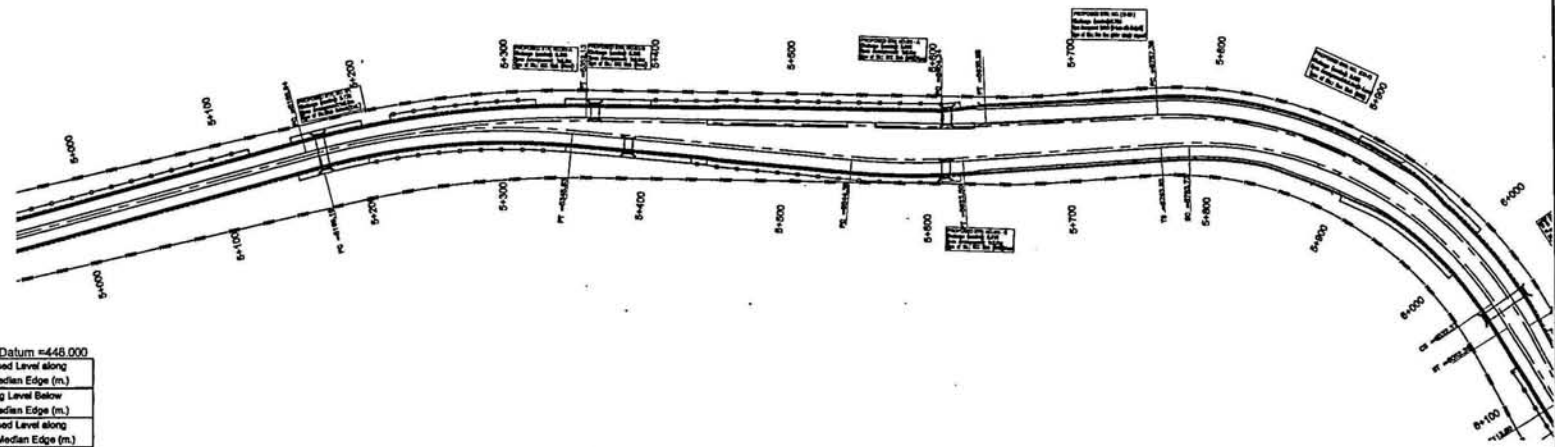
Drawing No.: SW/1018/ROADS/PP-55

Scale 1-: HOR:-1:2000 VER:-1:200

Dm: K.L.M. **Dgn.:** K.PRASAD **Appd:** BRG.C.D.PURI **Date:** JUNE-2009

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| |
|---|
| Level Datum = 446.000 |
| Proposed Level along Left Median Edge (m.) |
| Existing Level Below Left Median Edge (m.) |
| Proposed Level along Right Median Edge (m.) |
| Existing Level Below Right Median Edge (m.) |
| Horizontal Alignment Left |
| Horizontal Alignment Right |
| Vertical Alignment Left |
| Vertical Alignment Right |
| LHS Super-elevation |
| RHS Super-elevation |
| Left Profile Chainage |
| Right Profile Chainage |

| REVISED | FOR | BY | DATE |
|---------|--------|--------|--------|
| 1 | As per | As per | As per |
| 2 | As per | As per | As per |
| 3 | As per | As per | As per |
| 4 | As per | As per | As per |
| 5 | As per | As per | As per |
| 6 | As per | As per | As per |
| 7 | As per | As per | As per |
| 8 | As per | As per | As per |
| 9 | As per | As per | As per |
| 10 | As per | As per | As per |

Project Title
 Consultancy services for preparation of detailed project report for Rehabilitation and upgrading of existing 2-lane road to 4-lane divided carriageway configuration of Jammu - Udhampur section of NH-1A. (Km 0.00 to Km 66.00) including Jammu/ParV Nagar and Udhampur Bypass Phase - III Programme of North - South corridor Project, Contract Package C-02/14

Notes
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 3. The drawings shall be used only for the purpose specified in the contract documents.
 4. The drawings shall be used only for the purpose specified in the contract documents.
 5. The drawings shall be used only for the purpose specified in the contract documents.
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 8. The drawings shall be used only for the purpose specified in the contract documents.
 9. The drawings shall be used only for the purpose specified in the contract documents.
 10. The drawings shall be used only for the purpose specified in the contract documents.

Client
NATIONAL HIGHWAYS AUTHORITY OF INDIA
 (Ministry of Shipping, Road Transport & Highways)

Drawing Title: PLAN & PROFILE
 Km. 5+000 to 6+000 (UDHAMPUR BYPASS)

Drawing No.: SW/1019/ROADS/PP-56

Scale: HOR-1:2000 VER-1:200

Drawn: K.L.N **Dgn.:** K.PRASAD **Appd:** BRG.C.D.PURI **Date:** JUNE-2009

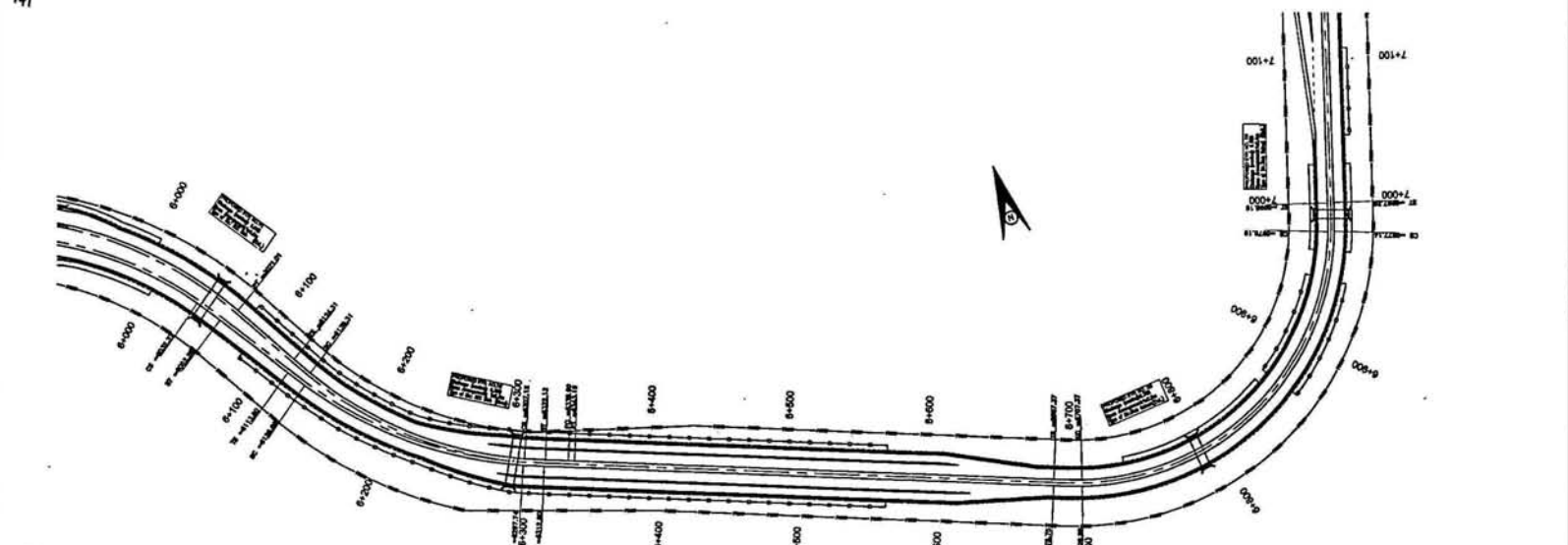
Sheet: 1 of 2

Client: SCOTI ROAD AND SERVICES LIMITED
 10/26/4, MOHAN CO-OPERATIVE IND. ESTATE
 NEW DELHI - 110044
 Tele: (011) 41679340 - 49
 Fax: (011) 41679350
 E-mail: scoti@scoti.com



Jammu ←

→ Udhampur



Level Datum = 454.000

| |
|---|
| Proposed Level along Left Median Edge (m.) |
| Existing Level Below Left Median Edge (m.) |
| Proposed Level along Right Median Edge (m.) |
| Existing Level Below Right Median Edge (m.) |
| Horizontal Alignment Left |
| Horizontal Alignment Right |
| Vertical Alignment Left |
| Vertical Alignment Right |
| LHS Super-elevation |
| RHS Super-elevation |
| Left Profile Chainage |
| Right Profile Chainage |

| Sl. No. | For Proposed | For Existing |
|---------|--------------------|--------------------|
| 1 | 1. Existing 4.5 m | 1. Existing 4.5 m |
| 2 | 2. Proposed 4.5 m | 2. Proposed 4.5 m |
| 3 | 3. Proposed 4.5 m | 3. Proposed 4.5 m |
| 4 | 4. Proposed 4.5 m | 4. Proposed 4.5 m |
| 5 | 5. Proposed 4.5 m | 5. Proposed 4.5 m |
| 6 | 6. Proposed 4.5 m | 6. Proposed 4.5 m |
| 7 | 7. Proposed 4.5 m | 7. Proposed 4.5 m |
| 8 | 8. Proposed 4.5 m | 8. Proposed 4.5 m |
| 9 | 9. Proposed 4.5 m | 9. Proposed 4.5 m |
| 10 | 10. Proposed 4.5 m | 10. Proposed 4.5 m |

Project Title
 Consultancy services for preparation of detailed project report for Rehabilitation and upgrading of existing 2-lane road to 4-lane divided carriageway configuration of Jammu - Udhampur section of NH-1 A (Km 6.00 to Km 64.00) including Jammu(Ur) Bypass and Udhampur Bypass Phase - III Programme of North-South corridor Project, Contract Package C-011

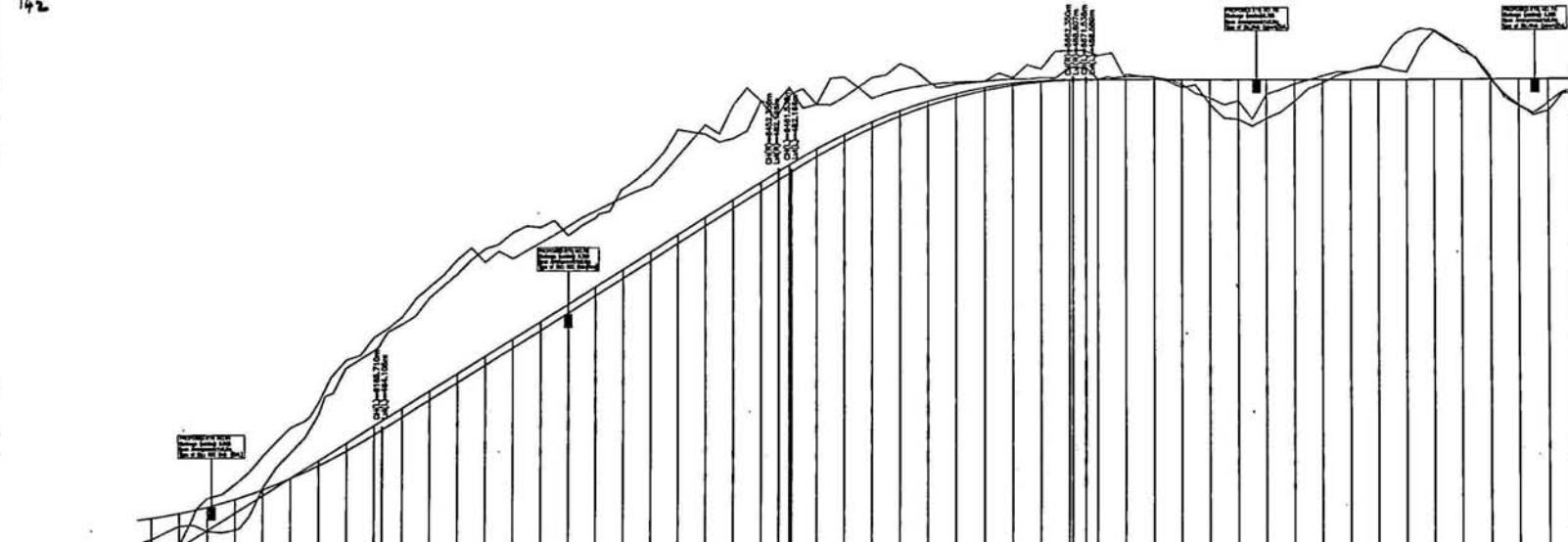
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Client
NATIONAL HIGHWAYS AUTHORITY OF INDIA
 (Ministry of Shipping, Road Transport & Highways)

Drawing Title: PLAN & PROFILE
 Km. 6+000 to 7+000 (UDHAMPUR BYPASS)
Drawing No.: SW/1019/ROADS/PP-57
Scale: HOR:-1:2000 VER:-1:200
Drn: K.L.N **Dgn:** K.PRASAD **Appd:** BSG.C.D.PURI **Date:** JUNE-2009

Client
Scott Wilson India Private Limited
 16/4, MOHAN CO-OPERATIVE IND. ESTATE
 MATURA ROAD
 NEW DELHI - 110044
 Tele: (011) 41679340 - 49
 Fax: (011) 41679350
 E-mail: scwilson@rediffmail.com





| Level Datum = 454.000 | Proposed Level along Left Median Edge (m.) | Existing Level Below Left Median Edge (m.) | Proposed Level along Right Median Edge (m.) | Existing Level Below Right Median Edge (m.) |
|-----------------------|--|--|---|---|
| 462.980 | 462.980 | 462.980 | 462.980 | 462.980 |
| 463.000 | 463.000 | 463.000 | 463.000 | 463.000 |
| 463.020 | 463.020 | 463.020 | 463.020 | 463.020 |
| 463.040 | 463.040 | 463.040 | 463.040 | 463.040 |
| 463.060 | 463.060 | 463.060 | 463.060 | 463.060 |
| 463.080 | 463.080 | 463.080 | 463.080 | 463.080 |
| 463.100 | 463.100 | 463.100 | 463.100 | 463.100 |
| 463.120 | 463.120 | 463.120 | 463.120 | 463.120 |
| 463.140 | 463.140 | 463.140 | 463.140 | 463.140 |
| 463.160 | 463.160 | 463.160 | 463.160 | 463.160 |
| 463.180 | 463.180 | 463.180 | 463.180 | 463.180 |
| 463.200 | 463.200 | 463.200 | 463.200 | 463.200 |
| 463.220 | 463.220 | 463.220 | 463.220 | 463.220 |
| 463.240 | 463.240 | 463.240 | 463.240 | 463.240 |
| 463.260 | 463.260 | 463.260 | 463.260 | 463.260 |
| 463.280 | 463.280 | 463.280 | 463.280 | 463.280 |
| 463.300 | 463.300 | 463.300 | 463.300 | 463.300 |
| 463.320 | 463.320 | 463.320 | 463.320 | 463.320 |
| 463.340 | 463.340 | 463.340 | 463.340 | 463.340 |
| 463.360 | 463.360 | 463.360 | 463.360 | 463.360 |
| 463.380 | 463.380 | 463.380 | 463.380 | 463.380 |
| 463.400 | 463.400 | 463.400 | 463.400 | 463.400 |
| 463.420 | 463.420 | 463.420 | 463.420 | 463.420 |
| 463.440 | 463.440 | 463.440 | 463.440 | 463.440 |
| 463.460 | 463.460 | 463.460 | 463.460 | 463.460 |
| 463.480 | 463.480 | 463.480 | 463.480 | 463.480 |
| 463.500 | 463.500 | 463.500 | 463.500 | 463.500 |
| 463.520 | 463.520 | 463.520 | 463.520 | 463.520 |
| 463.540 | 463.540 | 463.540 | 463.540 | 463.540 |
| 463.560 | 463.560 | 463.560 | 463.560 | 463.560 |
| 463.580 | 463.580 | 463.580 | 463.580 | 463.580 |
| 463.600 | 463.600 | 463.600 | 463.600 | 463.600 |
| 463.620 | 463.620 | 463.620 | 463.620 | 463.620 |
| 463.640 | 463.640 | 463.640 | 463.640 | 463.640 |
| 463.660 | 463.660 | 463.660 | 463.660 | 463.660 |
| 463.680 | 463.680 | 463.680 | 463.680 | 463.680 |
| 463.700 | 463.700 | 463.700 | 463.700 | 463.700 |
| 463.720 | 463.720 | 463.720 | 463.720 | 463.720 |
| 463.740 | 463.740 | 463.740 | 463.740 | 463.740 |
| 463.760 | 463.760 | 463.760 | 463.760 | 463.760 |
| 463.780 | 463.780 | 463.780 | 463.780 | 463.780 |
| 463.800 | 463.800 | 463.800 | 463.800 | 463.800 |
| 463.820 | 463.820 | 463.820 | 463.820 | 463.820 |
| 463.840 | 463.840 | 463.840 | 463.840 | 463.840 |
| 463.860 | 463.860 | 463.860 | 463.860 | 463.860 |
| 463.880 | 463.880 | 463.880 | 463.880 | 463.880 |
| 463.900 | 463.900 | 463.900 | 463.900 | 463.900 |
| 463.920 | 463.920 | 463.920 | 463.920 | 463.920 |
| 463.940 | 463.940 | 463.940 | 463.940 | 463.940 |
| 463.960 | 463.960 | 463.960 | 463.960 | 463.960 |
| 463.980 | 463.980 | 463.980 | 463.980 | 463.980 |
| 464.000 | 464.000 | 464.000 | 464.000 | 464.000 |

| LEGEND | Proposed Road Profile |
|---------------------------------------|---------------------------------------|
| 1. Existing R.O. alignment | 1. Proposed road and left side slope |
| 2. Proposed road and right side slope | 2. Proposed road and right side slope |
| 3. Proposed road and left side slope | 3. Proposed road and left side slope |
| 4. Proposed road and right side slope | 4. Proposed road and right side slope |
| 5. Proposed road and left side slope | 5. Proposed road and left side slope |
| 6. Proposed road and right side slope | 6. Proposed road and right side slope |
| 7. Proposed road and left side slope | 7. Proposed road and left side slope |
| 8. Proposed road and right side slope | 8. Proposed road and right side slope |

Project Title
 Consultancy services for preparation of detailed project report for Rehabilitation and upgrading of existing 3-lane road to 4-lane divided carriageway configuration of Jammu - Udhampur section of NH-1A
 (Km 6+00 to Km 6+70) including Jammu(U) Projects and Udhampur bypass Phase - III Programme of North-South corridor Project, Contract Package C-0314

Note:
 1. All measurements shall be in meters and rounded up to the nearest mm in any dimension of length and rounded up to the nearest 0.1 m in any dimension of area and rounded up to the nearest 0.01 m² in any dimension of volume.
 2. The existing road is 3-lane road and should not be widened to 4-lane road. The road is to be widened to 4-lane road by providing a 2.0 m wide shoulder on each side of the road.
 3. The existing road is 3-lane road and should not be widened to 4-lane road. The road is to be widened to 4-lane road by providing a 2.0 m wide shoulder on each side of the road.
 4. The existing road is 3-lane road and should not be widened to 4-lane road. The road is to be widened to 4-lane road by providing a 2.0 m wide shoulder on each side of the road.
 5. The existing road is 3-lane road and should not be widened to 4-lane road. The road is to be widened to 4-lane road by providing a 2.0 m wide shoulder on each side of the road.

Client
NATIONAL HIGHWAYS AUTHORITY OF INDIA
 (Ministry of Shipping, Road Transport & Highways)

Drawing Title: PLAN & PROFILE
 Km. 6+000 to 7+000 (UDHAMPUR BYPASS)
Drawing No.: SW/1019/ROADS/PP-57
Scale: HORIZ-1:2000 VERT-1:200
Date: JUNE-2009
Drawn by: K.L.N.
Checked by: K.P.RASAD
Appr'd by: B.M.G.C.D.PUR

Sothi Wilson India Private Limited
 158/A, MOHAN CO-OPERATIVE IND. ESTATE
 MATIGURA ROAD
 NEW DELHI - 110044
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 Fax: (011) 41679350
 E-mail: swindia@vsnl.com



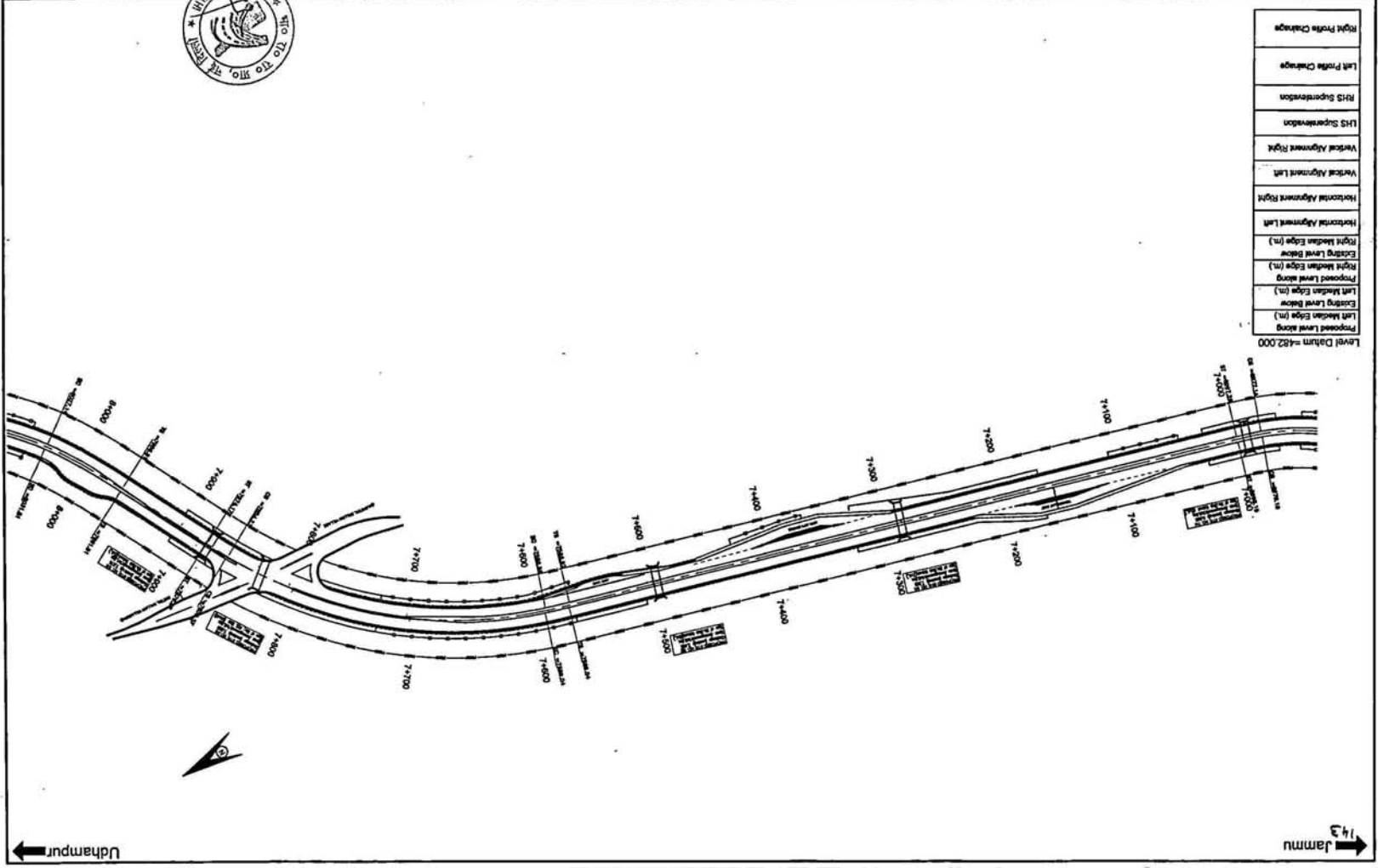
| | |
|----------------------------|-------|
| Proposed Level Above | _____ |
| Left Median Edge (m) | _____ |
| Proposed Level Below | _____ |
| Left Median Edge (m) | _____ |
| Right Median Edge (m) | _____ |
| Proposed Level Above | _____ |
| Right Median Edge (m) | _____ |
| Existing Level Below | _____ |
| Horizontal Alignment Left | _____ |
| Horizontal Alignment Right | _____ |
| Vertical Alignment Left | _____ |
| Vertical Alignment Right | _____ |
| LHS Superelevation | _____ |
| RHS Superelevation | _____ |
| Left Profile Change | _____ |
| Right Profile Change | _____ |

Project Title: _____
 Client: _____
 Drawing No.: _____
 Scale: _____
 Date: _____

NATIONAL HIGHWAYS
 AUTHORITY OF INDIA
 (Ministry of Shipping, Road Transport & Highways)

Drawing Title: **PLAN & PROFILE**
 Km. 7+000 to 8+000 (UDHAMPUR BYPASS)

Client: **Scott Wilson India Private Limited**
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 E-mail: info@swiindia.com

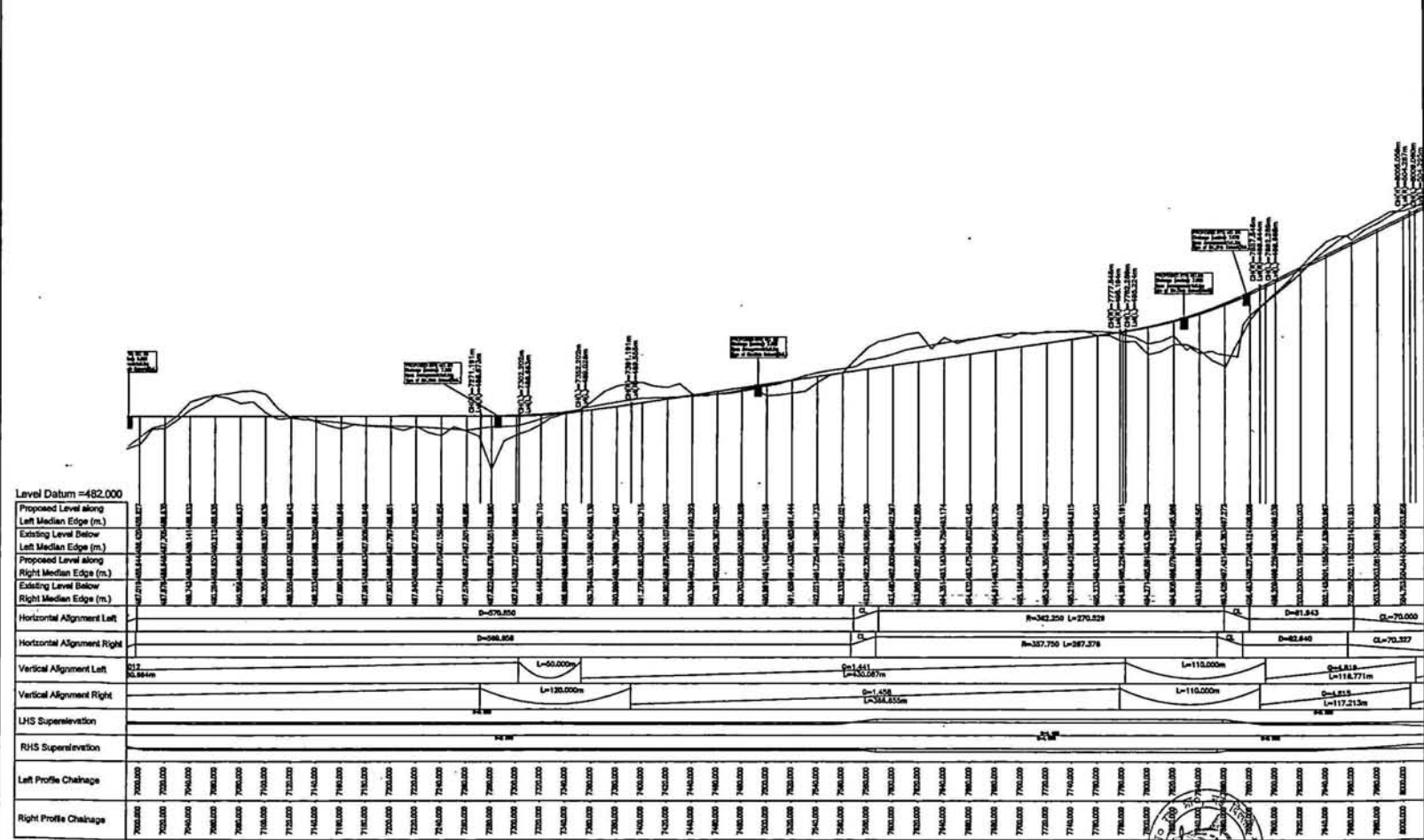


Jammu → 14

← Udhampur

Jammu
144

Udhampur



Project Title
 Consultancy services for preparation of detailed project report for Rehabilitation and upgrading of existing 3-lane road to 4-lane divided Carriageway (realignments of Jammu - Udhampur section of NH-1 A) (on 0.50 to km 64.80) including Jammu (Part) Bypass and Udhampur Bypass Phase - III Programme of North-South corridor Project, Contract Package C-8018

Client
 NATIONAL HIGHWAYS AUTHORITY OF INDIA
 (Ministry of Shipping, Road Transport & Highways)

Drawing Title: PLAN & PROFILE
 Km. 7+000 to 8+000 (UDHAMPUR BYPASS)

Drawing No.: SW/1018/ROADS/PP-58

Scale: HOR-1:2000 VER-1:200

Date: JUNE-2009

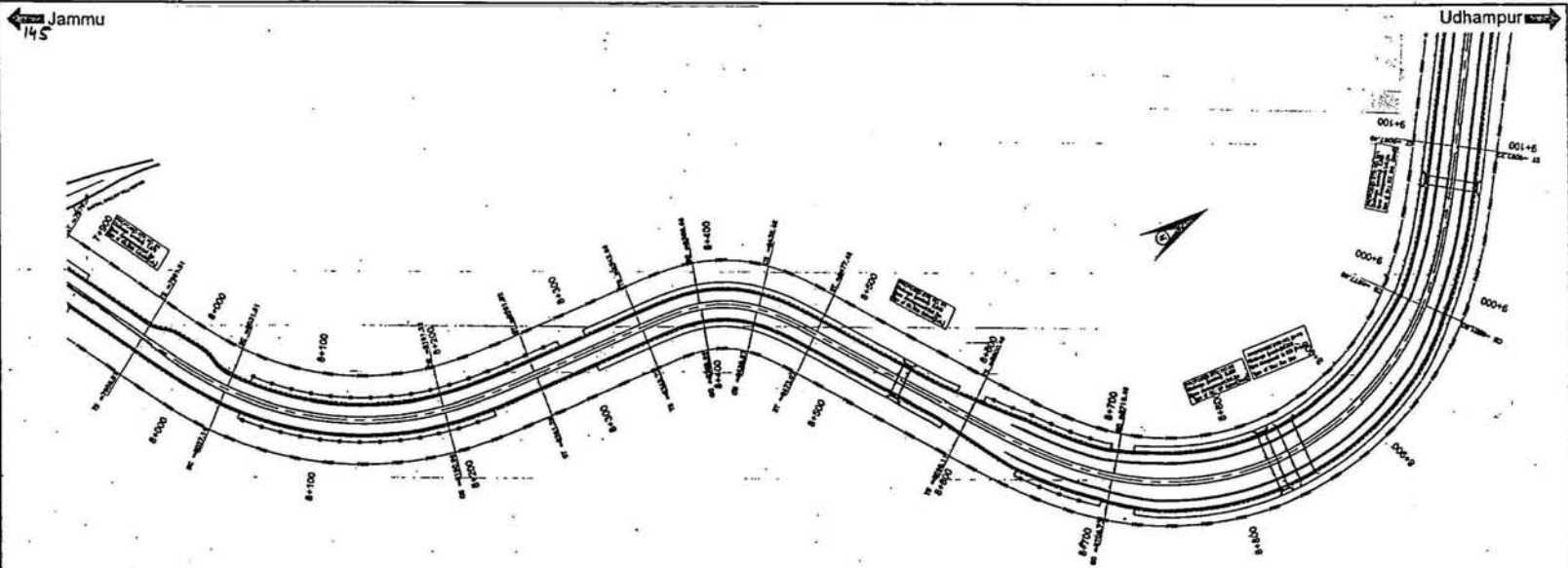
Drawn by: K.PRASAD

Checked by: ERIG.C.D.PURI

Scott Wilson India Private Limited
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 NEW DELHI - 110044
 Tele: (011) 41679340 - 49
 Fax: (011) 41679350
 E-mail: info@swi.com

Stamp: NATIONAL HIGHWAYS AUTHORITY OF INDIA





Level Datum = 480.000

| |
|---|
| Proposed Level along Left Median Edge (m.) |
| Existing Level Below Left Median Edge (m.) |
| Proposed Level along Right Median Edge (m.) |
| Existing Level Below Right Median Edge (m.) |
| Horizontal Alignment Left |
| Horizontal Alignment Right |
| Vertical Alignment Left |
| Vertical Alignment Right |
| LHS Superelevation |
| RHS Superelevation |
| Left Profile Chainage |
| Right Profile Chainage |

| Sl. No. | Particulars | Quantity |
|---------|-------------|----------|
| 1. | ... | ... |
| 2. | ... | ... |
| 3. | ... | ... |
| 4. | ... | ... |
| 5. | ... | ... |
| 6. | ... | ... |
| 7. | ... | ... |
| 8. | ... | ... |
| 9. | ... | ... |
| 10. | ... | ... |
| 11. | ... | ... |
| 12. | ... | ... |
| 13. | ... | ... |
| 14. | ... | ... |
| 15. | ... | ... |
| 16. | ... | ... |
| 17. | ... | ... |
| 18. | ... | ... |
| 19. | ... | ... |
| 20. | ... | ... |
| 21. | ... | ... |
| 22. | ... | ... |
| 23. | ... | ... |
| 24. | ... | ... |
| 25. | ... | ... |
| 26. | ... | ... |
| 27. | ... | ... |
| 28. | ... | ... |
| 29. | ... | ... |
| 30. | ... | ... |
| 31. | ... | ... |
| 32. | ... | ... |
| 33. | ... | ... |
| 34. | ... | ... |
| 35. | ... | ... |
| 36. | ... | ... |
| 37. | ... | ... |
| 38. | ... | ... |
| 39. | ... | ... |
| 40. | ... | ... |
| 41. | ... | ... |
| 42. | ... | ... |
| 43. | ... | ... |
| 44. | ... | ... |
| 45. | ... | ... |
| 46. | ... | ... |
| 47. | ... | ... |
| 48. | ... | ... |
| 49. | ... | ... |
| 50. | ... | ... |

Project Title
 Consultancy services for preparation of detailed project report for Rehabilitation and upgrading of existing 2-lane road to 4-lane divided carriageway configuration of Jammu - Udhampur section of NH-1A (Km 8.00 to Km 84.00) including Jammu/Parit Nagar and Udhampur Bypass. Plans - 02 Programme of North - South - considers Project, Contract Package C-42 14

The drawings in this project are prepared by the consultant on the basis of the data provided by the client. The consultant is not responsible for any errors or omissions in the drawings or for any damage or loss of property or other loss or injury to persons or property caused by the use of the drawings. The consultant is not responsible for any delay or non-completion of the project or for any other loss or injury to persons or property caused by the use of the drawings.

Client:
NATIONAL HIGHWAYS AUTHORITY OF INDIA
 (Ministry of Shipping, Road Transport & Highways)

Drawing Title: PLAN & PROFILE
 Km. 8+000 to 9+000 (UDHAMPUR BYPASS)

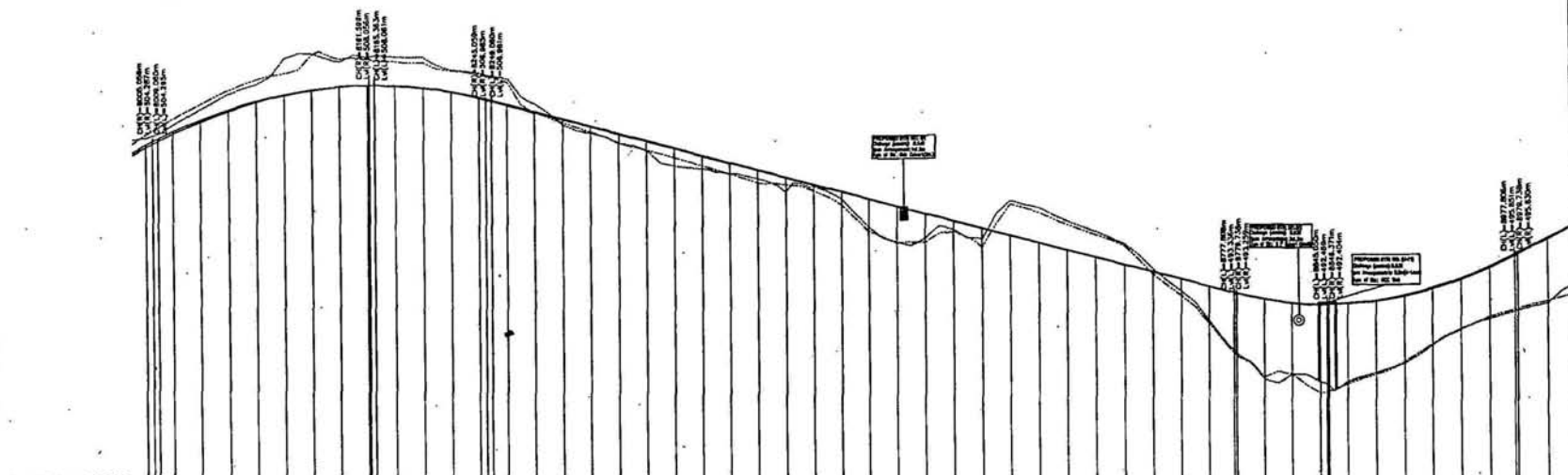
Drawing No.: SW/1019/ROADS/PP - 55

Scale: HOR:-1:2000 VER:-1:200

Dgn.: K.L.N
Appd.: K.PRASAD
Date: JUNE-2009

Scale: Wilson India Private Limited
 25/A, MOHAN CO-OPERATIVE IND. ESTATE
 MADHUR ROAD
 NEW DELHI - 110044
 Tele: (011) 41679240 - 49
 Fax: (011) 41679350
 E-mail: waindia@waindia.com





| | | | | | | | | | | | | | | | | | | | | | | |
|---|---------------------|----------|---------------------|----------|---------------------|----------|---------------------|----------|---------------------|----------|---------------------|----------|---------------------|----------|---------------------|----------|---------------------|----------|---------------------|----------|---------------------|--|
| Level Datum = 480.000 | 8000.000 | 8005.000 | 8010.000 | 8015.000 | 8020.000 | 8025.000 | 8030.000 | 8035.000 | 8040.000 | 8045.000 | 8050.000 | 8055.000 | 8060.000 | 8065.000 | 8070.000 | 8075.000 | 8080.000 | 8085.000 | 8090.000 | 8095.000 | 9000.000 | |
| Proposed Level along Left Median Edge (m.) | 8000.000 | 8005.000 | 8010.000 | 8015.000 | 8020.000 | 8025.000 | 8030.000 | 8035.000 | 8040.000 | 8045.000 | 8050.000 | 8055.000 | 8060.000 | 8065.000 | 8070.000 | 8075.000 | 8080.000 | 8085.000 | 8090.000 | 8095.000 | 9000.000 | |
| Existing Level Below Left Median Edge (m.) | 8000.000 | 8005.000 | 8010.000 | 8015.000 | 8020.000 | 8025.000 | 8030.000 | 8035.000 | 8040.000 | 8045.000 | 8050.000 | 8055.000 | 8060.000 | 8065.000 | 8070.000 | 8075.000 | 8080.000 | 8085.000 | 8090.000 | 8095.000 | 9000.000 | |
| Proposed Level along Right Median Edge (m.) | 8000.000 | 8005.000 | 8010.000 | 8015.000 | 8020.000 | 8025.000 | 8030.000 | 8035.000 | 8040.000 | 8045.000 | 8050.000 | 8055.000 | 8060.000 | 8065.000 | 8070.000 | 8075.000 | 8080.000 | 8085.000 | 8090.000 | 8095.000 | 9000.000 | |
| Existing Level Below Right Median Edge (m.) | 8000.000 | 8005.000 | 8010.000 | 8015.000 | 8020.000 | 8025.000 | 8030.000 | 8035.000 | 8040.000 | 8045.000 | 8050.000 | 8055.000 | 8060.000 | 8065.000 | 8070.000 | 8075.000 | 8080.000 | 8085.000 | 8090.000 | 8095.000 | 9000.000 | |
| Horizontal Alignment Left | R=237.750 L=158.803 | | R=243.250 L=163.828 | | R=243.250 L=163.828 | | R=243.250 L=163.828 | | R=243.250 L=163.828 | | R=243.250 L=163.828 | | R=243.250 L=163.828 | | R=243.250 L=163.828 | | R=243.250 L=163.828 | | R=243.250 L=163.828 | | R=243.250 L=163.828 | |
| Horizontal Alignment Right | R=237.750 L=158.803 | | R=243.250 L=163.828 | | R=243.250 L=163.828 | | R=243.250 L=163.828 | | R=243.250 L=163.828 | | R=243.250 L=163.828 | | R=243.250 L=163.828 | | R=243.250 L=163.828 | | R=243.250 L=163.828 | | R=243.250 L=163.828 | | R=243.250 L=163.828 | |
| Vertical Alignment Left | L=240.000m | | L=240.000m | | L=240.000m | | L=240.000m | | L=240.000m | | L=240.000m | | L=240.000m | | L=240.000m | | L=240.000m | | L=240.000m | | L=240.000m | |
| Vertical Alignment Right | L=240.000m | | L=240.000m | | L=240.000m | | L=240.000m | | L=240.000m | | L=240.000m | | L=240.000m | | L=240.000m | | L=240.000m | | L=240.000m | | L=240.000m | |
| LHS Super-elevation | 0.000 | | 0.000 | | 0.000 | | 0.000 | | 0.000 | | 0.000 | | 0.000 | | 0.000 | | 0.000 | | 0.000 | | 0.000 | |
| RHS Super-elevation | 0.000 | | 0.000 | | 0.000 | | 0.000 | | 0.000 | | 0.000 | | 0.000 | | 0.000 | | 0.000 | | 0.000 | | 0.000 | |
| Left Profile Chirage | 8000.000 | | 8005.000 | | 8010.000 | | 8015.000 | | 8020.000 | | 8025.000 | | 8030.000 | | 8035.000 | | 8040.000 | | 8045.000 | | 8050.000 | |
| Right Profile Chirage | 8000.000 | | 8005.000 | | 8010.000 | | 8015.000 | | 8020.000 | | 8025.000 | | 8030.000 | | 8035.000 | | 8040.000 | | 8045.000 | | 8050.000 | |

Legend:

| | |
|--|-------|
| Proposed Level | ----- |
| Existing Level | ----- |
| Proposed Level along Left Median Edge | ----- |
| Existing Level along Left Median Edge | ----- |
| Proposed Level along Right Median Edge | ----- |
| Existing Level along Right Median Edge | ----- |
| Horizontal Alignment | ----- |
| Vertical Alignment | ----- |
| LHS Super-elevation | ----- |
| RHS Super-elevation | ----- |
| Left Profile Chirage | ----- |
| Right Profile Chirage | ----- |

Project Title: Construction of 4-lane divided highway section of NH-1A from Km. 0+00 to Km. 9+000 including Jammu/Partly Rights and Udhampur Bypass Phase - II (Proprietary of North-South Corridor Project, Contract Package C-02/18)

Notes:

- The drawings are the property of the Engineer-in-Charge and must not be used for any purpose other than that intended without the written consent of the Engineer-in-Charge.
- The drawings are to be used as a guide only and are not to be used as a basis for any construction work without the written consent of the Engineer-in-Charge.
- The drawings are to be used as a guide only and are not to be used as a basis for any construction work without the written consent of the Engineer-in-Charge.

Client: NATIONAL HIGHWAYS AUTHORITY OF INDIA (Ministry of Shipping, Road Transport & Highways)

Drawing Title: PLAN & PROFILE
Km. 0+000 to 9+000 (UDHAMPUR BYPASS)

Drawing No.: SWW/1019/ROADS/PP-50
Scale: -1:2000 HOR: -1:200 VER: -1:200

Drn: K.L.N **Dgn:** K.PRASAD **Appd:** BRIG.C.D.PUR

Sheet: 2 of 2

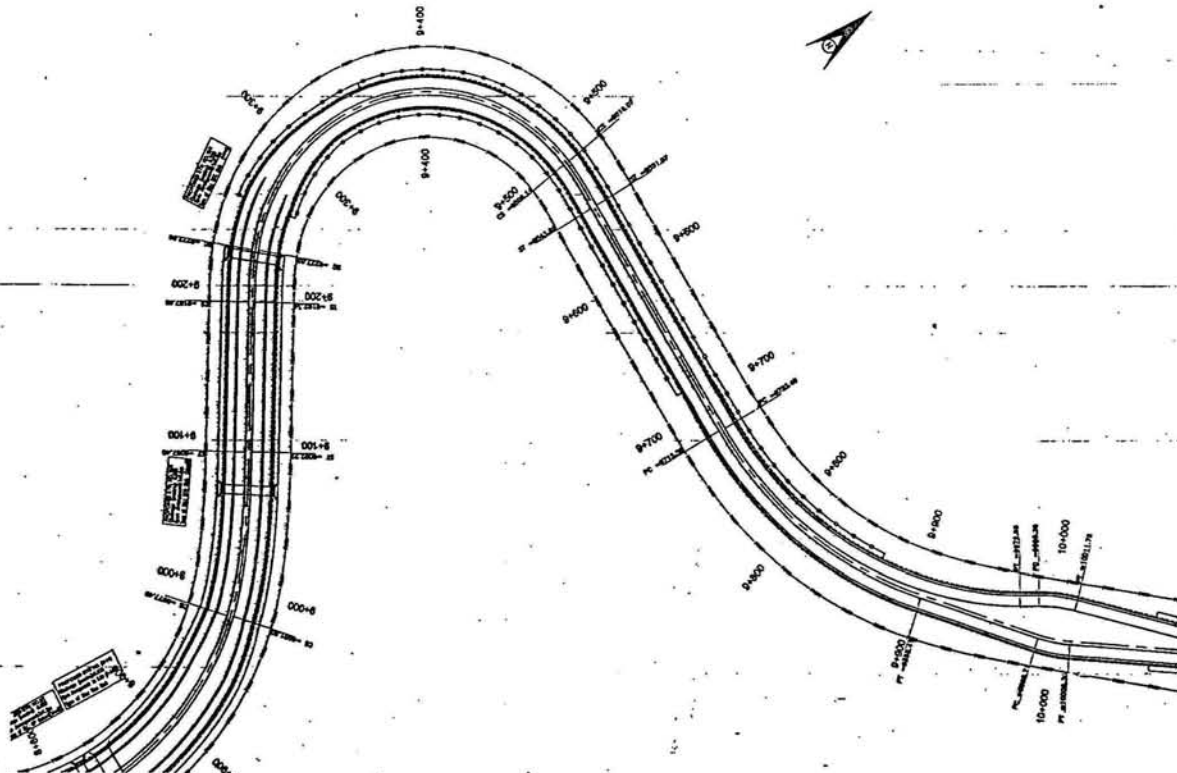
Date: JUNE-2009

Project: Scott Wilson India Private Limited
NEW DELHI - 110044
Tel: (011) 41679340 - 49
Fax: (011) 41679350
E-mail: scw@swi.com



Jammu
147

Udhampur



Level Datum = 452.000

| |
|---|
| Proposed Level along Left Median Edge (m.) |
| Existing Level Below Left Median Edge (m.) |
| Proposed Level along Right Median Edge (m.) |
| Existing Level Below Right Median Edge (m.) |
| Horizontal Alignment Left |
| Horizontal Alignment Right |
| Vertical Alignment Left |
| Vertical Alignment Right |
| LHS Superelevation |
| RHS Superelevation |
| Left Profile Chainage |
| Right Profile Chainage |

| SYMBOL | DESCRIPTION |
|--------|------------------------|
| 1 | Proposed Level |
| 2 | Existing Level |
| 3 | Horizontal Alignment |
| 4 | Vertical Alignment |
| 5 | LHS Superelevation |
| 6 | RHS Superelevation |
| 7 | Left Profile Chainage |
| 8 | Right Profile Chainage |

Project Title
 Consultancy services for preparation of detailed project report for rehabilitation and upgrading of existing 2-lane road to 4-lane divided highway comprising of 1.5km - 1.5km section of NH-147 (Km 9.00 to Km 10.50) including Jammu/Chut Nagpur and Udhampur Bypass Phase - II, Progress of Work - South - western Project, Contract Package C-2015

Client
NATIONAL HIGHWAYS AUTHORITY OF INDIA
 (Ministry of Shipping, Road Transport & Highways)

Drawing Title: PLAN & PROFILE
 Km. 9+000 to 10+000 (UDHAMPUR BYPASS)

Drawing No.: SWV/1019/ROADSPP-60
Scale: HOR:- 1:2000 VERT:- 1:200

Drn: K.L.N. **Dgn:** K.PRASAD **Appd:** SUG.C.D.PUR **Date:** JUNE-2009

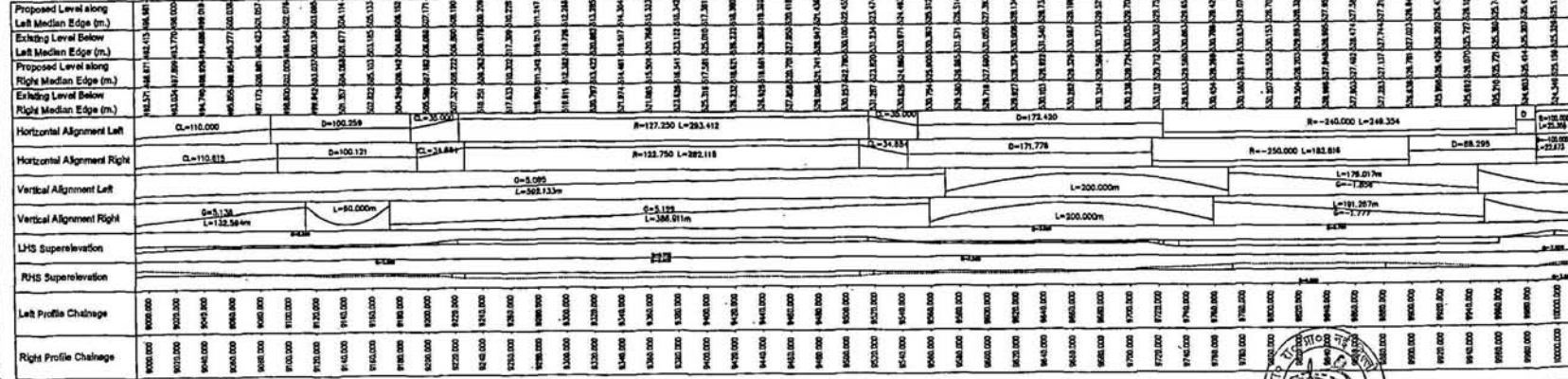
Soil Wilson India Private Limited
 28/4, MOHAN CO-OPERATIVE IND. ESTATE
 WAZIRRA ROAD
 NEW DELHI - 110044
 Tel: (011) 41679340 - 49
 Fax: (011) 41679350
 E-mail: swindco@swind.com



Jammu
14B

Udhampur

Level Datum = 492.000



| Symbol | Description |
|--------|-----------------------|
| --- | Proposed Road |
| --- | Existing Road |
| --- | Proposed Level |
| --- | Existing Level |
| --- | Proposed Right of Way |
| --- | Existing Right of Way |
| --- | Proposed Left of Way |
| --- | Existing Left of Way |
| --- | Proposed Median |
| --- | Existing Median |

Project Title
 Consultancy review for preparation of detailed project report for Rehabilitation and upgrading of existing 3-lane road to 4-lane divided Category road including of Jammu - Udhampur section of NH-14 A (km 0.00 to km 64.00) including Jammu/PP/Jammu and Udhampur by-pass. Phase - III Preparation of Work - Survey - condition Project. Contract Package C-2018

Client
NATIONAL HIGHWAYS AUTHORITY OF INDIA
 (Ministry of Shipping, Road Transport & Highways)

Drawing Title: **PLAN & PROFILE**
 Km. 0+000 to 10+000 (UDHAMPUR BYPASS)

Drawing No.: **SWW1019/ROADS/PP-60**

Scale: --- HQR:-1:2000 VER:-1:200

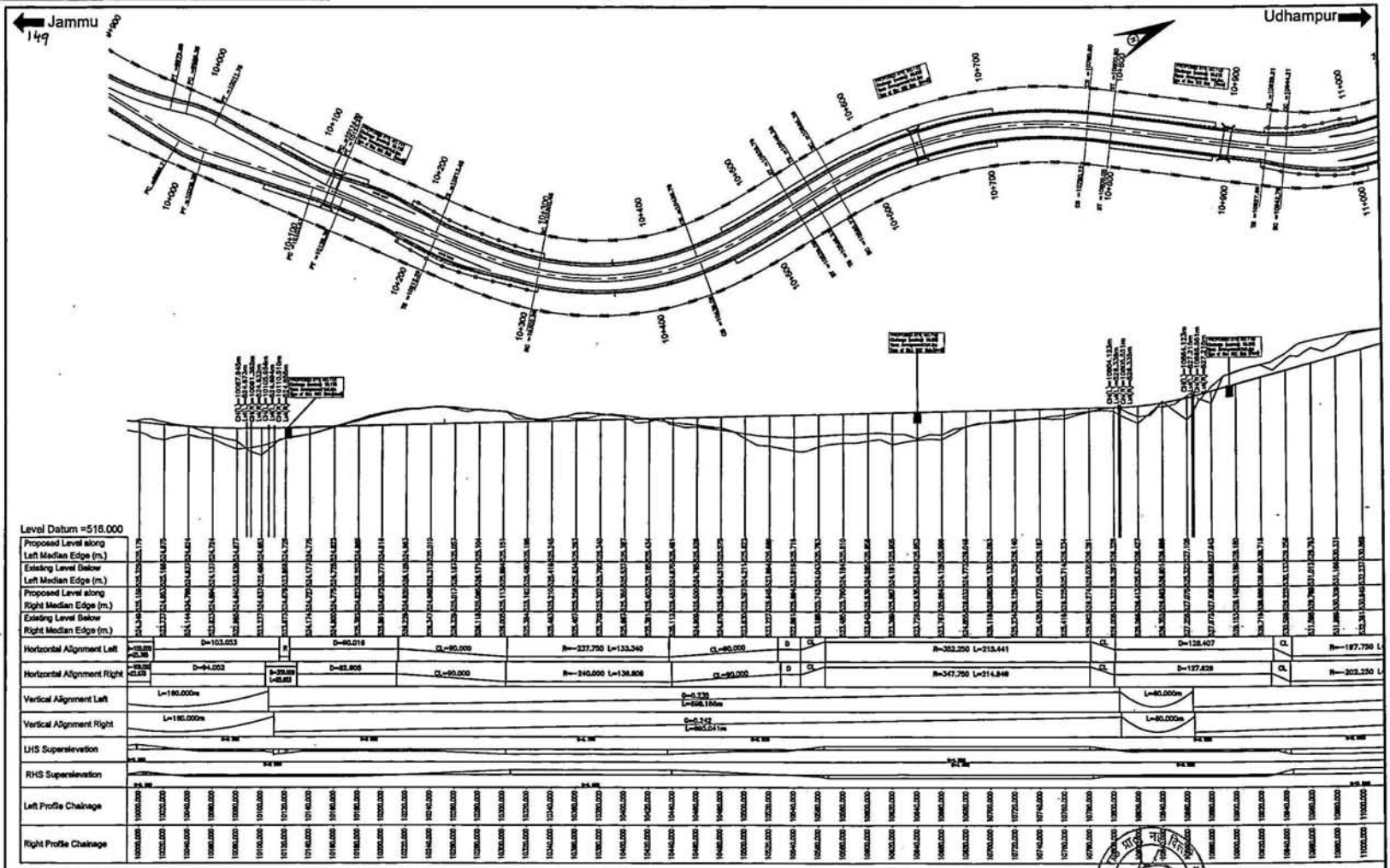
Drawn: **K.L.N**

Checked: **D.K.PRASAD**

Date: **JUNE-2009**

Scott Wilson India Private Limited
 A-72/1, MOHAN CO-OPERATIVE IND. ESTATE
 NEERKA ROAD
 NEERKA COLONY - 110044
 Tel: (011) 41679340 - 49
 Fax: (011) 41679350
 E-mail: swind@scwilson.com





| REVISION | |
|----------|-------------------|
| No. | Description |
| 1 | Issued for tender |
| 2 | As per comments |
| 3 | As per comments |
| 4 | As per comments |
| 5 | As per comments |
| 6 | As per comments |
| 7 | As per comments |
| 8 | As per comments |
| 9 | As per comments |
| 10 | As per comments |

Project Title
 Consultancy services for preparation of detailed project report for the rehabilitation and upgrading of existing 2-lane road to 4-lane divided carriageway and provision of drainage - Udhampur section of NH-149. A

Scale: 1:1000
 Date: 05.06.2008

By: **Phase - III** Preparation of North-South condition Project Contract Package C-0915

Client
NATIONAL HIGHWAYS AUTHORITY OF INDIA
 (Ministry of Shipping, Road Transport & Highways)

Drawing Title: PLAN & PROFILE
 Km. 10+000 to 11+000 (UDHAMPUR BYPASS)

Drawing No.: SW/1019/ROADS/PP-61

Scale: HOR-1:2000 VER-1:200

Drn: K.L.N. **Dgn:** K.PRASAD **Appd:** BRG.C.D.PURI **Date:** JUNE-2008

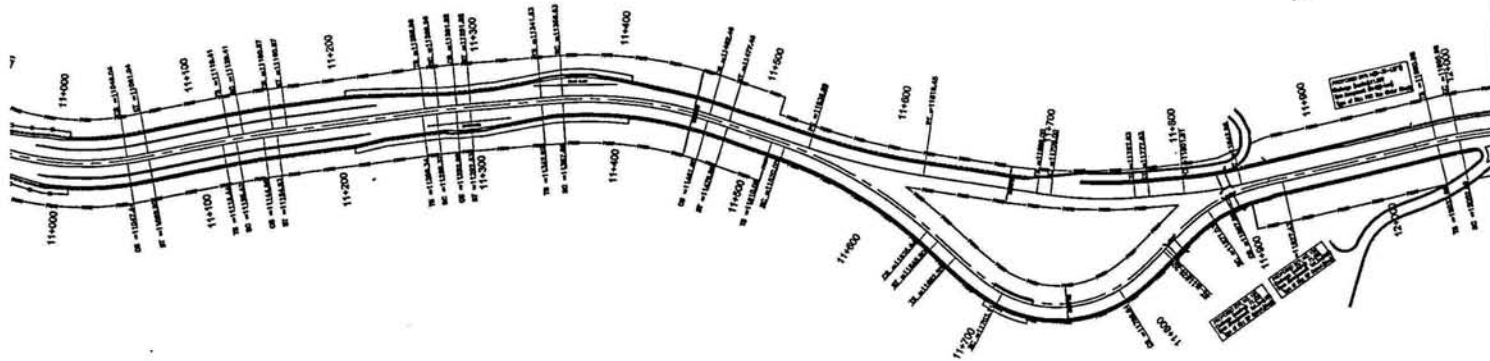
Scott Wilson India Private Limited
 A/27A, MOHAN CO-OPERATIVE IND. ESTATE
 MAYAPUR ROAD
 NEW DELHI - 110044
 Tele: (011) 41679340 - 49
 Fax: (011) 41679350
 E-mail: swinfo@swi.com

N.H.A.

Scott Wilson

Jammu
150

Udhampur



Level Datum = 512.000

| |
|---|
| Proposed Level along Left Median Edge (m.) |
| Existing Level Below Left Median Edge (m.) |
| Proposed Level along Right Median Edge (m.) |
| Existing Level Below Right Median Edge (m.) |
| Horizontal Alignment Left |
| Horizontal Alignment Right |
| Vertical Alignment Left |
| Vertical Alignment Right |
| LHS Superelevation |
| RHS Superelevation |
| Left Profile Chainage |
| Right Profile Chainage |

| For | Remarks |
|--------------|--------------|
| 1. Existing | 1. Existing |
| 2. Proposed | 2. Proposed |
| 3. Existing | 3. Existing |
| 4. Proposed | 4. Proposed |
| 5. Existing | 5. Existing |
| 6. Proposed | 6. Proposed |
| 7. Existing | 7. Existing |
| 8. Proposed | 8. Proposed |
| 9. Existing | 9. Existing |
| 10. Proposed | 10. Proposed |
| 11. Existing | 11. Existing |
| 12. Proposed | 12. Proposed |
| 13. Existing | 13. Existing |
| 14. Proposed | 14. Proposed |
| 15. Existing | 15. Existing |
| 16. Proposed | 16. Proposed |
| 17. Existing | 17. Existing |
| 18. Proposed | 18. Proposed |
| 19. Existing | 19. Existing |
| 20. Proposed | 20. Proposed |

Project Title
 Consultancy services for preparation of detailed project report for rehabilitation and upgrading of existing 2-lane road to 4-lane divided carriageway configuration of Jammu - Udhampur section of NH-1-A from 0+00 to km 64.90 including Jammu, Pooni, Nigrota and Udhampur Bypass Phase - III (Progression of North - South - corridor Project Contract Package C-018)

Notes:
 1. The drawing shall be in accordance with the provisions of the Indian Standards IS: 8000 and IS: 8001 and shall be drawn in accordance with the provisions of the Indian Standards IS: 8000 and IS: 8001.
 2. The drawing shall be drawn in accordance with the provisions of the Indian Standards IS: 8000 and IS: 8001 and shall be drawn in accordance with the provisions of the Indian Standards IS: 8000 and IS: 8001.
 3. The drawing shall be drawn in accordance with the provisions of the Indian Standards IS: 8000 and IS: 8001 and shall be drawn in accordance with the provisions of the Indian Standards IS: 8000 and IS: 8001.

Client
NATIONAL HIGHWAYS AUTHORITY OF INDIA
 (Ministry of Shipping, Road Transport & Highways)

Drawing Title: PLAN & PROFILE
 Km. 11+000 to 12+000 (UDHAMPUR BYPASS)

Drawing No.: SW/1019/ROADS/PP-62

Scale: HOR-1:2000 VER-1:250

Drawn: K.L.N. **Days:** K.PRASAD **Appd:** BRIG.C.D.PURI

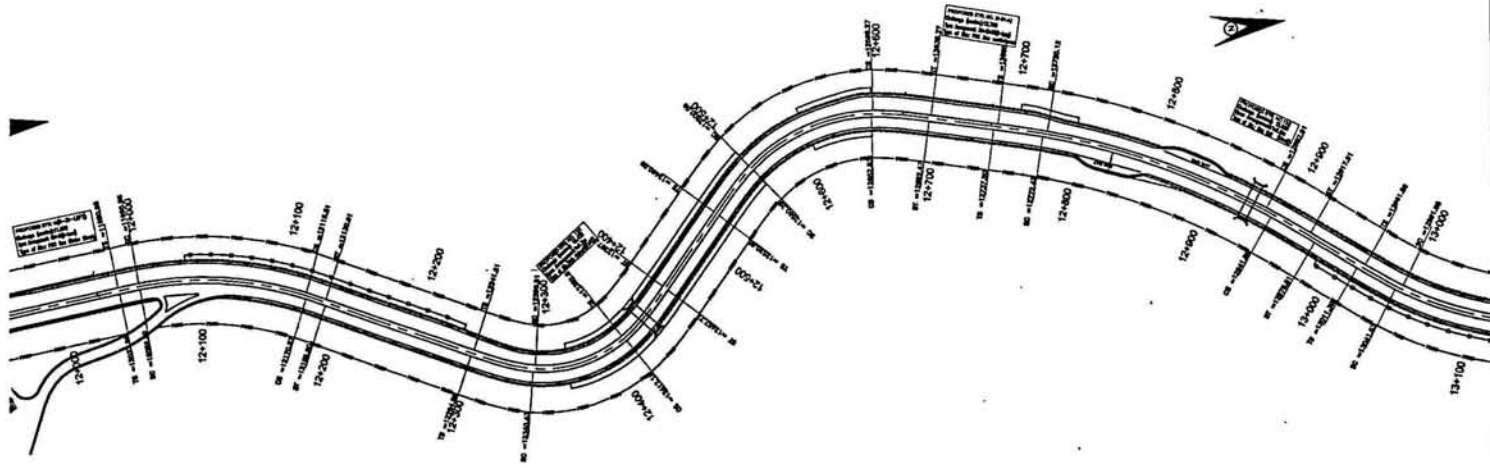
Date: JUNE-2008

Client: Scott Wilson India Private Limited
 NEW DELHI - 110044
 Tel: (011) 41679340 - 49
 Fax: (011) 41679350
 E-mail: scwilson@rediffmail.com



← Jammu
152

Udhampur →



Level Datum = 516.000

| |
|---|
| Proposed Level along Left Median Edge (m.) |
| Existing Level Below Left Median Edge (m.) |
| Proposed Level along Right Median Edge (m.) |
| Existing Level Below Right Median Edge (m.) |
| Horizontal Alignment Left |
| Horizontal Alignment Right |
| Vertical Alignment Left |
| Vertical Alignment Right |
| LHS Superelevation |
| RHS Superelevation |
| Left Profile Chainage |
| Right Profile Chainage |

| REVISIONS | |
|-----------|-------------------------|
| No. | Description |
| 1 | As per approved drawing |
| 2 | As per approved drawing |
| 3 | As per approved drawing |
| 4 | As per approved drawing |
| 5 | As per approved drawing |
| 6 | As per approved drawing |
| 7 | As per approved drawing |
| 8 | As per approved drawing |
| 9 | As per approved drawing |
| 10 | As per approved drawing |

Project Title
 Consultancy services for preparation of detailed project report for Rehabilitation and upgrading of existing 2 lane road to 4 lane divided Contingency configuration of Jammu - Udhampur section of NH-1 A
 Km. 12.00 to km. 13.00 (including Jammu-Udhampur Bypass and Udhampur Bypass Phase - III Programme of North-South corridor Project, Contract Package C-0211)

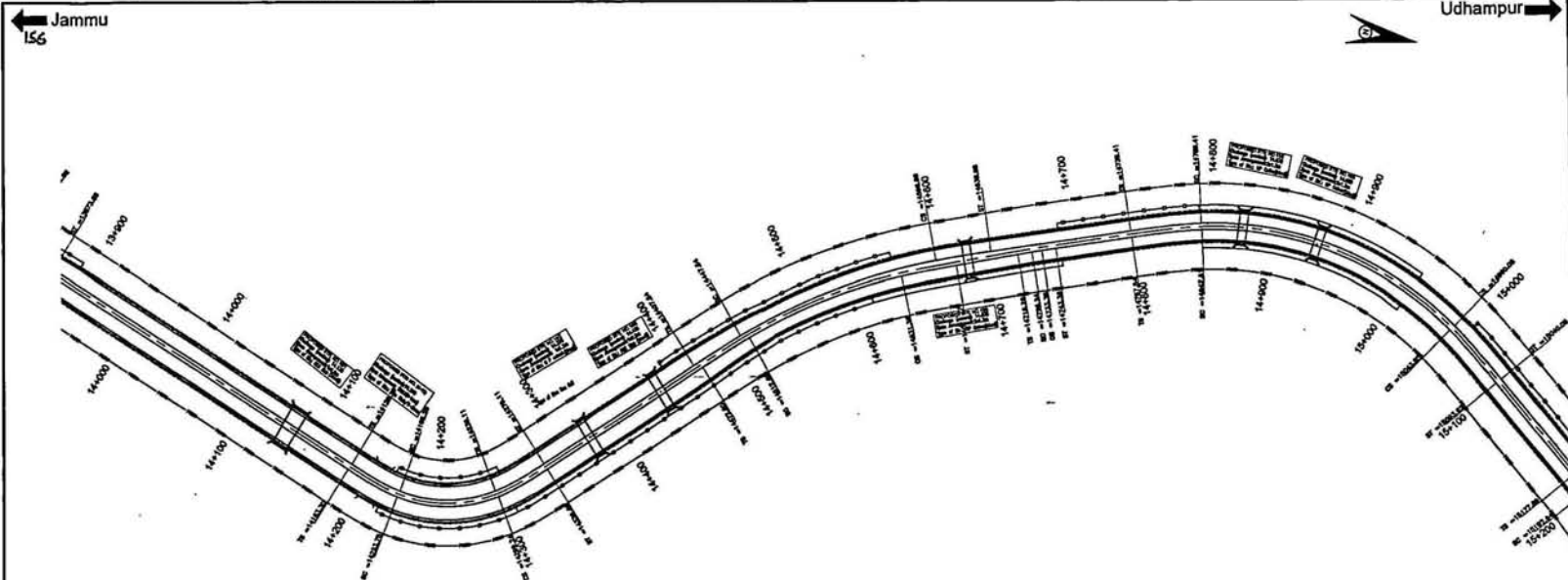
Client
 NATIONAL HIGHWAYS AUTHORITY OF INDIA
 (Ministry of Shipping, Road Transport & Highways)

Client
 NATIONAL HIGHWAYS AUTHORITY OF INDIA
 (Ministry of Shipping, Road Transport & Highways)

Drawing Title: PLAN & PROFILE
 Km. 12+000 to 13+000 (UDHAMPUR BYPASS)
Drawing No.: SW/1019/ROADS/PP-03
Scale: HOR:-1:2000 VER:-1:200
Drn: K.L.N. **Dgn:** K.PRASAD **Appd:** BRIG.C.D.PURI **Date:** JUNE-2009

Scott Wilson India Private Limited
 A-26/4, MOHAN CO-OPERATIVE IND. ESTATE
 MATHERA ROAD
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 E-mail: swindia@rediffmail.com





Level Datum = 538.000

| |
|--|
| Proposed Level along Left Median Edge (m) |
| Existing Level Below Left Median Edge (m) |
| Proposed Level along Right Median Edge (m) |
| Existing Level Below Right Median Edge (m) |
| Horizontal Alignment Left |
| Horizontal Alignment Right |
| Vertical Alignment Left |
| Vertical Alignment Right |
| LHS Superelevation |
| RHS Superelevation |
| Left Profile Chainage |
| Right Profile Chainage |

| For | Approved |
|-----------------------------------|----------|
| 1. Subdiv. of company | |
| 2. Proposed land along Left Side | |
| 3. Proposed land along Right Side | |
| 4. Existing land along Left Side | |
| 5. Existing land along Right Side | |

Project Title
 Consultancy services for preparation of detailed project report for Rehabilitation and upgrading of existing 2-lane road to 4-lane divided carriageway near junction of Jammu - Udhampur section of NH-14 A (Km 0.00 to Km 66.00) including Jammu(Ur) Program and Udhampur Bypass Phase - III Programme of North - South - corridor Project. Contract Package C-0218

Client
NATIONAL HIGHWAYS AUTHORITY OF INDIA
 (Ministry of Shipping, Road Transport & Highways)

Drawing Title: PLAN & PROFILE
 Km. 14+000 to 15+000 (UDHAMPUR BYPASS)

Drawing No.: SW/1019/ROADS/PP-85

Scale: HOR:-1:2000 VER:-1:200

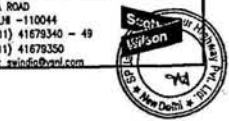
Date: JUNE-2009

Appd: ERG.C.D.PURI

Drawn: K.L.N
Dgn: K.PRASAD

Client: South Wilson India Private Limited
 #22/A, MOHAN CO-OPERATIVE IND. ESTATE
 MATURA ROAD
 NEW DELHI - 110044
 Tel: (011) 41678340 - 49
 Fax: (011) 41678350
 E-mail: swinfo@swindia.com

1. The drawings shall be in D & 42 and shall be in accordance with the provisions of the Indian Standards IS: 8753 and IS: 8754. The drawings shall be in accordance with the provisions of the Indian Standards IS: 8753 and IS: 8754. The drawings shall be in accordance with the provisions of the Indian Standards IS: 8753 and IS: 8754.

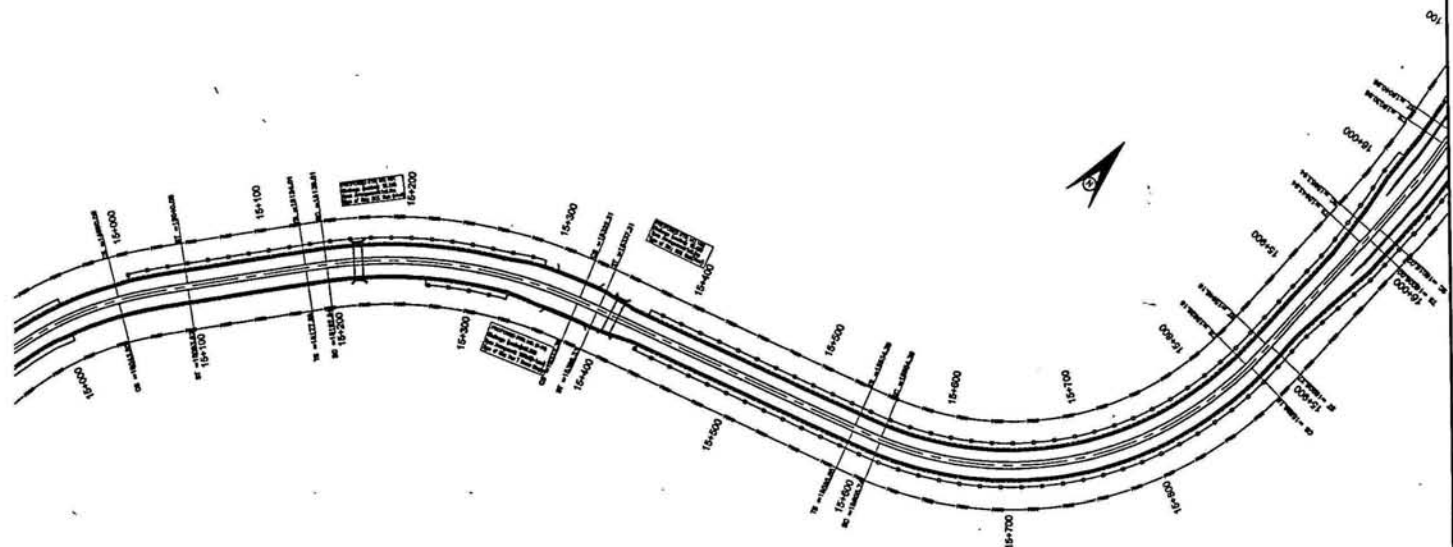


Udhampur

Jammu 156

← Jammu
ISB

Udhampur →



Level Datum = 570.000

| |
|----------------------------|
| Proposed Level along |
| Left Median Edge (m.) |
| Existing Level Below |
| Left Median Edge (m.) |
| Proposed Level along |
| Right Median Edge (m.) |
| Existing Level Below |
| Right Median Edge (m.) |
| Horizontal Alignment Left |
| Horizontal Alignment Right |
| Vertical Alignment Left |
| Vertical Alignment Right |
| LHS Superelevation |
| RHS Superelevation |
| Left Profile Chainage |
| Right Profile Chainage |

| Rev. | Description | By | Check | Date |
|------|----------------|----|-------|------|
| 1 | As per drawing | | | |
| 2 | As per drawing | | | |
| 3 | As per drawing | | | |
| 4 | As per drawing | | | |
| 5 | As per drawing | | | |
| 6 | As per drawing | | | |
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| 19 | As per drawing | | | |
| 20 | As per drawing | | | |
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| 37 | As per drawing | | | |
| 38 | As per drawing | | | |
| 39 | As per drawing | | | |
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| 91 | As per drawing | | | |
| 92 | As per drawing | | | |
| 93 | As per drawing | | | |
| 94 | As per drawing | | | |
| 95 | As per drawing | | | |
| 96 | As per drawing | | | |
| 97 | As per drawing | | | |
| 98 | As per drawing | | | |
| 99 | As per drawing | | | |
| 100 | As per drawing | | | |

Project Title
 Consultancy services for preparation of detailed project report for
 Rehabilitation and upgrading of existing 3-lane road to 4-lane divided
 Contingency mobilization of Jammu - Udhampur section of NH-1A
 (km 0.00 to km 64.30) including Jammu(Ur) Bypass and Udhampur
 Bypass Phase - II Progression of North - South - corridor Project
 Contract Package C-0213

Client
 NATIONAL HIGHWAYS
 AUTHORITY OF INDIA
 (Ministry of Shipping, Road Transport & Highways)

Drawing Title: PLAN & PROFILE
 Km. 15+000 to 16+000 (UDHAMPUR BYPASS)

Drawing No.: SW/1015/ROADS/PP-68

Scale: HOR:-1:2000 VER:-1:200

Drawn: K.L.N **Design:** K.PRASAD **Appr:** BRG.C.D.PURI

Date: JUNE-2009

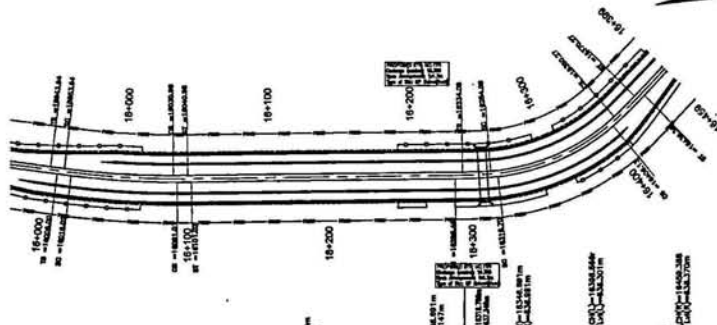
Sheet: 1 of 2

Client: Scott Wilson India Private Limited
 A-28/A, MOHAN CO-OPERATIVE IND. ESTATE
 NEW DELHI - 110044
 Tel: (011) 41679340 - 49
 Fax: (011) 41679350
 E-mail: info@swi.com

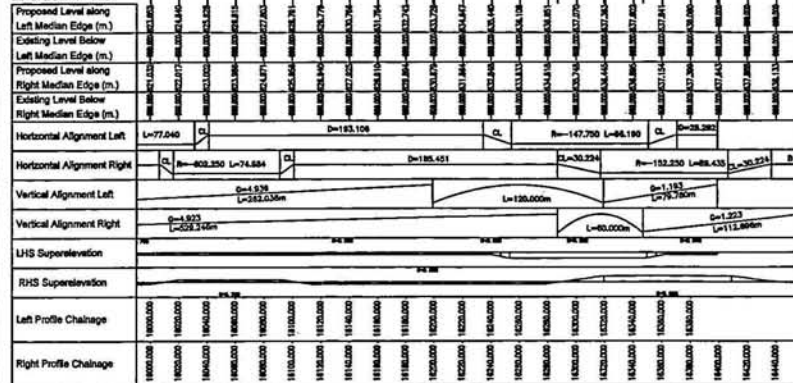


Jammu
160

Udhampur



Level Datum = 620



| Rev | Description | Date |
|-----|-------------|------|
| 1 | As per | |
| 2 | As per | |
| 3 | As per | |
| 4 | As per | |
| 5 | As per | |
| 6 | As per | |
| 7 | As per | |
| 8 | As per | |
| 9 | As per | |
| 10 | As per | |

Project Title
 Consultancy services for preparation of detailed project report for rehabilitation and widening of existing 2 lane road to 4 lane divided carriageway configuration of Jammu - Udhampur section of NH-1 A
 (Km 0.00 to Km 16.00) including Jammu-Udhampur and Udhampur Bypass Phase - III Programme of North - South corridor Project, Contract Package C-0271

Client
NATIONAL HIGHWAYS AUTHORITY OF INDIA
 (Ministry of Shipping, Road Transport & Highways)

Drawing Title: PLAN & PROFILE
 Km. 16+000 to 16+460 (UDHAMPUR BYPASS)

Drawing No.: SW/1018/ROADS/PP-67 **Sheet:** _____

Scale: --- **HOR:** 1:2000 **VER:** 1:200

Date: JUNE-2009

Appd: _____ **Date:** _____

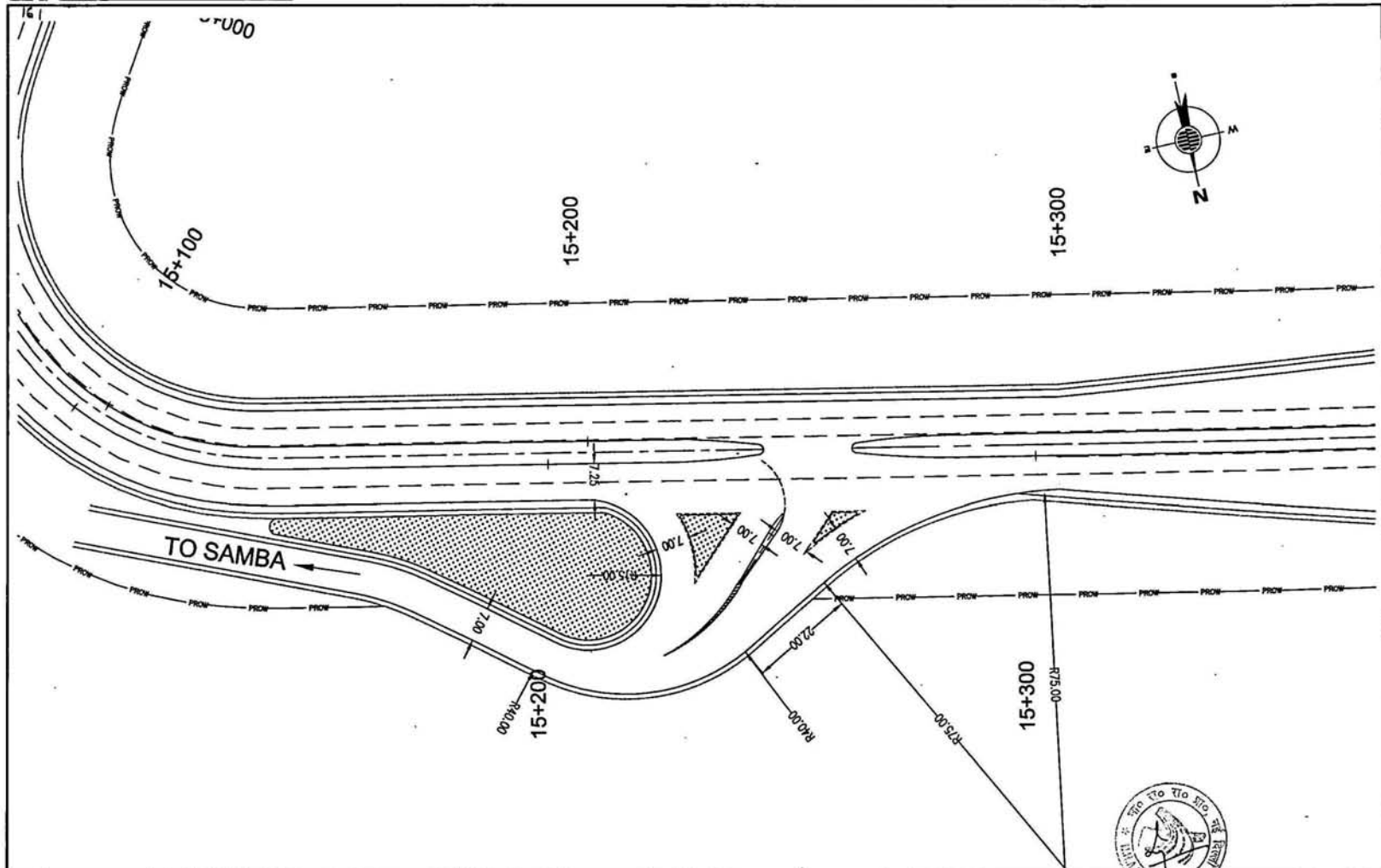
Drn: K.L.N. **Pcn:** K.PRASAD **Appd:** B.R.G.C.D.PURI

Client
NATIONAL HIGHWAYS AUTHORITY OF INDIA
 (Ministry of Shipping, Road Transport & Highways)



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 Fax: (011) 41679350
 E-mail: swinfo@swi.com





| Revision Details | By | Chk | Date | Staff |
|------------------|----|-----|------|-------|
| | | | | |
| | | | | |
| | | | | |

Project Title
 Consultancy services for preparation of detailed project report for
 rehabilitation and upgrading of existing 2-lane road to 4-lane divided
 carriageway configuration of Jammu - Udhampur section of NH-1 A.
 (km 6.00 to km 66.00) including Jammu (Pur) Bypass and Udhampur
 Bypass Phase - III Programme of North-South corridor Project,
 Contract Package C-2018

Client
**NATIONAL HIGHWAYS
 AUTHORITY OF INDIA**
 (Ministry of Shipping, Road Transport & Highways)

Drawing Title: JUNCTION IMPROVEMENT
 AT CH- 15+250 (JAMMU BYPASS)

Drawing No.: SW/1018/ROADS/JUN-01

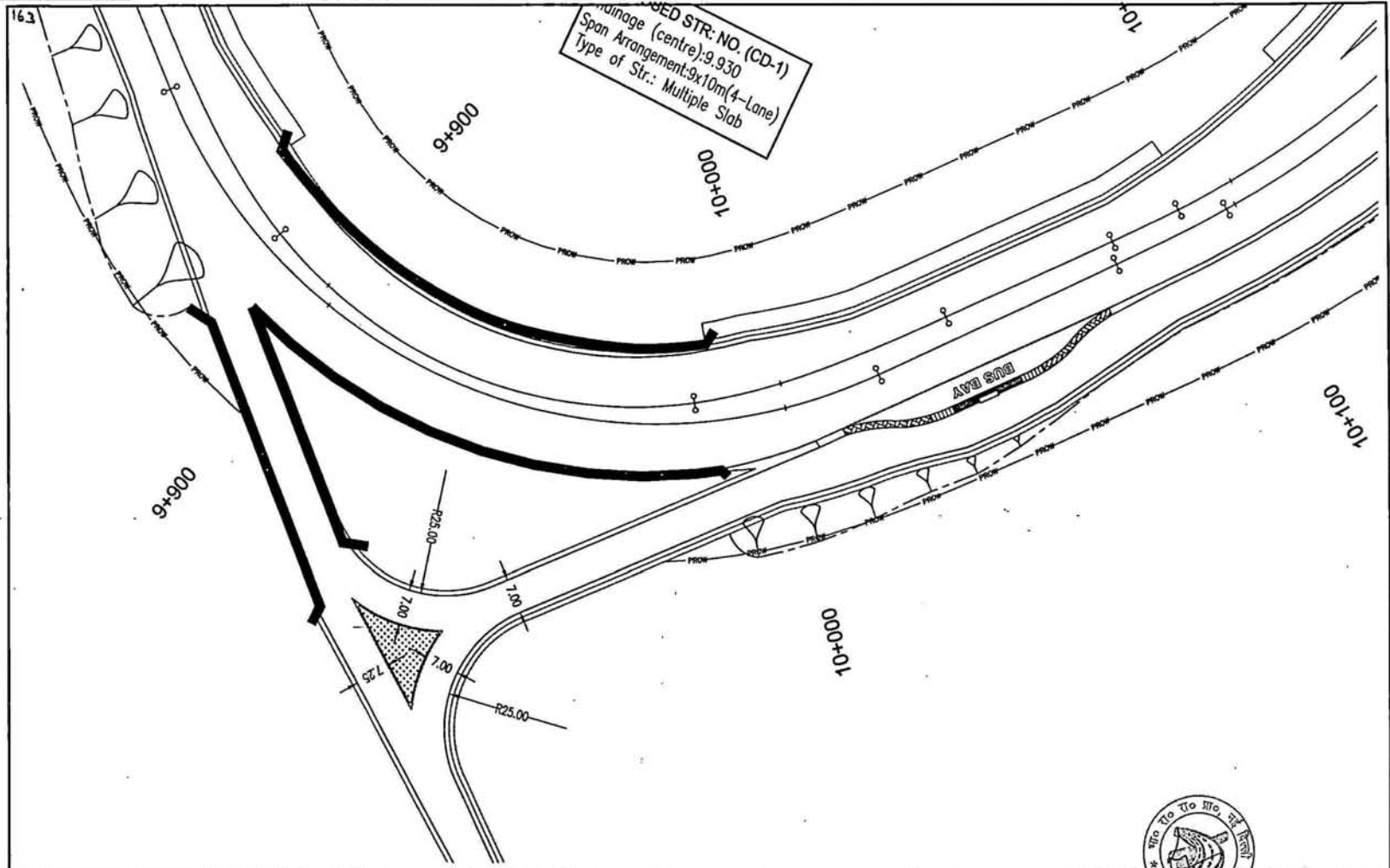
Scale: 1:500

Drn: K.L.N **Depn:** K.PRASAD **Appd:** BRG.C.D.PURI **Date:** JUNE-2009

Sheet : _____

Client: Scott Wilson India Private Limited
 A-26/4, MOHAN CO-OPERATIVE IND. ESTATE
 MATHURA ROAD
 NEW DELHI - 110044
 Tele: (011) 41679340 - 49
 Fax: (011) 41679350
 E-mail: swinfo@swi.com





DESIGNED STR. NO. (CD-1)
 Drainage (centre): 9.930
 Span Arrangement: 9x10m (4-Lane)
 Type of Str.: Multiple Slab

| Revision Details | By | CHK | Date | Sluff |
|------------------|----|-----|------|-------|
| | | | | |
| | | | | |
| | | | | |

Project Title
 Consultancy services for preparation of detailed project report for
 Rehabilitation and upgrading of existing 3-lane road to 4-lane divided
 carriageway configuration of Jammu - Lillapagar section of NH-1 A
 (On 8.00 to Km 14.00) including Junction (J) at Lillapagar and Lillapagar
 Bypass Phase - III Programme of North-South corridor Project,
 Contract Package C-02/14

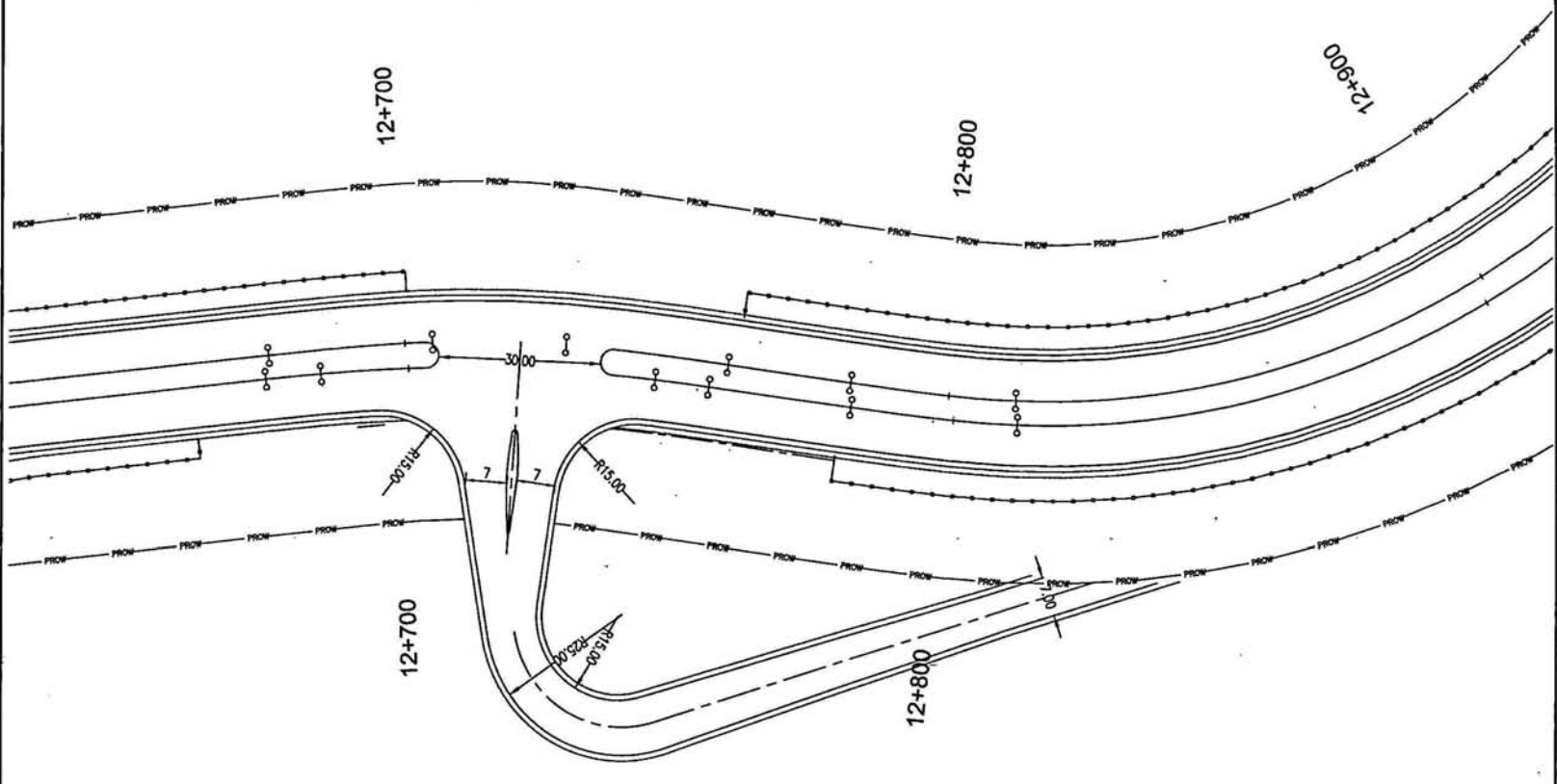
Client
**NATIONAL HIGHWAYS
 AUTHORITY OF INDIA**
 (Ministry of Shipping, Road Transport & Highways)

Drawing Title: JUNCTION IMPROVEMENT
AT CH- 21+900
Drawing No.: SW/1018/ROADS/JUN-03
Scale: 1:500
Des.: K.L.N
Dgn.: K.PRASAD
Appd: BRIG. C.D.PURM
Date: JUNE-2009

Sheet: 1
Scale: 1:500
Des.: K.L.N
Dgn.: K.PRASAD
Appd: BRIG. C.D.PURM
Date: JUNE-2009

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 E-mail: swindco@rediffmail.com





| Revision | By | Date | Desc |
|----------|----|------|------|
| | | | |
| | | | |
| | | | |
| | | | |

Project Title
 Consultancy services for preparation of detailed project report for
 Rehabilitation and upgrading of existing 3-lane road to 4-lane divided
 carriageway configuration of Jammu - Udhampur section of NH-1A
 (km 0.00 to km 66.00) including Jammu/Var) Nagarn and Udhampur
 Bypass Phase - III Programme of North-South corridor Project,
 Contract Package C-0918

Scale
 1:500

Client
 NATIONAL HIGHWAYS
 AUTHORITY OF INDIA
 (Ministry of Shipping, Road Transport & Highways)

Drawing Title: JUNCTION IMPROVEMENT
 AT CH- 12+700

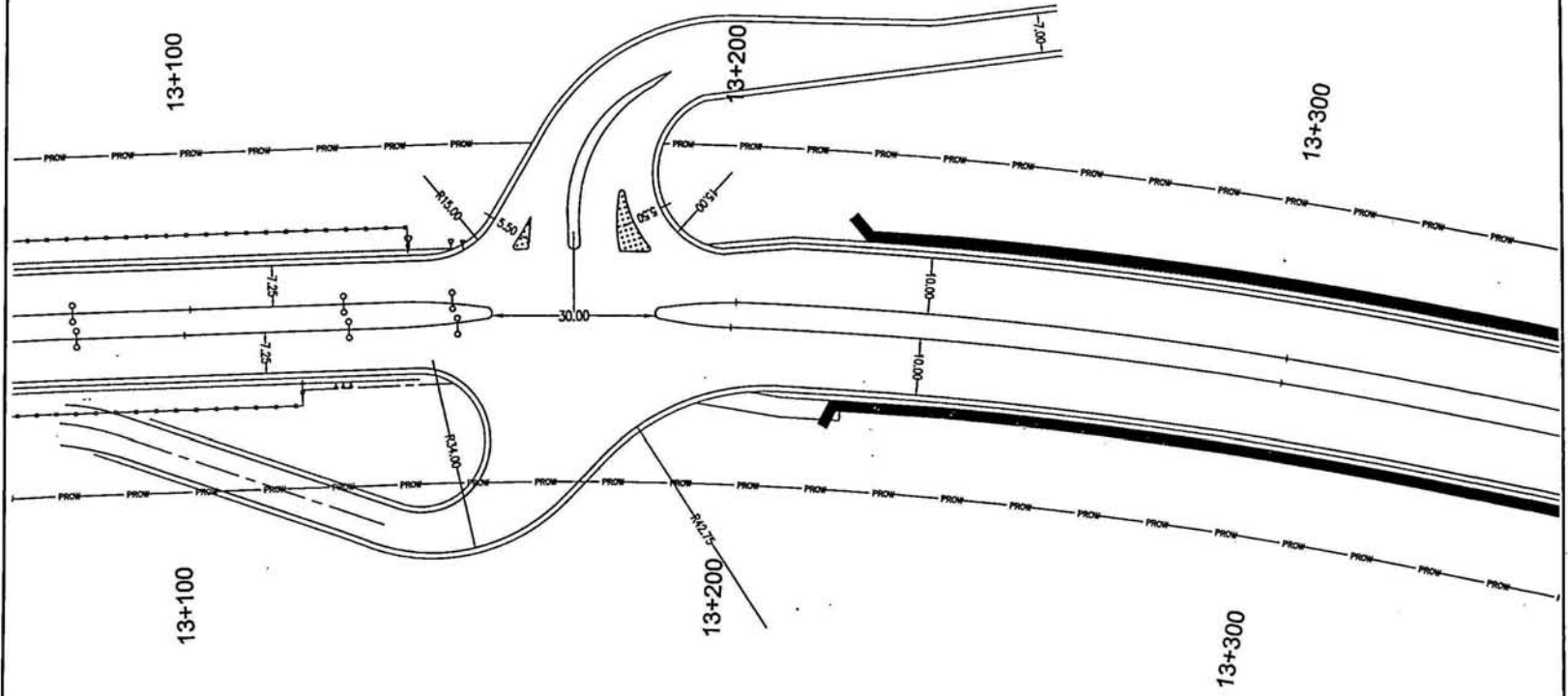
Drawing No.: SWH/1019/ROADS/JUN-04

Scale: 1:500

Drawn: K.L.N
Dgn.: K.PRASAD
Appd.: BRIG. C.D. PUNJ
Date: JUNE-2009

Client
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 ANJAN CO-OPERATIVE IND. ESTATE
 MATARWA ROAD
 NEW DELHI - 110044
 Tele: (011) 41679340 - 49
 Fax: (011) 41679350
 E-mail: wipr@indiaprivateltd.com





| Revision Details | By | Date | Scale |
|------------------|----|------|-------|
| | | | |
| | | | |
| | | | |
| | | | |

Project Title
 Consultancy services for preparation of detailed project report for rehabilitation and upgrading of existing 3-lane road to 4-lane divided carriageway and provision of Jamma - Dhamra section of NH-1 A (km 0.00 to km 66.00) including Jamma/Pad/Megala and Uthmaniyer Phase - III Programme of North-South corridor Project. Contract Package C-0974

Scale
 1:500

Client
NATIONAL HIGHWAYS AUTHORITY OF INDIA
 (Ministry of Shipping, Road Transport & Highways)

Drawing Title: JUNCTION IMPROVEMENT AT CH- 13+160
Drawing No.: SW/1019/ROADS/JUN- 05
Scale: 1:500
Drn: K.L.N
Dgn: K.PRASAD
Appd: BRIG.C.D.PURI
Date: JUNE-2009

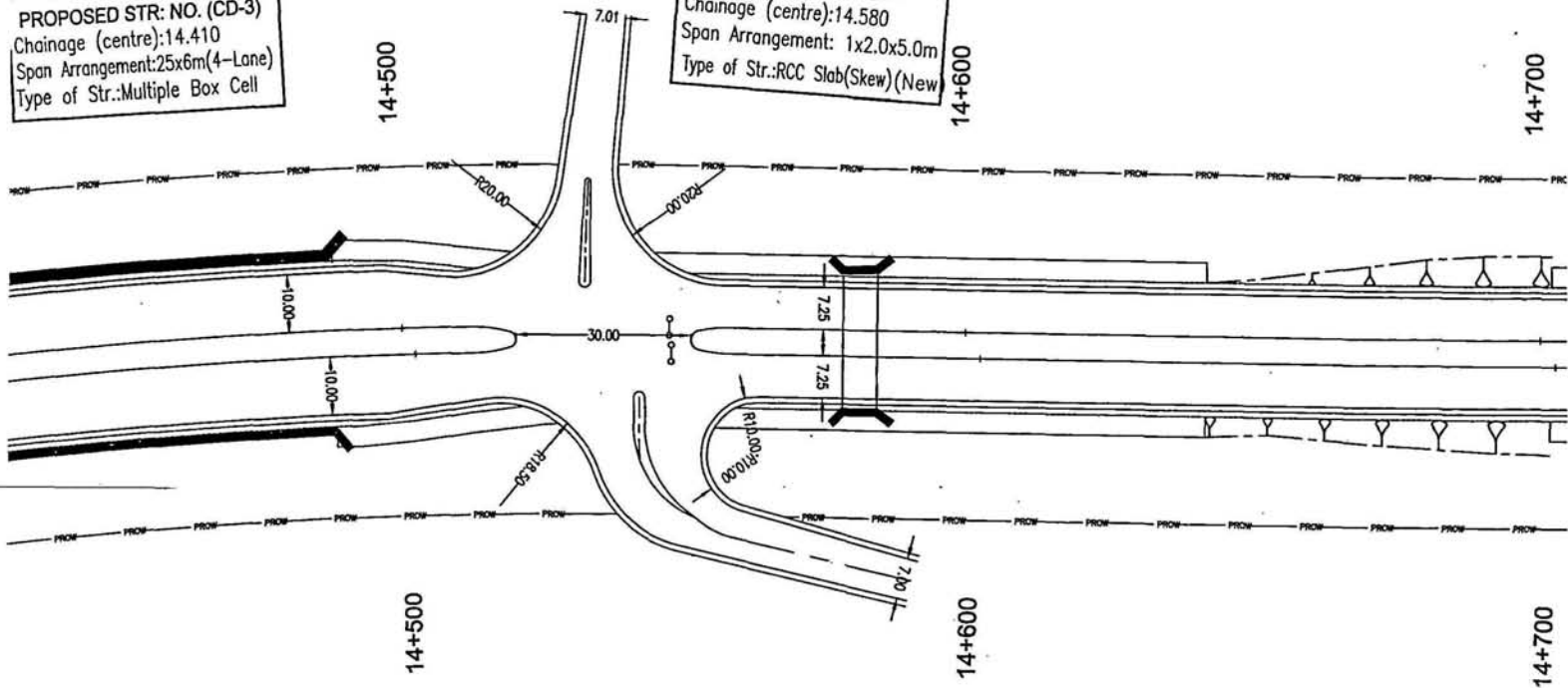


Wilson India Private Limited
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 E-mail: wain@waind.com



PROPOSED STR: NO. (CD-3)
Chainage (centre):14.410
Span Arrangement:25x6m(4-Lane)
Type of Str.:Multiple Box Cell

PROPOSED STR: NO.15/1
Chainage (centre):14.580
Span Arrangement: 1x2.0x5.0m
Type of Str.:RCC Slab(Skew)(New)



| | | | | |
|------------------|----|-----|------|--------|
| Revision Details | By | CHK | Date | Stuffs |
| | | | | |
| | | | | |
| | | | | |

Project Title
Consultancy services for preparation of detailed project report for Rehabilitation and upgrading of existing 2-lane road to 4-lane divided Carriageway configuration of Jammu - Udhampur section of NH-1 A (Up to 6.00 to km 66.00) including Jammu/PO Nagrota and Udhampur Bypass Phase - III Progression of North-South corridor Project, Contract Package C-0211

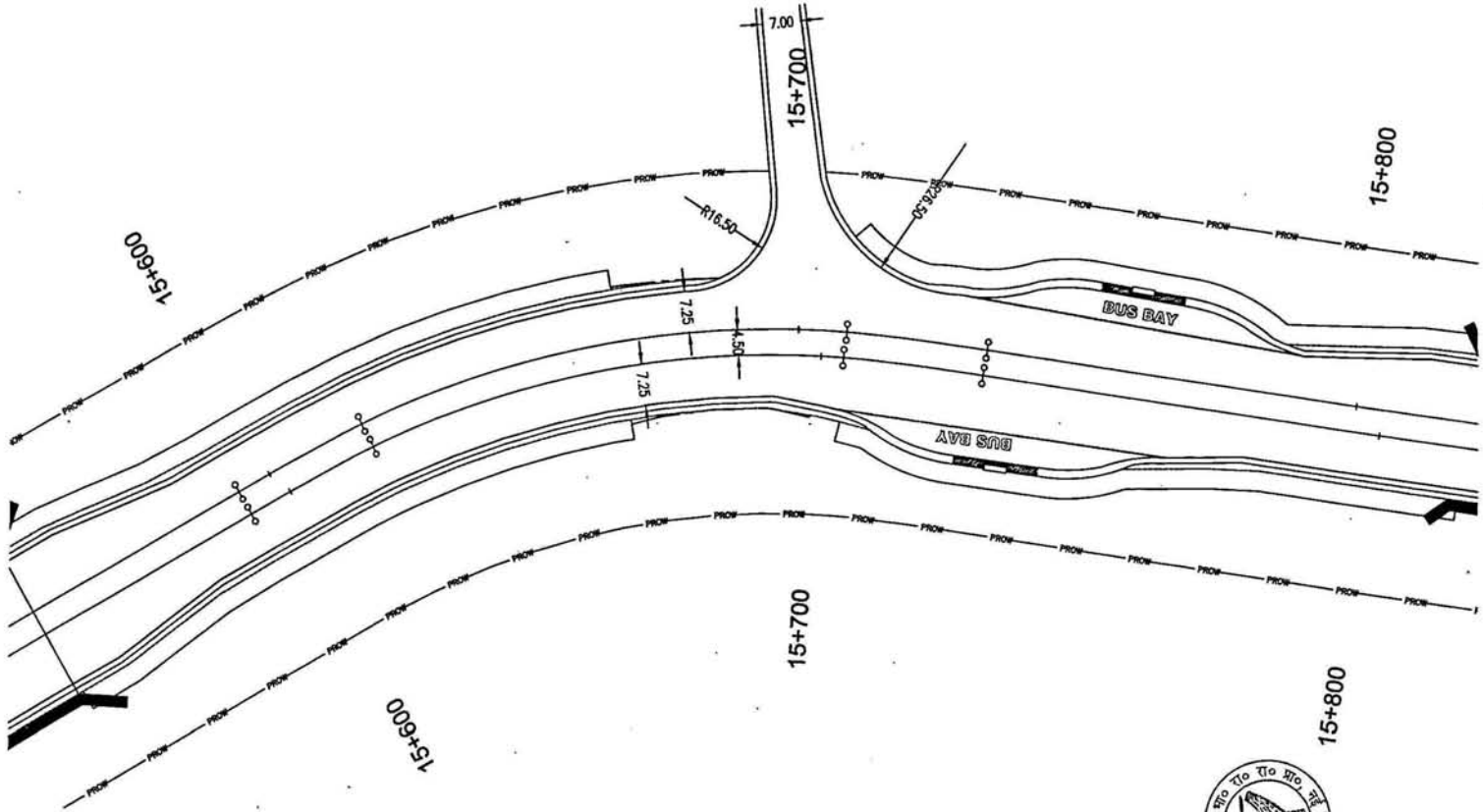
Scale
Original Scale: A2
Drawing Scale: As indicated in the drawing
Author: [Signature]
Checked: [Signature]
Approved: [Signature]

Client
NATIONAL HIGHWAYS AUTHORITY OF INDIA
(Ministry of Shipping, Road Transport & Highways)

Drawing Title: JUNCTION IMPROVEMENT AT CH- 14+530
Drawing No.: SW/1019/ROADS/JUN- 06
Scale: 1:500
Dn: K.L.N
Dgn: K.PRASAD
Appd: BRG.C.D.PURH
Date: JUNE-2009

Scott Wilson India Private Limited
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| | | | | |
|------------------|----|-----|------|--------|
| Revision Details | By | Chk | Date | Suffix |
| | | | | |
| | | | | |
| | | | | |

Project Title
 Consultancy services for preparation of detailed project report for
 Rehabilitation and upgrading of existing 3-lane road to 4-lane divided
 carriageway and provision of 2-lane + 1-lane section of 205-1 A
 (km 0.00 to km 66.00) including Jambhoni (Pur) Bypass and (M) Bypass
 Bypass Phase - III Programme of North-South corridor Project,
 Contract Package C-10218

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Original Scale: 1:2
 Plotting Scale: 1:200
 Plot Size: 300x450

Client
NATIONAL HIGHWAYS
AUTHORITY OF INDIA
 (Ministry of Shipping, Road Transport & Highways)

Drawing Title: JUNCTION IMPROVEMENT
 AT CH- 15+710

Drawing No.: SW/1019/ROADS/JUN- 07

Scale: 1:1500

Date: K.L.N

Dgn.: K.PRASAD

Appd: 0909.C.D.PURM

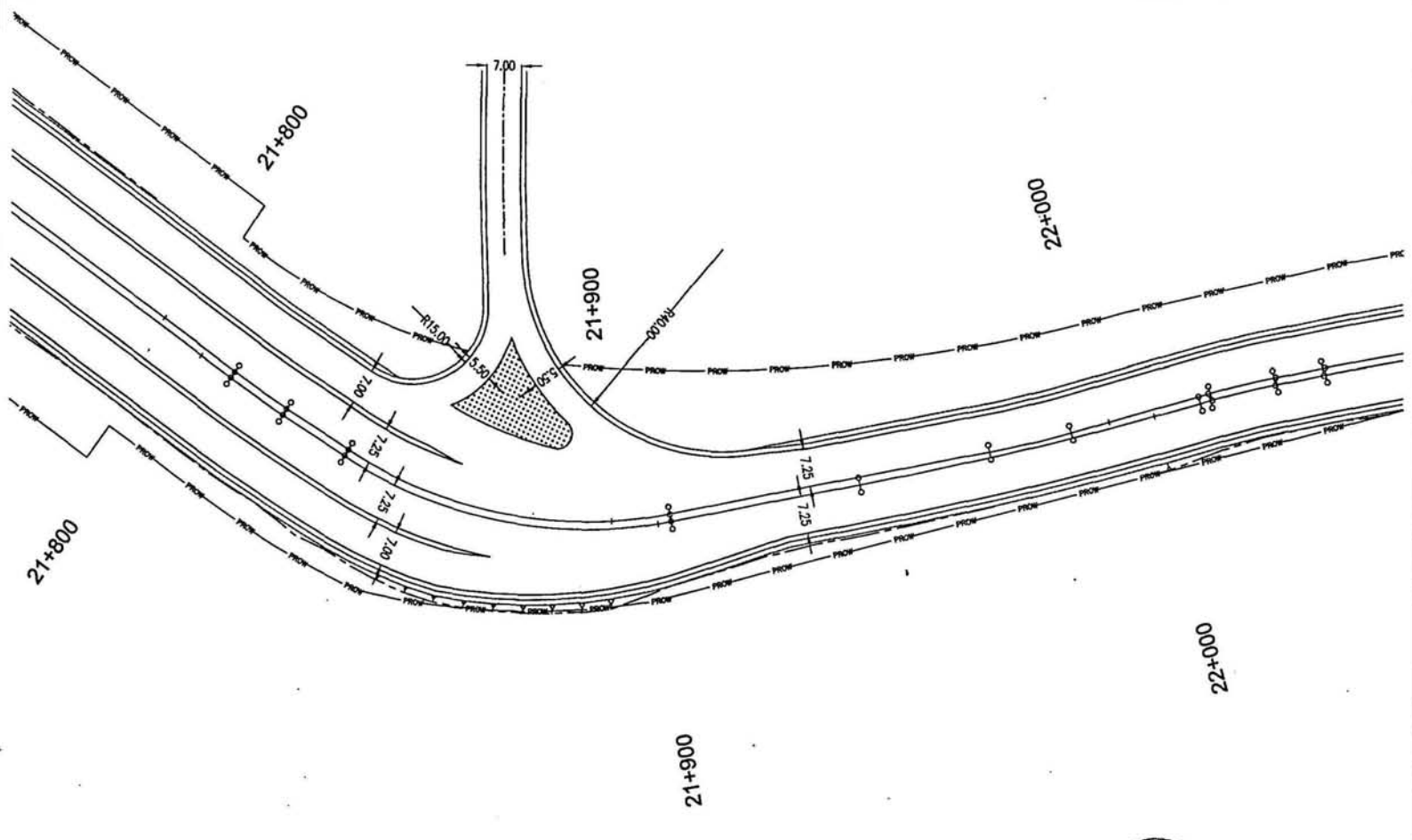
Date: JUNE-2009



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 E-mail: swni@scw.com



16B



| Revision Details | By | Date | Suffix |
|------------------|----|------|--------|
| | | | |
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| | | | |

Project Title
 Consultancy services for preparation of detailed project report for Rehabilitation and upgrading of existing 2-lane road to 4-lane divided carriageway configuration of Jaunpur - Lakhimpur section of NH-11 A.
 (km 6.58 to km 66.00) including Jaunpur (700) Nagrota and Lakhimpur
 System Plan - 02 Programme of North-South corridor Project, Contract Package C-02/11

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 Publication Part: Professional Drawing
 Plot Area:

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Drawing Title: **JUNCTION IMPROVEMENT AT CH- 29+200**
 Drawing No: **SW/1019/ROADS/JUN-08** Sheet:

Scale: 1:1500

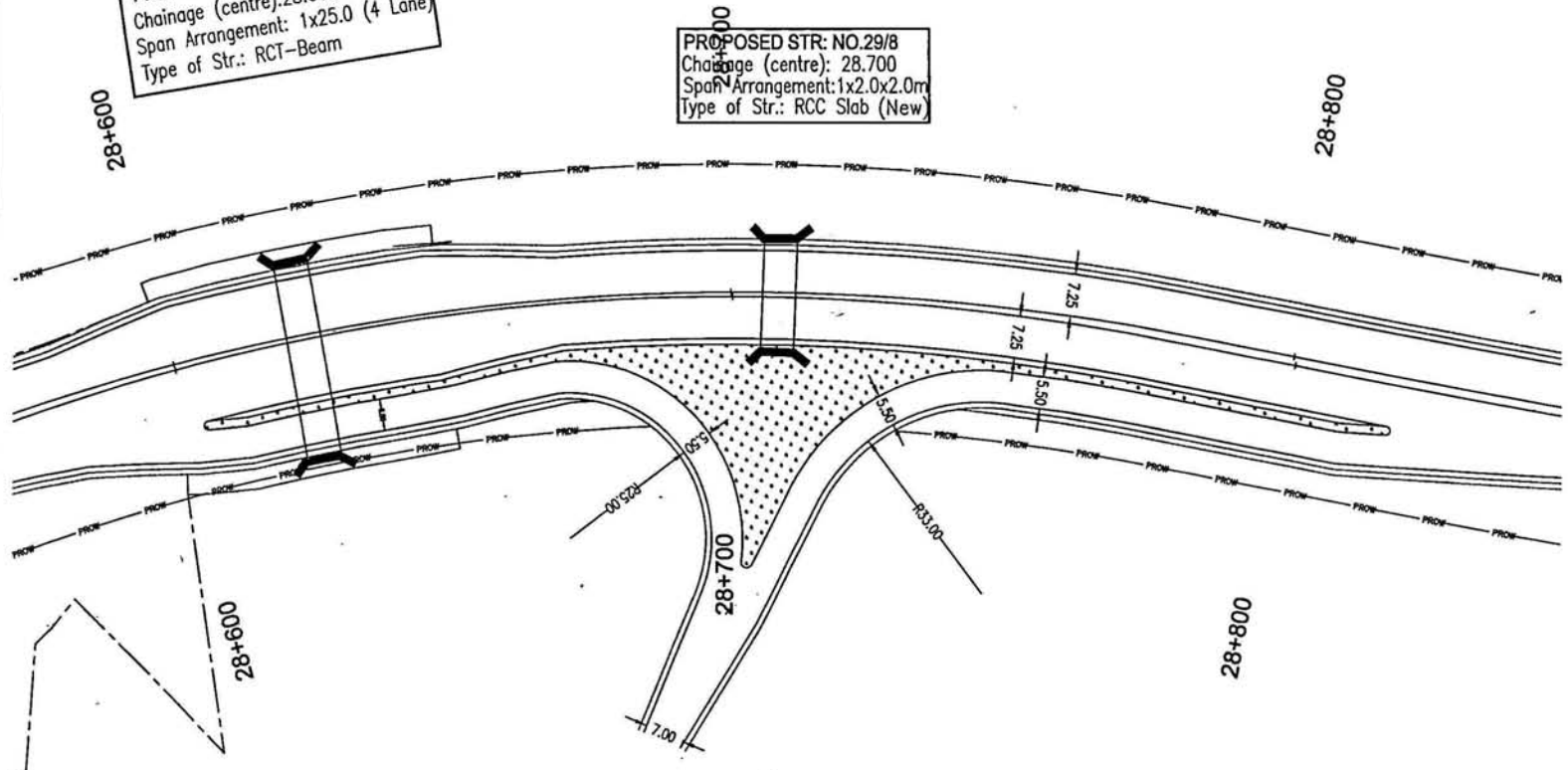
Des: **K.L.N** Dgn: **K.PRASAD** Appd: **(Engg. C.D. Puri)** Date: **JUNE-2009**

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 4/26/4, MOHAN CO-OPERATIVE IND. ESTATE
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 E-mail: swindia@swindia.com



PROPOSED STR. NO. (V-7A)
 Chainage (centre): 28.613
 Span Arrangement: 1x25.0 (4 Lane)
 Type of Str.: RCT-Beam

PROPOSED STR. NO.29/8
 Chainage (centre): 28.700
 Span Arrangement: 1x2.0x2.0m
 Type of Str.: RCC Slab (New)



| Revision | By | Date | Reason |
|----------|----|------|--------|
| | | | |
| | | | |
| | | | |

Project Title
 Consultancy services for preparation of detailed project report for Rehabilitation and upgrading of existing 2-lane road to 4-lane divided carriageway configuration of Jaunes - Lalmogur section of NH-1 A (km 0.00 to km 66.00) including Jaunes/Var/ Nagpur and Lalmogur Bypass Phase - III Programme of North - South - corridor Project, Contract Package C-08/18

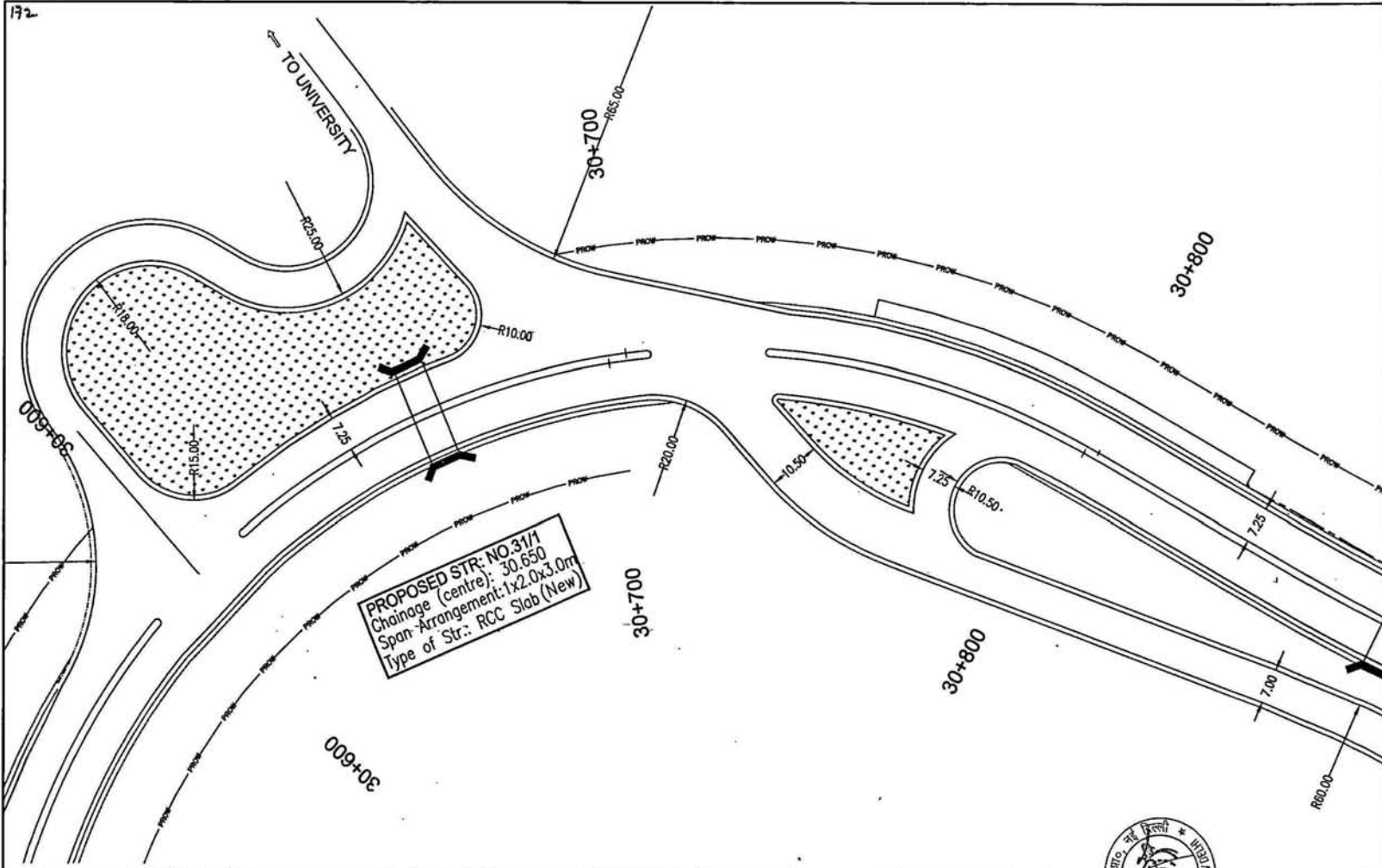
Original Scale
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Plot Scale
 1:1000
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Client
NATIONAL HIGHWAYS AUTHORITY OF INDIA
 (Ministry of Shipping, Road Transport & Highways)

Drawing Title: JUNCTION IMPROVEMENT AT CH- 28+700
Drawing No.: SW/1019/ROADS/JUN- 09
Scale: 1:1500
Drn: K.L.N
Dgn: K.PRASAD
Appd: ENG.C.D.PURM
Date: JUNE-2009

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PROPOSED STR. NO. 31/1
 Chainage (centre): 30.650
 Span Arrangement: 1x2.0x3.0m
 Type of Str.: RCC Slab (New)

| Revision Details | By | CHK | Date | Stuffs |
|------------------|----|-----|------|--------|
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Project Title
 Consultancy services for preparation of detailed project report for rehabilitation and upgrading of existing 2-lane road to 4-lane divided highway consisting of 2 lanes in 1 direction and 2 lanes in 1 direction. (2x2 lanes) including 1.5km of 2-lane road to be upgraded to 4-lane road.

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Original Scale
 1:1000

Plot Scale
 1:1000

Client
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Drawing Title: JUNCTION IMPROVEMENT AT CH- 30+580 & 30+830

Drawing No.: SWW/1019/ROADS/JUN- 12

Scale :- 1:1000

Date: K.L.N

Dgn.: K.PRASAD

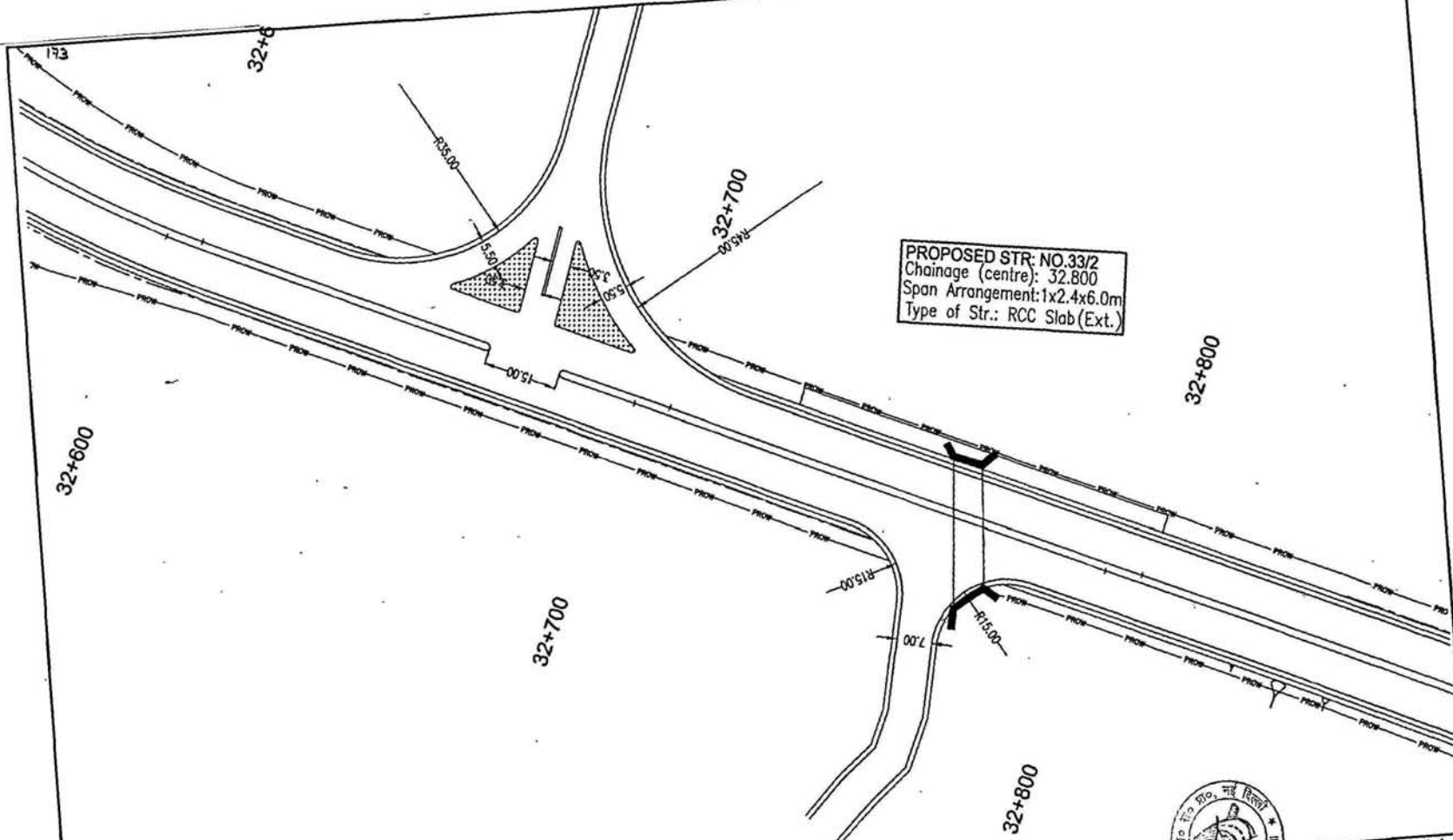
Appd: RINO.C.O.PUNJ

Date: JUNE-2009



Client
 Scate Wilson India Private Limited
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PROPOSED STR: NO.33/2
 Chainage (centre): 32.800
 Span Arrangement: 1x2.4x6.0m
 Type of Str.: RCC Slab (Ext.)

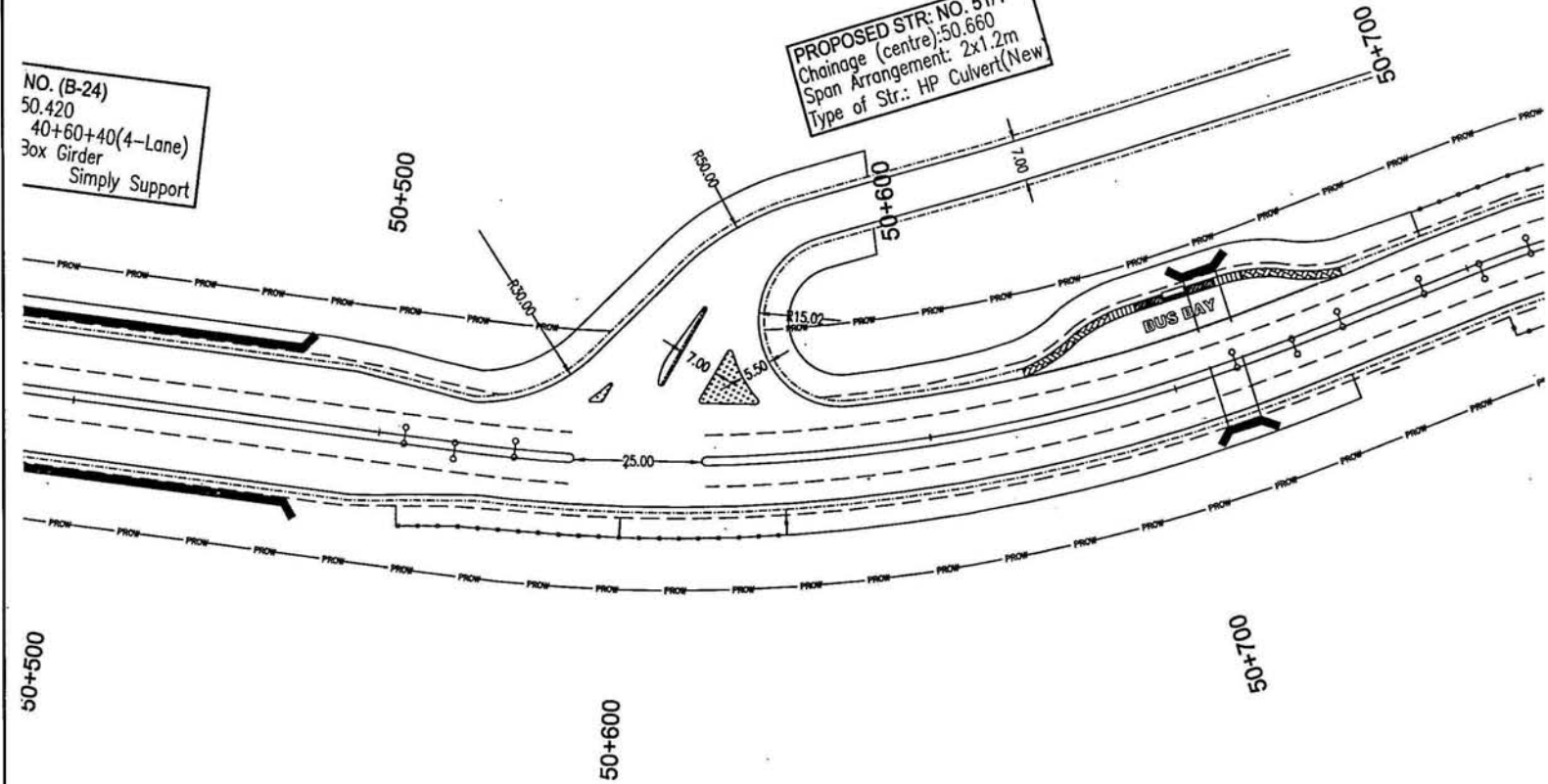
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|--|--|---|--|--|--|--|--|--|--|
| Project Title Consultancy services for preparation of detailed project report for Rehabilitation and upgrading of existing 3-lane road to 4-lane divided Category configuration of Section - 1 (Bypass section of NH-1 A Km 4.00 to Km 66.00) including Junction/overpass and (Bypass Bypass Phase - II) Programme of North-South corridor Project, Contract Package C-07/18 | | Client NATIONAL HIGHWAYS AUTHORITY OF INDIA (Ministry of Shipping, Road Transport & Highways) | | Drawing Title: JUNCTION IMPROVEMENT AT CH- 32+790 | | Drawing No.: SWM/1019/ROADS/JUN- 13 Sheet: N.H.A. 1 | | Client Wilson India Private Limited 20-26/4, MOHAN CO-OPERATIVE HO. ESTATE NEHRU ROAD NEW DELHI - 110044 Tel: (011) 41678340 - 48 Fax: (011) 41678350 E-mail: wipro@wilson.com | |
| Scale: 1:1500 Dgn: K.P.RASMD Appd: IRRIS C.D.PURI Date: JUNE-2009 | | Original Scale: 1:1500 Project No.: SWM/1019/ROADS/JUN-13 Plot No.: | | Scale: 1:1500 Dgn: K.P.RASMD Appd: IRRIS C.D.PURI Date: JUNE-2009 | | Scale: 1:1500 Dgn: K.P.RASMD Appd: IRRIS C.D.PURI Date: JUNE-2009 | | Scale: 1:1500 Dgn: K.P.RASMD Appd: IRRIS C.D.PURI Date: JUNE-2009 | |



| Revision Details | By | Date | Surf |
|------------------|----|------|------|
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| | | | |

NO. (B-24)
50.420
40+60+40(4-Lane)
Box Girder
Simply Support

PROPOSED STR: NO. 51/1
Chainage (centre): 50.660
Span Arrangement: 2x1.2m
Type of Str.: HP Culvert(New)



| Revision | Details | By | Chk | Date | Suffr |
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| | | | | | |
| | | | | | |
| | | | | | |

Project Title
Consultancy services for preparation of detailed project report for
Subclassification and upgrading of existing 3-lane road to 4-lane divided
Carriageway configuration of Jaunpur - Unhappur section of NH-1 A
(km 0.00 to km 66.00) including Jaunpur/Unhappur Bypass and Unhappur
Bypass Phase - III Programme of North-South corridor Project,
Contract Package C-07/18

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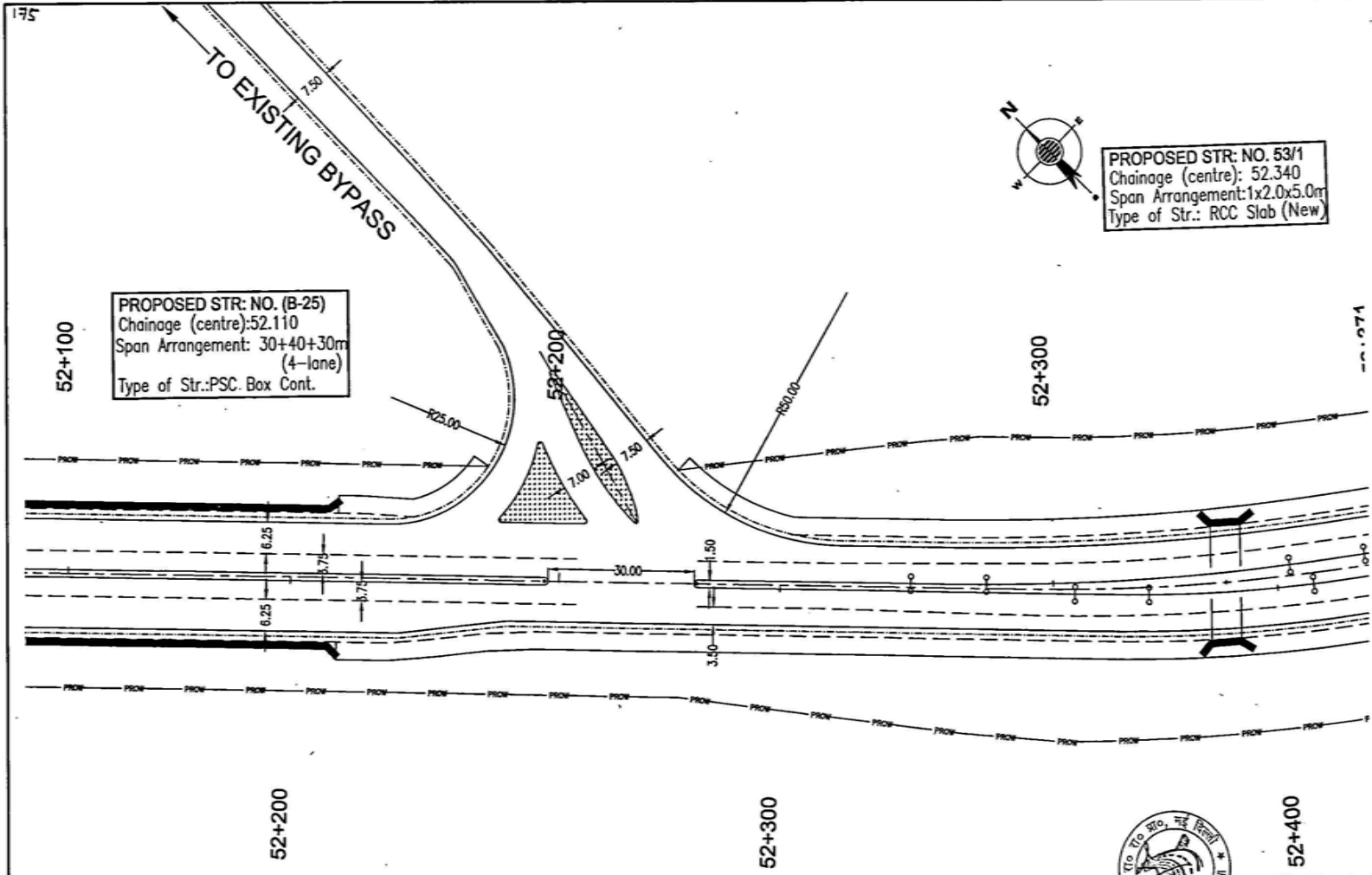
Original Scale: As per
Fabrication Scale: As per
Plot Scale: As per

Client
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AUTHORITY OF INDIA**
(Ministry of Shipping, Road Transport & Highways)

Drawing Title: JUNCTION IMPROVEMENT
AT CH- 50+530
Drawing No.: SWW1019/ROADS/JUN-14
Scale: -
Dra: K.L.N
Dgn: K.PRASAD
Appd: BBR2.C.D.PURI
Date: JUNE-2009

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PROPOSED STR: NO. (B-25)
Chainage (centre): 52.110
Span Arrangement: 30+40+30m
(4-lane)
Type of Str.: PSC. Box Cont.

PROPOSED STR: NO. 53/1
Chainage (centre): 52.340
Span Arrangement: 1x2.0x5.0m
Type of Str.: RCC Slab (New)



| Revision | By | CHK | Date | Scale |
|----------|----|-----|------|-------|
| | | | | |
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Project Title
Consulting services for preparation of detailed project report for Rehabilitation and upgrading of existing 2-lane road to 4-lane divided Carriageway and Upgradation of Junction - (Chainage portion of NH-1 A (km 8.00 to km 14.00) including Juncos/over) Nagaria and (Chainage Bypass Phase - III Programme of North-South corridor Project, Contract Package C-02/1)

Client
NATIONAL HIGHWAYS AUTHORITY OF INDIA
(Ministry of Shipping, Road Transport & Highways)

Client
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(Ministry of Shipping, Road Transport & Highways)

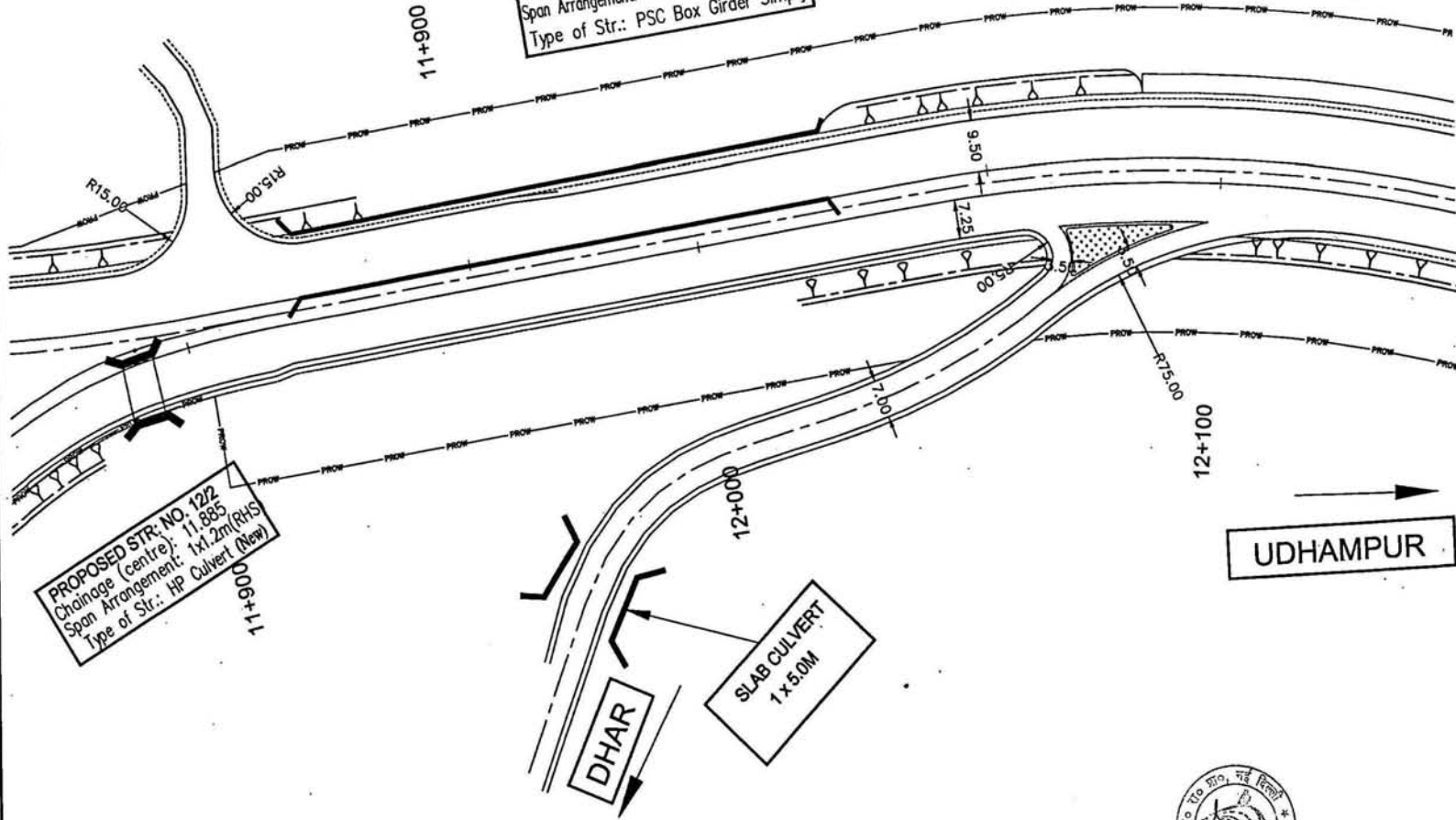
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Drawing No.: SWN/019/ROADS/JUN-15
Scale: -
Dra: K.L.N. Dgn: K.PRASAD Appd: BRD.C.D.PUR Date: JUNE-2009



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PROPOSED STR. NO. (B-31-LEFT)
 Chainage (centre): 11.970
 Span Arrangement: 60+40(2-Lane)
 Type of Str.: PSC Box Girder Simply



PROPOSED STR. NO. 122
 Chainage (centre): 11.885
 Span Arrangement: 1x1.2m(RHS)
 Type of Str.: HP Culvert (New)

SLAB CULVERT
 1 x 5.0M

DCHAR

UDHAMPUR

| Revision | Date | By | CHKD | DATE | BY |
|----------|------|----|------|------|----|
| | | | | | |
| | | | | | |
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Project Title
 Consultancy services for preparation of detailed project report for Rehabilitation and upgrading of existing 2-lane road to 4-lane divided. Contiguity configurations of Jammu - Udhampur section of NH-1A (km 0.00 to km 66.00) including Jammu(Part) Bypass and Udhampur Bypass Phase - II Programme of North - South - corridor Project. Contract Package C-10/11

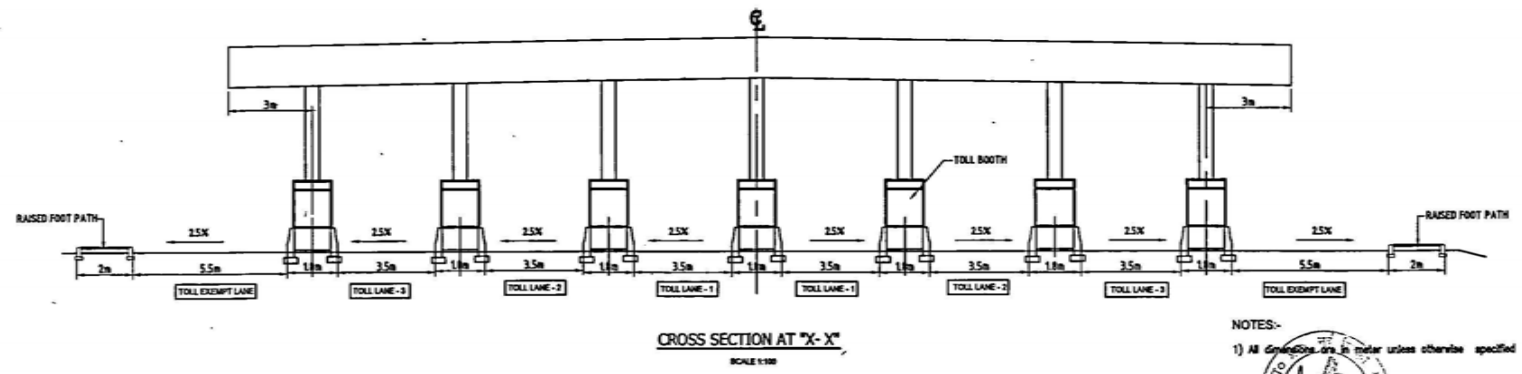
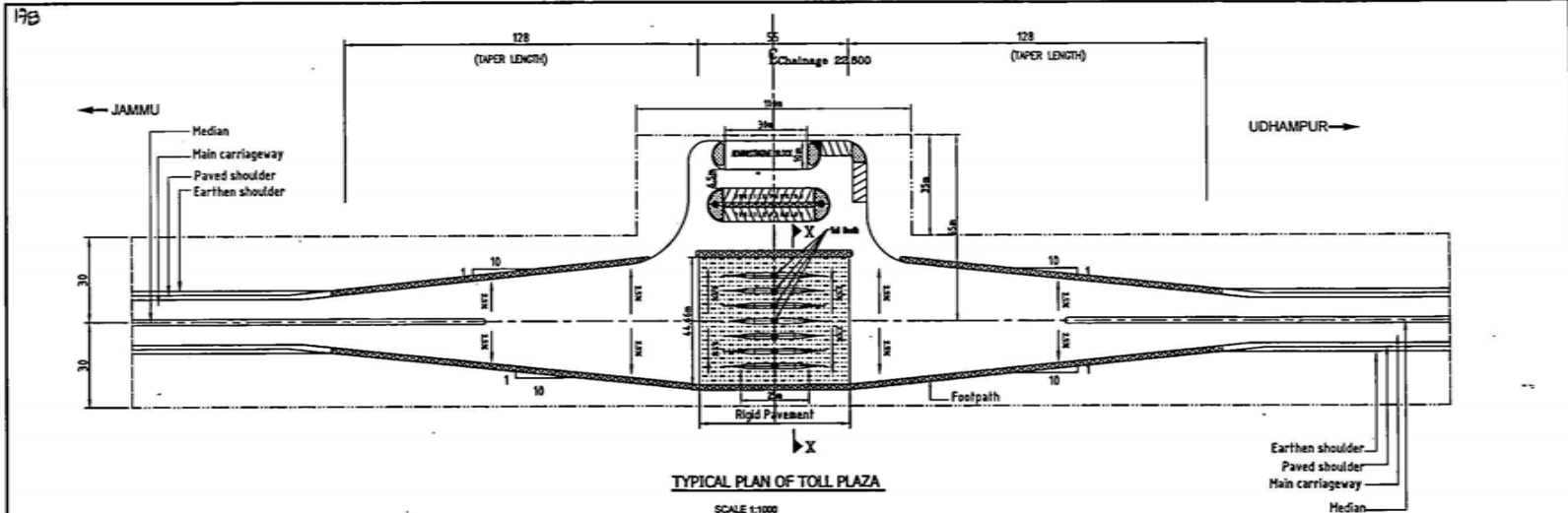
Client
NATIONAL HIGHWAYS AUTHORITY OF INDIA
 (Ministry of Shipping, Road Transport & Highways)

Drawing Title: JUNCTION IMPROVEMENT AT CH-12+100 (UDHAMPUR BYPASS)

Drawing No.: SW/1019/ROADS/JUN-17
Scale: 1:1000
Date: JUNE-2009

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NOTES:-
 1) All dimensions are in meter unless otherwise specified



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|------------------|------|-------|-------|--------|
| Revision Details | 100 | 1000 | 10000 | 100000 |
| By | Chal | Dotra | Suffa | |

Project Title
 Consultancy services for preparation of detailed project report for Rehabilitation and upgrading of existing 3-lane road to 4-lane divided carriageway configuration of Jammu - Udhampur section of NH-1A (Km 0.00 to Km 44.50) including Jammu (Pur) Bypass and Udhampur Bypass Phase - II Progression of North-South corridor Project, Contract Package C-02/18

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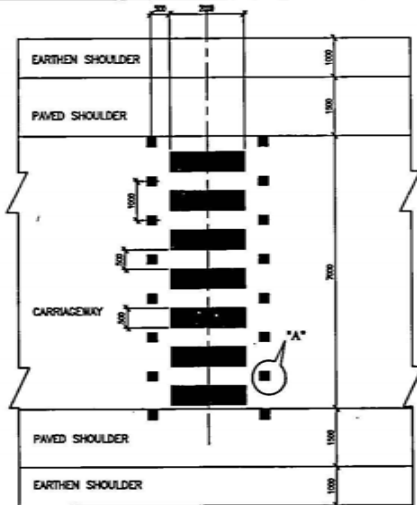
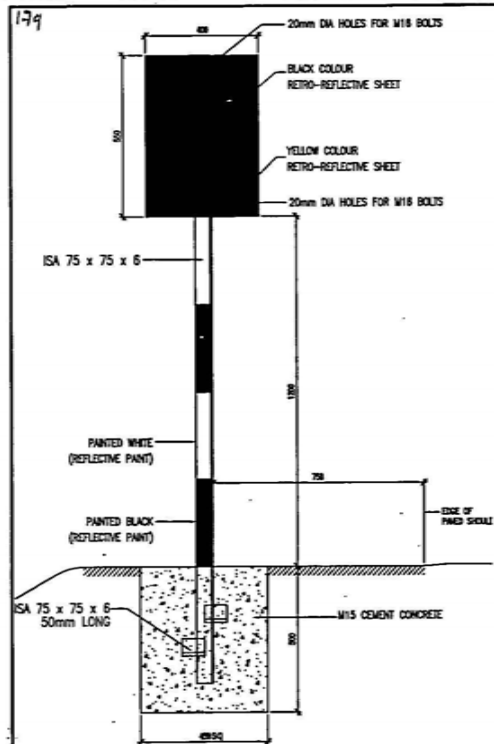
Original Size: A2
 Plot Size: 420mm x 594mm
 Scale: 1:1000

Client
NATIONAL HIGHWAYS AUTHORITY OF INDIA
 (Ministry of Shipping, Road Transport & Highways)

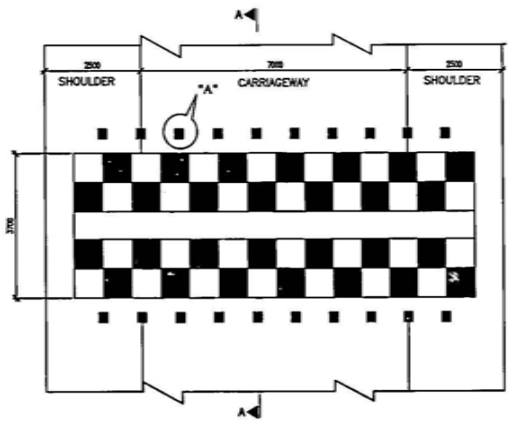
Drawing Title: **TYPICAL TOLL PLAZA DETAILS**
 Drawing No.: SW/1019/ROADS/MISC-01
 Scale: AS SHOWN
 Dn: K.L.N
 Dpn: K.PRASAD
 Appd: C.D.PURI
 Date: June-2009

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 WAZIRPUR, NEW DELHI - 110044
 Tel: (011) 41679340 - 49
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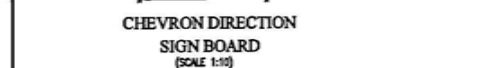




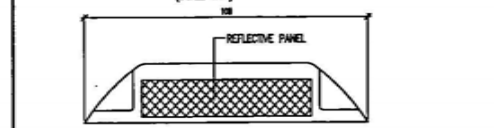
LOCATION OF RPM BEFORE PEDESTRIAN CROSSING
(SCALE 1:75)



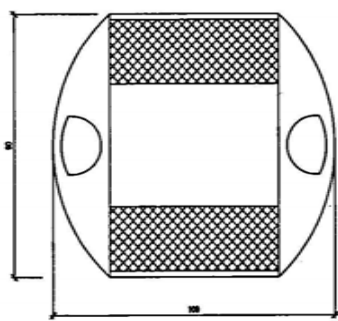
DETAILS OF ROAD HUMPS
(SCALE 1:75)



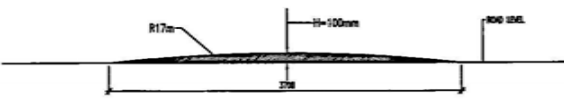
CHEVRON DIRECTION SIGN BOARD
(SCALE 1:10)



REFLECTIVE PAVEMENT MARKERS (RPM)
DETAIL - "A" (SIDE VIEW)
(SCALE 1:5)



REFLECTIVE PAVEMENT MARKERS (RPM)
DETAIL - "A" (TOP VIEW)
(SCALE 1:5)



SECTION - "A - A"
(SCALE 1:30)

REFLECTIVE PAVEMENT MARKERS

- SPECIFICATION OF MATERIAL, DESIGN, OPTICAL PERFORMANCE, TESTS AND FIXING OF RPM SHALL BE IN ACCORDANCE WITH TECH.SPEC. CL.812.
- LOCATIONS OF RPM

| CLOUR OF RPM | LOCATION | SPACING |
|--------------|--|---------|
| WHITE | ROAD HUMPS AS SHOWN IN DRG. | 1m C/C |
| AMBER | CURVES < 500m RADIUS ALONG CENTERLINE, AT MEDIAN OPENINGS | 3m C/C |
| RED | BEFORE AND AFTER PEDESTRIAN CROSSING AS SHOWN IN DRG. AND AT APPROACHES TO ROAD INTERSECTION AS SHOWN IN DRAWING | 1m C/C |

- RPM SHOULD MEET MINIMUM CIL VALUES EQUIVALENT TO CATEGORY 'A' STUDS AS PER CLAUSE 812 OF TECH.SPEC.

CHEVRON SIGN BOARD

- THE SIZE OF "CHEVRON" SIGN BOARD IS 400mm X 550mm.
- THE SIGN BOARD SHOULD BE WIDE-ANGLE MICRO-PRISMATIC LENS CONFORMING TO TECH.SPEC. CL.801.3.2.2
- THE RETRO-REFLECTIVE SURFACE AFTER CLEANING WITH SOAP AND WATER IN ANY CONDITION SHOULD HAVE THE MINIMUM CO-EFFICIENT OF RETRO-REFLECTION (NORTH WIDE 801.3.2 CLAUSE & MINISTRY'S LETTER NO. RW/MS-3302/51/88-00/111) AS INDICATED BELOW FOR ENCAPSULATED LENS.

| | |
|---------------|---|
| Project Title | Consultancy services for preparation of detailed project report for rehabilitation and upgrading of existing 2-lane road to 4-lane divided carriageway and provision of Jambus - Udhampur section of NH-1A. |
| Client | NATIONAL HIGHWAYS AUTHORITY OF INDIA |
| Scale | AS SHOWN |
| Date | June-2009 |

Client
NATIONAL HIGHWAYS AUTHORITY OF INDIA
(Ministry of Shipping, Road Transport & Highways)

| | |
|---------------|--|
| Drawing Title | TYPICAL DETAILS FOR ROAD HUMPS AND REFLECTIVE PAVEMENT MARKERS |
| Drawing No. | SW/1019/ROADS/MISC-02 |
| Scale | AS SHOWN |
| Drawn | K.L.N. |
| Checked | K.PRASAD |
| Appd. | C.D.PURI |
| Date | June-2009 |

Client
SOSU Wilson India Private Limited
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NEW DELHI - 110044
Tel: (011) 41678340 - 48
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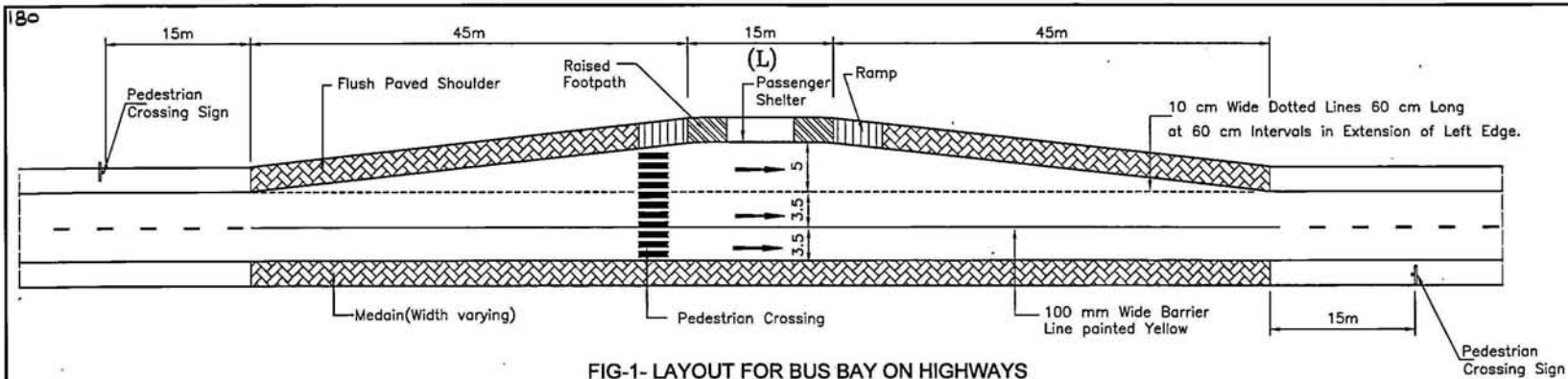


FIG-1- LAYOUT FOR BUS BAY ON HIGHWAYS
(Scale 1:300)

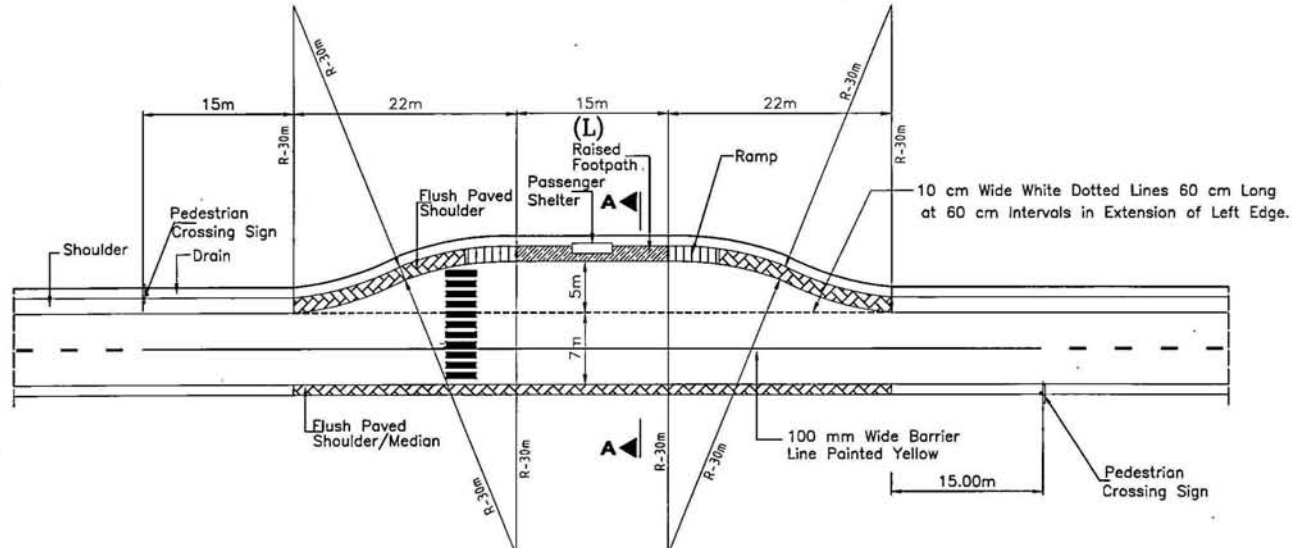
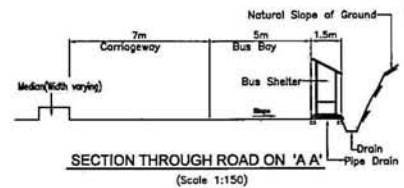


FIG-2- LAYOUT FOR BUS BAY ON HIGHWAYS IN HILLY AREAS
(Scale 1:300)

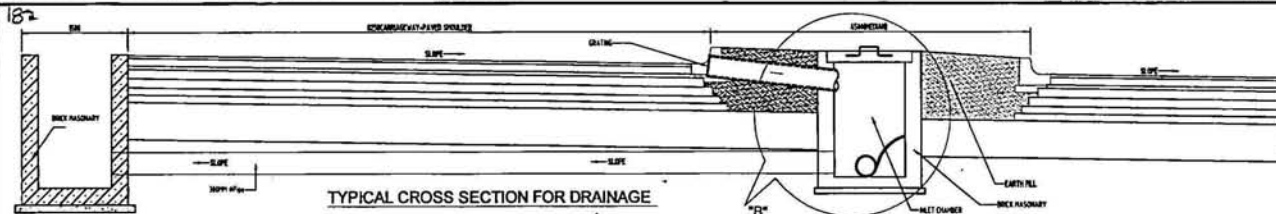


SECTION THROUGH ROAD ON 'A-A'
(Scale 1:150)

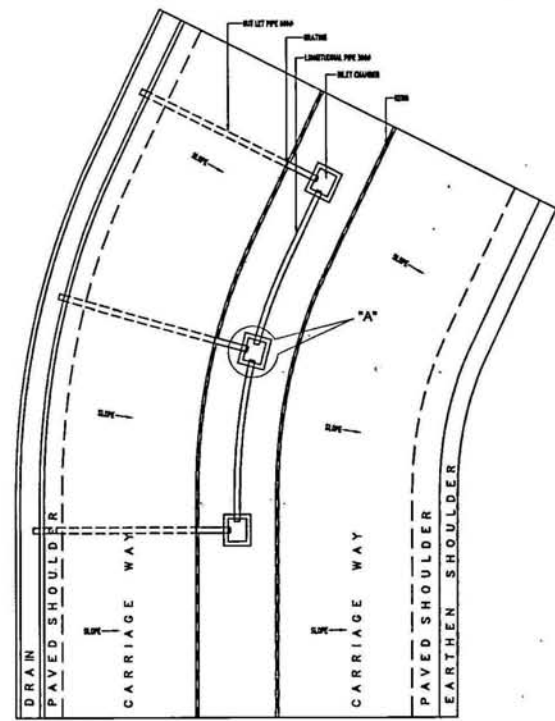
- NOTE:-**
- 1) All dimensions are in metre unless otherwise specified.
 - 2) "L"=15 m only one bus is expected at a time, it may be increase by 15 m for additional bus expected to stop.
 - 3) All traffic signs shall conform to IRC: 67-2001

| | | | | | | | | | | | | | | | |
|---|--|--|--|---|--|--|--|---|--|--|--|---|--|--|--|
| Project Title Consultancy services for preparation of detailed project report for Rehabilitation and upgrading of existing 3-lane road to 4-lane divided Carriageway configuration of Jammu - Udhampur section of NH-1 A (0m 0.00 to km 46.00) including Jammu/Parji Nagaria and Udhampur Bypass Phase - II Programme of North-South corridor Project, Contract Package C-22116 | | | | Client NATIONAL HIGHWAYS AUTHORITY OF INDIA (Ministry of Shipping, Road Transport & Highways) | | | | Drawing Title: TYPICAL BUS BAY DETAILS | | | | Scott Wilson India Private Limited A-26/4, MOHAN CO-OPERATIVE IND. ESTATE MATUHURA ROAD NEW DELHI - 110044 Tele: (011) 41679340 - 49 Fax: (011) 41679350 E-mail: swi@swindia.com | | | |
| Revision Details By: [] Date: [] | | | | Scale :- KTS Dm: K.L.N. Dgn: K.PRASAD Appd: C.O.PURI Date: June-2009 | | | | Sheet No. SW/1019/ROADS/MISC-03 | | | | | | | |

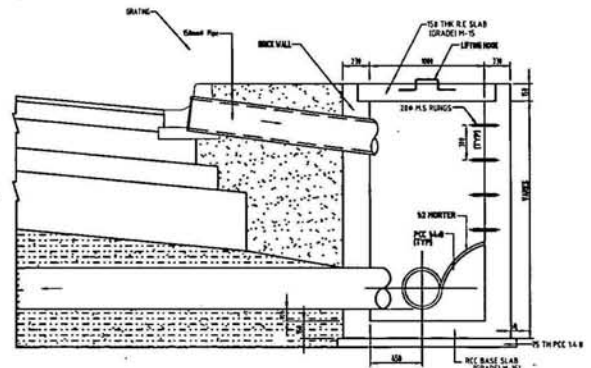




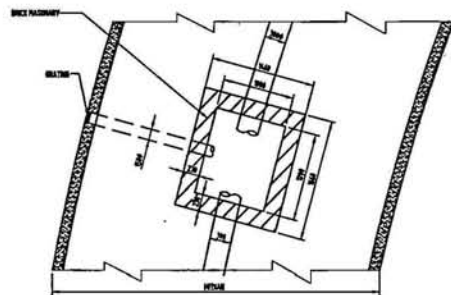
TYPICAL CROSS SECTION FOR DRAINAGE



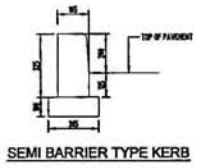
DRAIN ARRANGEMENT IN SUPERELEVATION AREA



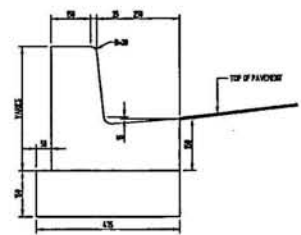
DETAIL "B"



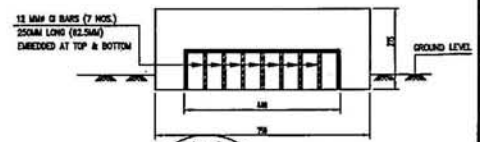
DETAIL "A"



SEMI BARRIER TYPE KERB



KERB DETAILS



GRATING DETAILS

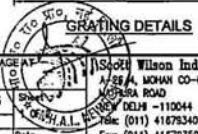
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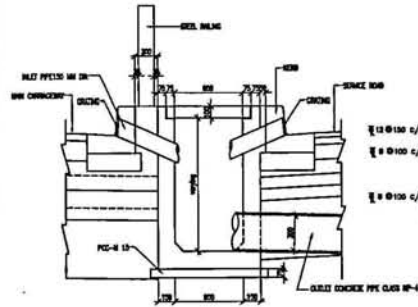
Project Title
 Consultancy services for preparation of detailed project report for Rehabilitation and upgrading of existing 3 lane road to 4 lane divided Carriageway configuration of 2 lanes - 1 lane median at 790+4 A (Km 6.09 to Km 64.00) including Jammu/Var) Highways and Unimager System Phase - II Programme of North-South corridor Project, Contract Package C-0318

Client
 NATIONAL HIGHWAYS AUTHORITY OF INDIA
 (Ministry of Shipping, Road Transport & Highways)

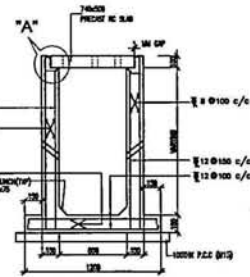
Drawing Title: TYPICAL DETAILS FOR DRAINAGE AT SUPER ELEVATION
Drawing No.: SW/1019/ROADS/MISC- 05
Scale: 1=N.T.S.
Date: June-2009
Des: K.L.N. **Appd:** C.D.PURJ

Client: Scott Wilson India Private Limited
 A-28 A, MOHAN CO-OPERATIVE IND. ESTATE
 PHOENIX ROAD
 Gurgaon, Haryana
 Phone: (011) 41678340 - 49
 Fax: (011) 41678350
 E-mail: scwilson@swi.com

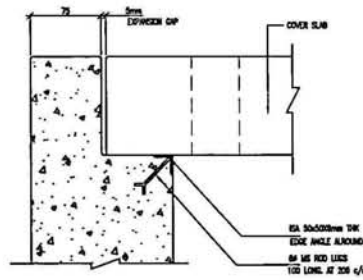




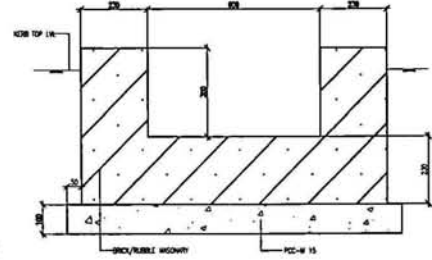
SECTIONAL DETAILS OF LONGITUDINAL DRAIN
SCALE-1:40



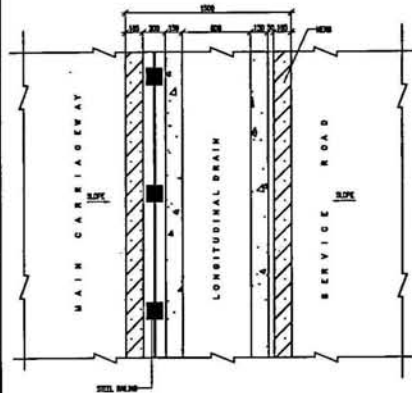
REINFORCEMENT DETAILS
SCALE-1:40



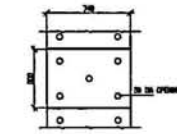
DETAIL-A"
SCALE-1:3



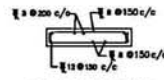
VIEW-1-1
SCALE-1:100



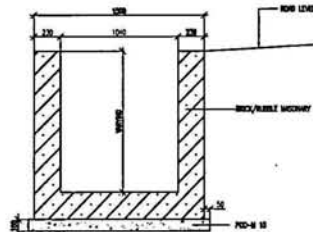
PLAN
SCALE-1:140



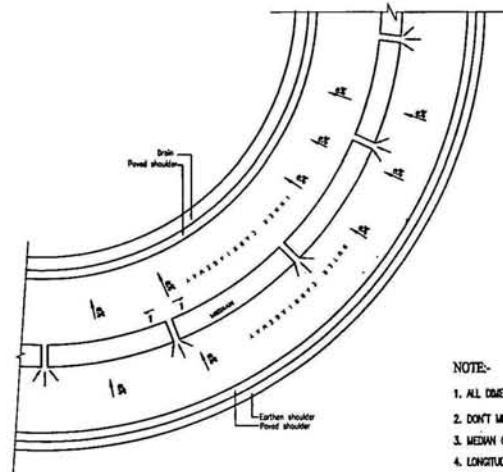
PLAN OF COVER SLAB
SCALE-1:40



DETAILS OF COVER SLAB
SCALE-1:40



OPEN DRAIN
SCALE-1:40



MEDIAN CUT FOR DRAINAGE IN SUPERELEVATION AREA
MEDIAN WIDTH (1.5M)
SCALE-1:15

NOTE:-

1. ALL DIMENSIONS ARE IN MM UNLESS SPECIFIED
2. DON'T MEASURE THE DRAWING ONLY WRITTEN DIMENSIONS IS FOLLOWED.
3. MEDIAN CUT DRAIN PROVIDING ONLY WHERE MEDIAN WIDTH 1.5 M
4. LONGITUDINAL DRAIN IS PROVIDING WHERE SERVICE ROAD EITHER BOTH SIDE OR ONE SIDE ALONG THE MAIN CARRIAGEWAY
5. MEDIAN CUT FOR DRAINAGE PURPOSE IS PROVIDED IN SUPER ELEVATION AREA. THE SIZE OF THE OPENING AND DISTANCE IN BETWEEN THE MEDIAN CUT ARE DECIDED ACCORDING TO THE SITE CONDITION.



| Revision Details | By | Date | Suffix |
|------------------|----|------|--------|
| | | | |
| | | | |

Project Title
Consultancy services for preparation of detailed project report for Rehabilitation and upgrading of existing 3-lane road to 4-lane divided Category and upgrading of Access - Collector section of NH-15 A Size 120 to km (64.03) including Interchange/Overpass and Underpass Project Phase - III Progression of North-South-connection Project, Contract Package C-02/15

Client
NATIONAL HIGHWAYS AUTHORITY OF INDIA (Ministry of Shipping, Road Transport & Highways)

Drawing Title
DRAINAGE DETAILS

Drawing No.
SW/1015/ROADS/MISC-05

Scale
K.L.S. 2 OF 2

Dr.
K.L.N.

Dgn.
K.PRASAD

Appd.
C.D.PURI

Date
June-2009

Client
NATIONAL HIGHWAYS AUTHORITY OF INDIA (Ministry of Shipping, Road Transport & Highways)

Drawing Title
DRAINAGE DETAILS

Drawing No.
SW/1015/ROADS/MISC-05

Scale
K.L.S. 2 OF 2

Dr.
K.L.N.

Dgn.
K.PRASAD

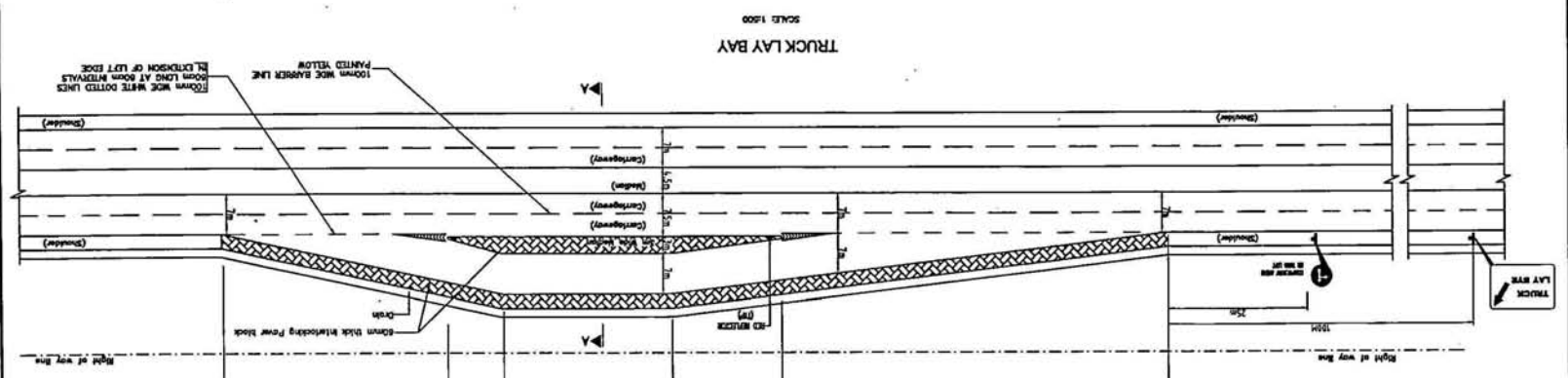
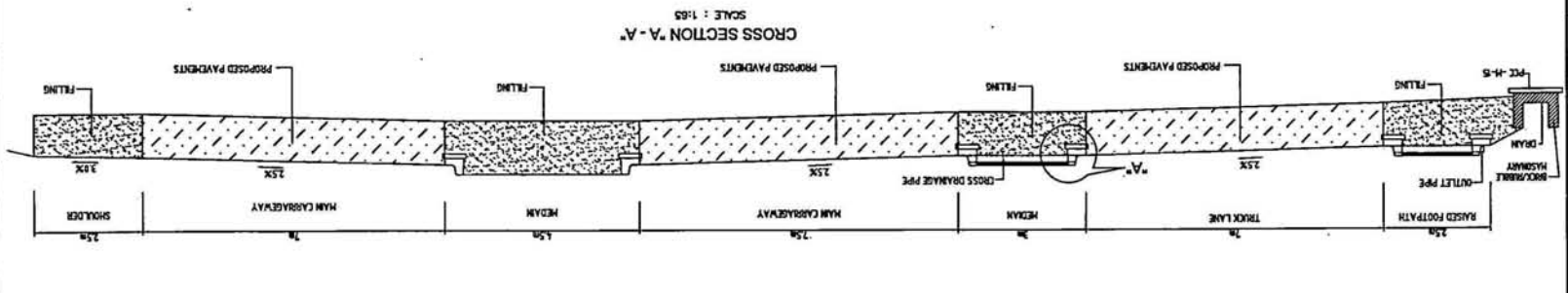
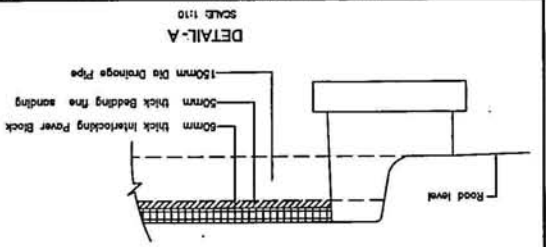
Appd.
C.D.PURI

Date
June-2009

Scott Wilson India Private Limited
A-26/A, MOHAN CO-OPERATIVE IND. ESTATE
MATHURA ROAD
NEW DELHI - 110044
Tele: (011) 41679340 - 49
Fax: (011) 41679350
E-mail: info@swindia.com



| | | | | | | | | | |
|--|--|----|--|------|--|---------|--|------|--|
| Revision Details | | By | | Date | | Checked | | Date | |
| | | | | | | | | | |
| Project Title TYPICAL DETAILS OF TRUCK LAY BAY | | | | | | | | | |
| Drawing No. S/W/19/ROADS/MISC-08 | | | | | | | | | |
| Scale: AS SHOWN | | | | | | | | | |
| Drawing Date: June-2009 | | | | | | | | | |
| Project Name: S&P Wilson India Private Limited | | | | | | | | | |
| Project Location: NEW DELHI - 110041 | | | | | | | | | |
| Project No: 11/19/ROADS/MISC-08 | | | | | | | | | |
| Project Manager: S&P Wilson India Private Limited | | | | | | | | | |
| Project Engineer: S&P Wilson India Private Limited | | | | | | | | | |
| Project Designer: S&P Wilson India Private Limited | | | | | | | | | |
| Project Checker: S&P Wilson India Private Limited | | | | | | | | | |
| Project Approver: S&P Wilson India Private Limited | | | | | | | | | |
| Project Status: Approved | | | | | | | | | |





T-INTERSECTION



GAP IN MEDIAN



NARROW ROAD



ROAD WIDENS



RIGHT HAND CURVE



LEFT HAND CURVE



SIDE ROAD RIGHT



SIDE ROAD LEFT



TWO WAY TRAFFIC



Y-INTERSECTION



ROAD HUMP



Y-INTERSECTION



Y-INTERSECTION



STAGGERED INTERSECTION



MAJOR ROAD



HAIR PIN BED RIGHT



HAIR PIN BED LEFT



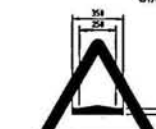
PEDESTRIAN CROSSING



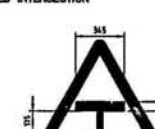
PEDESTRIAN CROSSING



TRAFFIC SIGNAL



SPEED BREAKER



T INTER SECTION



CROSS ROAD



SCHOOL



HAIR PIN BED LEFT



ROUND ABOUT



STEEP ASCENT



STEEP DESCENT



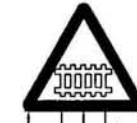
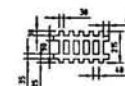
START OF DUAL CARRIAGEWAY



END OF DUAL CARRIAGEWAY



REDUCED CARRIAGEWAY



GUARDED RAILWAY CROSSING

CAUTIONARY/WARNING SIGNS

| | | | |
|--|--|--|--|
| | | | |
| | | | |
| | | | |
| | | | |

Project Title
 Consultancy services for preparation of detailed project report for
 Rehabilitation and upgrading of existing 3-lane road to 4-lane divided
 Carriageway configuration of Junction - 128kmpt section of NH-1 A
 (km 6.00 to km 64.00) including Access/Driveways and Underpass
 System Plans - E2 Programme of North-South corridor Project,
 Chennai Package-C/2/14

Client
 NATIONAL HIGHWAYS
 AUTHORITY OF INDIA
 (Ministry of Shipping, Road Transport & Highways)

Drawing Title: ROAD SIGNS
 WARNING /CAUTIONARY SIGNS
Drawing No.: SW/1019/ROADS/MISC-07
Scale: 1:20
Sheet: 1 of 3
Date: June-2009

Designers: K.L.N., K.PRASAD, C.D.PURI



Soori Wilson India Private Limited
 A-26/1, SOHAN CO-OPERATIVE IND. ESTATE
 NEW DELHI - 110044
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 Fax: (011) 41679350
 E-mail: swi@soorindia.com



MANDATORY/REGULATORY SIGNS



STRAIGHT PROHIBITED
OR NO ENTRY



ONE WAY SIGNS



STRAIGHT TURN PROHIBITED



LEFT TURN PROHIBITED



U-TURN PROHIBITED



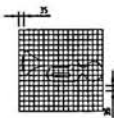
NO ENTRY



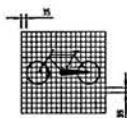
GIVE WAY



HORN PROHIBITED



CYCLE PROHIBITED



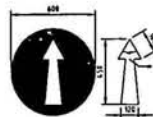
NO PARKING OR NO STANDING



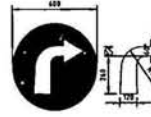
NO PARKING



COMPULSORY
TURN LEFT
(FOR THROUGH TRAFFIC TO
BE OBSERVED)



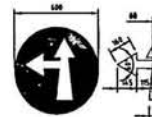
COMPULSORY
AHEAD ONLY



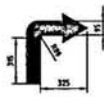
COMPULSORY
TURN RIGHT AHEAD
(FOR THROUGH TRAFFIC TO
BE OBSERVED)



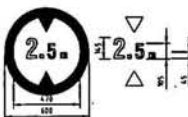
COMPULSORY
KEEP LEFT



COMPULSORY
AHEAD OR TURN LEFT
(FOR THROUGH TRAFFIC TO
BE OBSERVED)



STOP



HEIGHT LIMIT



SPEED LIMIT



NO PARKING OR STANDING



NO PARKING



COMPULSORY CYCLE TRACK



COMPULSORY SOUND HORN

NOTE:

1. The exact location of signs shall be determined at site.
2. The size and location of all Signs should follow as per IRC-67-2001
3. Signs for through traffic shall be of 900 mm dia and for Service Roads 750 mm dia.
4. The pavement markings at signalized junction to comply with IRC : 35-1997
5. All Dimension are in mm

| | | | | | |
|----------|---------|----|-----|------|------|
| Revision | Details | By | Chd | Date | Surf |
| | | | | | |

Project Title
 Consultancy services for preparation of detailed project report for
 Rehabilitation and upgrading of existing 2-lane road to 4-lane divided
 carriageway configuration of Section - Dhanuvar section of NH-1 A
 (km 0.00 to km 64.00) including Interchange (I/C) Nagaria and Dhanuvar
 Project Phase - II Programme of North-South corridor Project,
 Contract Package C-02718

Original Title A2
 Plot Scale
 1:10 1:20 1:30 1:40 1:50

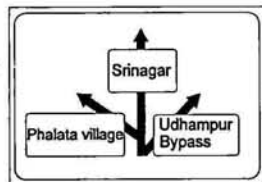
Client
**NATIONAL HIGHWAYS
 AUTHORITY OF INDIA**
 (Ministry of Shipping, Road Transport & Highways)

Drawing Title: ROAD SIGNS REGULATORY SIGNS
 Drawing No.: SW/1019/ROADS/MISC- 07
 Scale :- 1:20
 Dm K.L.N Dgn. K.PRASAD Appd. C.D.PURI Date June-2009
 Sheet 2 of 3
 Client: SSM Wilson India Private Limited
 A-26/2, ANHAN CO-OPERATIVE IND. ESTATE
 MIDC ROAD
 NEW DELHI - 110044
 Tel: (011) 41679340 - 49
 Fax: (011) 41679350
 E-mail: ssmindia@rediffmail.com

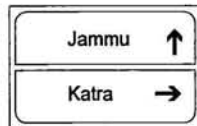


DIRECTION AND PLACE IDENTIFICATION SIGNS

THE SIZE OF THESE SIGNS DEPENDS ON THE MESSAGE AND SIZE OF LETTERS



ADVANCE DIRECTION SIGN
B-20



DESTINATION SIGN



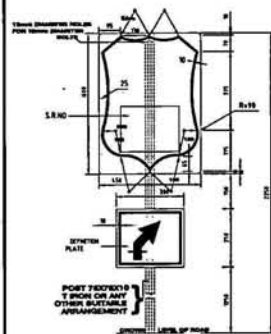
RE - ASSURANCE SIGN



DIRECTION SIGN
B-21

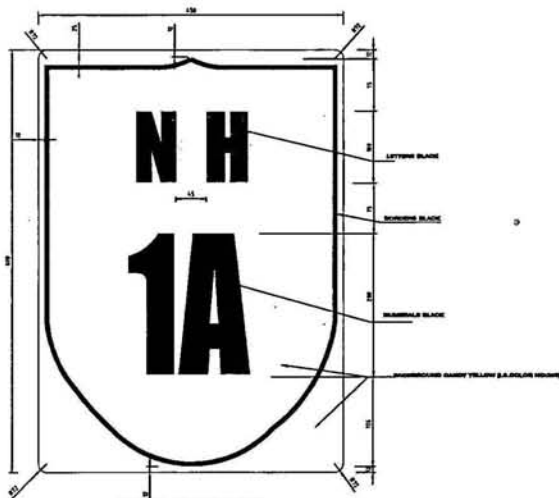


PLACE IDENTIFICATION SIGN



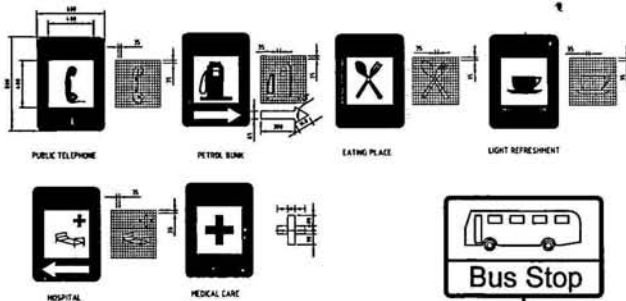
ASSEMBLY OF ROUTE MARKER SIGN

ARRANGEMENT FOR ERECTION OF STATE ROUTE MARKER SIGN
B-17B-24



NH ROUTE MARKER SIGN
B-19A-23

INFORMATORY SIGNS



PUBLIC TELEPHONE

PETROL BUNK

EATING PLACE

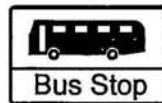
LIGHT REFRESHMENT

HOSPITAL

MEDICAL CARE



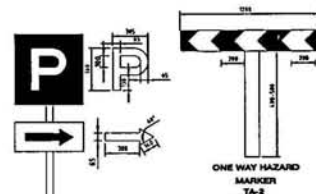
BUS STOP



Bus Stop

100m

BUS STOP



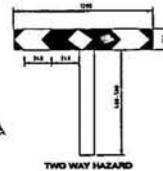
ONE WAY HAZARD MARKER
TA-2



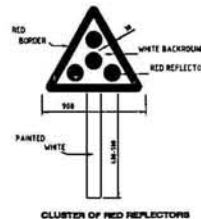
PARK THIS SIDE



PARKING BOTH SIDES



TWO WAY HAZARD MARKER
TA-3



CLUSTER OF RED REFLECTORS
T-5

General Notes and Remarks

1. The exact location of signs shall be determined at site.
2. The size and location of all signs should follow as per IRC-67-2001
3. Signs for through traffic shall be of 900 mm dia and for Service Roads 750 mm dia.
4. The pavement markings at signalized junction to comply with IRC : 35-1997
5. All Dimension shown in the drawings are in mm
6. Road signs numbers (S-17,S-24,S-23,T-1,TA-3) are for referring the signs in other drawings
7. In case of facility information signs, the size of the rectangle shall be 80cm x 60cm for normal sized sign and 60 cm for small sized sign. The size of the square shall be 40cm x 40cm respectively. The size of the symbols shown in for normal sized sign and in case of small sized sign, the symbol should be three-fourth of the size shown.

| | | | | |
|------------------|----|-----|------|------|
| Revision Details | By | Chk | Date | Suff |
| | | | | |

Project Title
 Consultancy services for preparation of detailed project report for Rehabilitation and upgrading of existing 2-lane road to 4-lane divided carriageway configuration of Jammu - Udhampur section of NH-1A (km 0.00 to km 64.00) including Jammu/Port Nagar and Udhampur Bypass Phase - III Programme of North-South - corridor Project, Corridor Package C-0218

Client
NATIONAL HIGHWAYS AUTHORITY OF INDIA
 (Ministry of Shipping, Road Transport & Highways)

Client
NATIONAL HIGHWAYS AUTHORITY OF INDIA
 (Ministry of Shipping, Road Transport & Highways)

Drawing Title: **ROAD SIGNS INFORMATORY SIGNS**
 Drawing No.: **SW/1019/ROADS/MISC- 07**
 Scale: 1:20
 Dgn: K.PRASAD
 Appd: C.D.PURI

Drawn by: **Scoti Wilson India Private Limited**
 SHEKHAR A.L. BHAWHARA ROAD
 NEW DELHI - 110044
 Tel: (011) 41679340 - 49
 Fax: (011) 41679350
 E-mail: scoti@scoti.com





**NATIONAL HIGHWAYS
AUTHORITY OF INDIA**

Ministry of Shipping, Road Transport & Highways)

Client: National Highways Authority of India
Project Title: Construction and upgrading of existing 2 lane road to 4 lane divided highway (2 lanes in each direction) with provision of drainage, lighting and sound barrier.
Contract No.: NHAI/2018/ROADS/MS-08
Scale: 1:100
Sheet No.: NHAI/2018/ROADS/MS-08/100

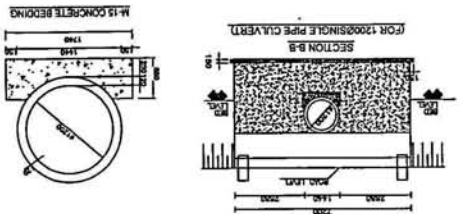
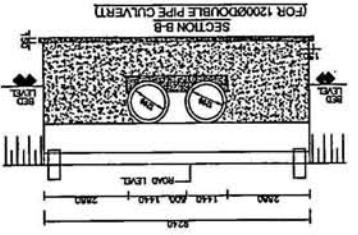
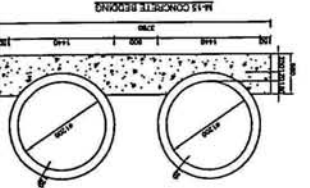
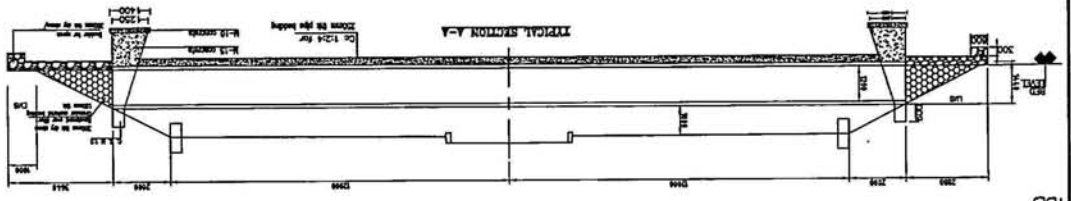
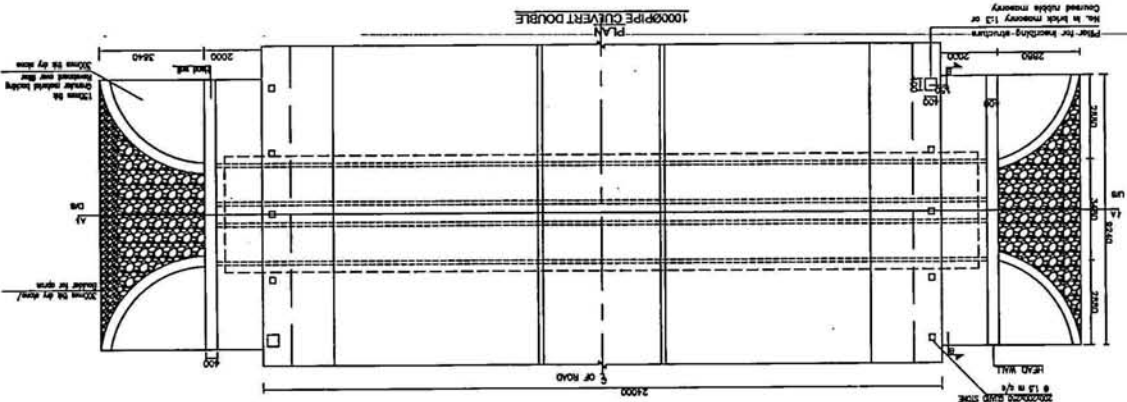
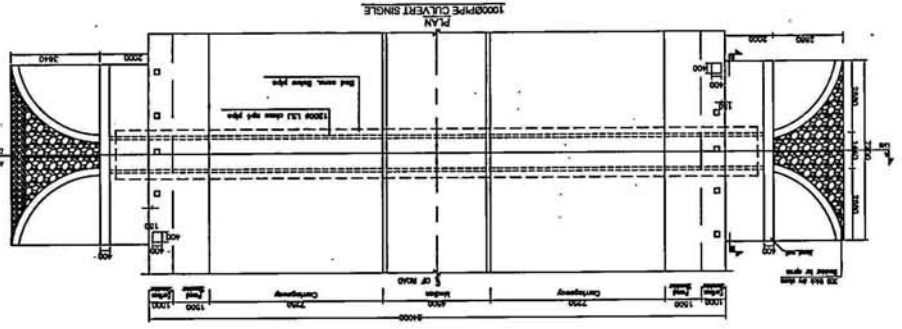
Revision Details:

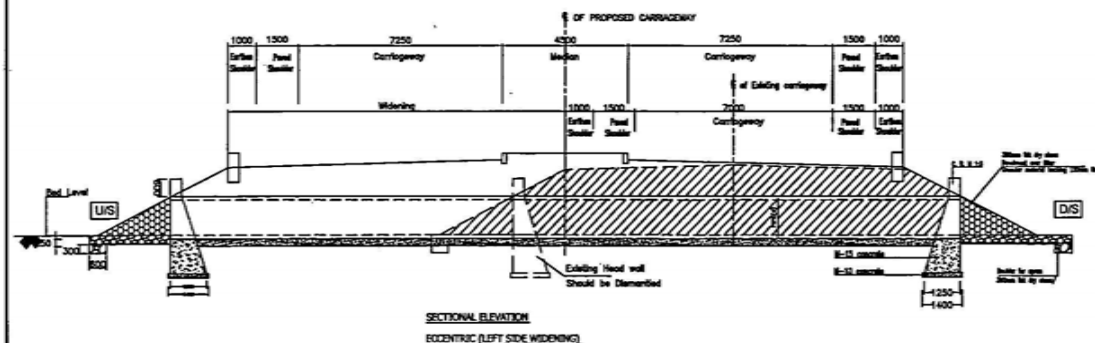
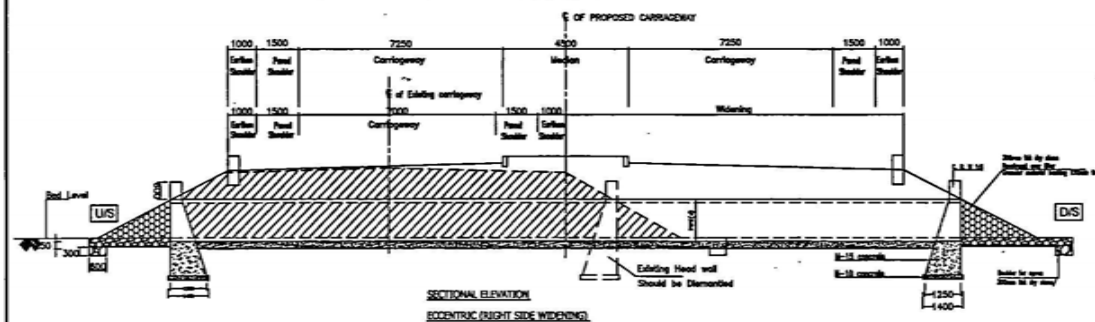
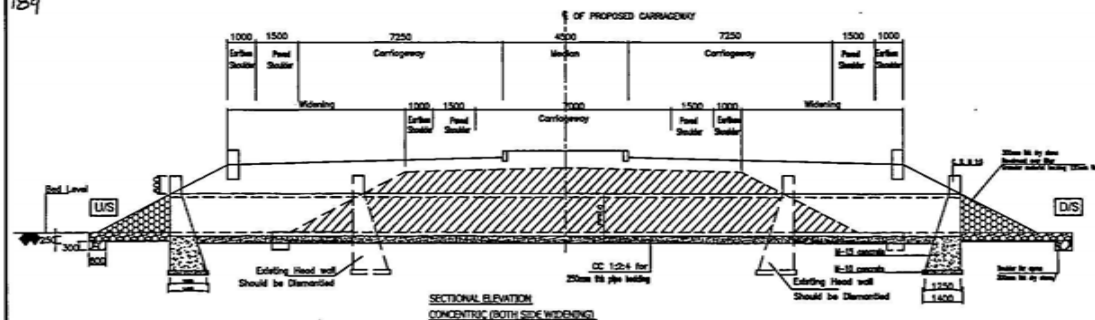
| Rev. | By | Date | Remarks |
|------|-----|------|---------------|
| 01 | XXX | XXX | Initial Issue |
| 02 | XXX | XXX | Revised |
| 03 | XXX | XXX | Revised |
| 04 | XXX | XXX | Revised |

| Rev. | By | Date | Remarks |
|------|----|------|---------|
| | | | |
| | | | |
| | | | |

Working Title: TYPICAL PIPE CULVERTS DETAILS

- NOTES:**
- 1) ALL DIMENSIONS ARE IN MM UNLESS OTHERWISE STATED.
 - 2) THE PIPES SHALL BE R.C.C. H.P. 4 NON PRESSURE PIPES. THE SPECIFICATIONS OF PIPES SHALL BE AS PER I.S. CODES.
 - 3) MINIMUM CUSHION ABOVE PIPE INCLUDING ROAD CURST SHALL BE 1.0 M. IN CASES IF DOWN STREAM IS LOW SLOPE MAY BE INCREASED UP TO 1.20.
 - 4) MINIMUM DEPTH OF EXISTING CUSHION OVER PIPE INCLUDING ROAD CURST SHALL NOT BE LESS THAN 100MM FOR NEW/RECONSTRUCTION CULVERTS IN CASE OF EXISTING SOUND AND SWEET CULVERTS A MINIMUM CUSHION OF 60MM MAY BE ACCEPTABLE.
 - 5) EXISTING CULVERTS OF DAMAGED ROAD AND ABOVE ARE SOUND CONDITION AND FUNCTIONING SATISFACTORILY MAY BE RETAINED AND EXTENDED.
 - 6) MINIMUM DIAMETER OF PIPES FOR NEW PIPE CULVERTS SHALL BE 1200MM.
 - 7) LONGITUDINAL SLOPE OF PIPE SHOULD BE MAINTAINED 1:100.
 - 8) FIRST CLASS BEDDING CAN BE USED FOR MINIMUM DEPTH OF ALL OF 4 METERS.





NOTES:-

- 1) ALL DIMENSIONS ARE IN MM UNTIL & UNLESS STATED.
- 2) THE PIPES SHALL BE R.C.C NP-4 NON PRESSURE PIPES. THE SPECIFICATIONS OF PIPES SHALL BE AS PER I.S. CLASS
- 3) MINIMUM CUSHION ABOVE PIPE INCLUDING ROAD CRUST SHALL BE 1.0 M.
- 4) IN CASES IF DOWN STREAM IS LOW SLOPE MAY BE INCREASED UPTO 1:20.
- 5) IF UPSTREAM IS HIGH HALF ROUND WELL BE PROVIDED.
- 6) MINIMUM DEPTH OF EARTH CUSHION OVER PIPE INCLUDING ROAD CRUST SHALL NOT BE LESS THAN 1000MM FOR NEW/RECONSTRUCTION CULVERTS. IN CASE OF EXISTING SOUND AND SAFE CULVERTS A MINIMUM CUSHION OF 600MM MAY BE ACCEPTABLE.
- 7) EXISTING CULVERTS OF DIAMETER 500MM AND ABOVE ARE SOUND CONDITION AND FUNCTIONING SATISFACTORILY MAY BE RETAINED AND EXTENDED.
- 8) MINIMUM DIAMETER OF PIPES FOR NEW PIPE CULVERTS SHALL BE 1200MM
- 9) LONGITUDINAL SLOPE OF PIPE SHOULD BE MINIMUM 1 IN 1000
- 10) FIRST CLASS BEDDING CAN BE USED FOR MAXIMUM HEIGHT OF FILL OF 4 METERS.
- 11) EXISTING PIPE DIA (φ) SHOULD BE VARYING 900 MM TO 1200MM

| Revision | Details | By | Chk | Date | Surf |
|----------|---------|----|-----|------|------|
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Project Title
 Consultancy services for preparation of detailed project report for
 Rehabilitation and upgrading of existing 13km road to 4-lane divided
 Carriageway configuration of Jammu - Udhampur section at NH-1A
 (as I.S. to be 66.00) including Jammu (NH) Nagar and Udhampur
 Bypass Phase - III Programme of North - South corridor Project,
 Contract Package C-2014

Client
 NATIONAL HIGHWAYS
 AUTHORITY OF INDIA
 (Ministry of Shipping, Road Transport & Highways)

Drawing Title: TYPICAL PIPE CULVERTS
 WIDENING DETAILS - D8
 Drawing No.: SWM1019/ROADS/MISC-D8
 Scale: 1:15
 Dn: K.L.N
 Dgn: K.PRASAD
 Appd: C.D.PURI
 Date: 20.06.2009

Sheet
 Scale: 1:15
 Dn: K.L.N
 Dgn: K.PRASAD
 Appd: C.D.PURI
 Date: 20.06.2009



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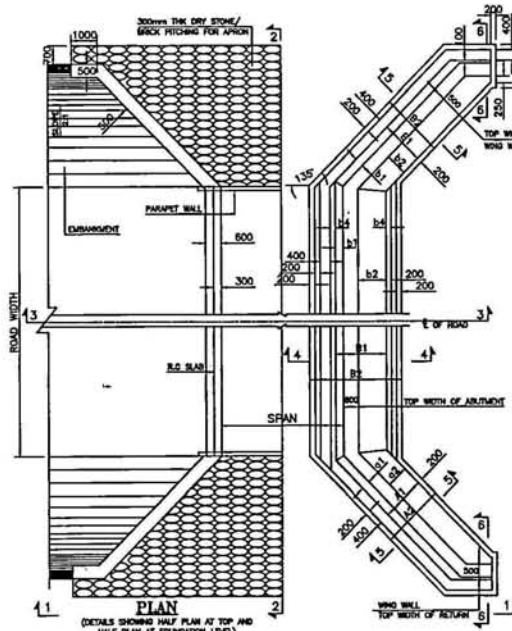


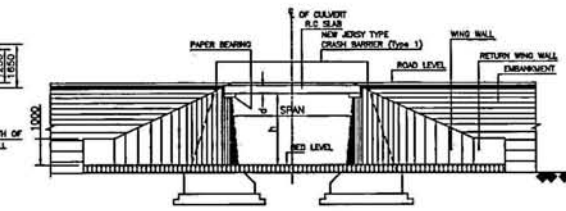
TABLE OF DIMENSIONS FOR WING WALL

| Span | Up to 3 metres | | | | 3 metres | | | |
|------|----------------|------|------|------|----------|------|------|------|
| | 1.5 | 2.0 | 2.5 | 3.0 | 1.5 | 2.0 | 2.5 | 3.0 |
| H | 0.18 | 0.20 | 0.22 | 0.25 | 0.19 | 0.21 | 0.23 | 0.24 |
| B1 | 0.45 | 0.50 | 0.55 | 0.60 | 0.48 | 0.53 | 0.58 | 0.63 |
| B2 | 1.15 | 1.3 | 1.45 | 1.6 | 1.2 | 1.35 | 1.5 | 1.65 |
| B3 | 1.8 | 2.0 | 2.2 | 2.45 | 1.9 | 2.1 | 2.3 | 2.55 |

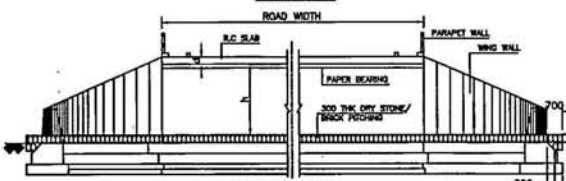
| Span | 4 metres | | | | 6 metres | | | | 8 metres | | | |
|------|----------|------|------|------|----------|------|------|------|----------|------|------|------|
| | 1.5 | 2.0 | 2.5 | 3.0 | 1.5 | 2.0 | 2.5 | 3.0 | 1.5 | 2.0 | 2.5 | 3.0 |
| H | 0.18 | 0.21 | 0.24 | 0.28 | 0.19 | 0.22 | 0.25 | 0.28 | 0.2 | 0.23 | 0.26 | 0.29 |
| B1 | 0.48 | 0.53 | 0.58 | 0.63 | 0.51 | 0.56 | 0.61 | 0.66 | 0.7 | 0.75 | 0.8 | 0.85 |
| B2 | 1.15 | 1.3 | 1.45 | 1.6 | 1.2 | 1.35 | 1.5 | 1.65 | 1.4 | 1.55 | 1.7 | 1.85 |
| B3 | 1.8 | 2.0 | 2.2 | 2.45 | 1.9 | 2.1 | 2.3 | 2.55 | 2.7 | 2.9 | 3.1 | 3.3 |

TABLE OF DIMENSIONS FOR ABUTMENT

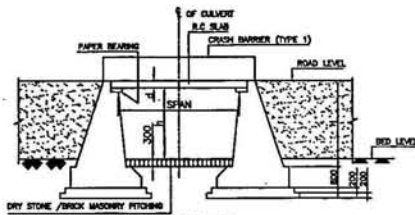
| Effective Span | 3.0m, 4.0m and 5.0m | | | | 5.0m and 6.0m | | | | 6.0m and 8.0m | | | | | |
|----------------|---------------------|------|------|------|---------------|------|------|------|---------------|------|------|------|------|-----|
| | 1.5m | 2.0m | 2.5m | 3.0m | 1.5m | 2.0m | 2.5m | 3.0m | 2.0m | 2.5m | 3.0m | 3.5m | 4.0m | |
| H | 0.15 | 0.2 | 0.25 | 0.3 | 0.15 | 0.2 | 0.25 | 0.3 | 0.4 | 0.2 | 0.25 | 0.3 | 0.35 | 0.4 |
| B1 | 0.5 | 0.7 | 0.95 | 1.1 | 0.5 | 0.7 | 0.95 | 1.1 | 1.25 | 1.4 | 1.6 | 1.8 | 2.0 | 2.2 |
| B2 | 0.5 | 0.7 | 0.95 | 1.1 | 0.5 | 0.7 | 0.95 | 1.1 | 1.25 | 1.4 | 1.6 | 1.8 | 2.0 | 2.2 |
| B3 | 0.5 | 0.7 | 0.95 | 1.1 | 0.5 | 0.7 | 0.95 | 1.1 | 1.25 | 1.4 | 1.6 | 1.8 | 2.0 | 2.2 |
| B4 | 1.45 | 1.7 | 2.0 | 2.3 | 1.6 | 1.9 | 2.2 | 2.5 | 2.7 | 2.9 | 3.1 | 3.3 | 3.5 | 3.7 |
| B5 | 2.05 | 2.3 | 2.6 | 2.9 | 3.15 | 3.4 | 3.7 | 4.0 | 4.3 | 4.6 | 4.9 | 5.2 | 5.5 | 5.8 |



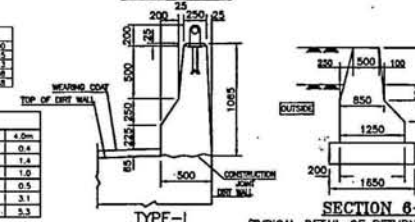
VIEW 1-1



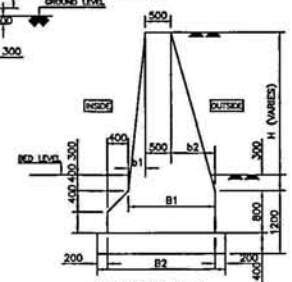
SECTION 2-2



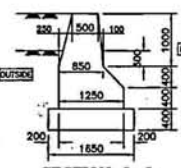
SECTION 3-3



SECTION 4-4 (TYPICAL DETAIL OF ABUTMENT) SCALE 1:50



SECTION 5-5 (TYPICAL DETAIL OF WING WALL)



SECTION 6-6 (TYPICAL DETAIL OF RETURN WING WALL FOR ALL SECTIONS)

- NOTES:-**
1. ABUTMENT AND WING WALL SECTIONS FOR INTERMEDIATE HEIGHTS TO BE ADOPTED SIMILARLY.
 2. THE VARIOUS DIMENSIONS TO BE SLIGHTLY ADJUSTED TO SUIT THE SIZE OF BRICKS WHENEVER NECESSARY.
 3. THE SECTIONS ARE APPLICABLE FOR CULVERTS REQUIRING FOR CLASS 70 OR 3 LINES OF CLASS A LOADS, UNLESS OTHERWISE SPECIFIED WITHOUT PROVISION OF APPROACH SLAB.
 4. THE SECTIONS SHALL BE IN CONCRETE GRADE 1:3:6, BRICK MASONRY IN CONCRETE GRADE 1:3 OR COARSE RUMBLE MASONRY (2nd SPEC) IN CONCRETE GRADE 1:3. THE FOUNDATION CONCRETE SHALL BE IN CONCRETE GRADE 1:3:6.
 5. THESE SECTIONS ARE NOT APPLICABLE TO BRIDGE CLASS IV AND V.
 6. ALL DIMENSIONS ARE IN MM.
 7. DO NOT SCALE THE DIMENSIONS FOLLOWING DIMENSIONS.
 8. FOR SLAB THICKNESS AND OTHER DETAILS REFER INC.13/2004.
 9. TYPICAL ARRANGEMENT OF CURBING SPOLTS HAS BEEN SHOWN IN PLAN. SPOULTS SPECIFICATIONS MAY BE MADE BY THE ENGINEER. III - CURBING AS PER SITE CONDITIONS AND CONTRACT SPECIFICATIONS.
 10. ABUTMENT AND WING WALL SECTIONS ARE APPLICABLE FOR A MINIMUM BEARING CAPACITY OF THE SOIL OF 10 TONS PER SQUARE METRE. FOR LOWER BEARING CAPACITY THE SECTIONS SHOULD BE ADJUSTED SIMILARLY.

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Project Title
 Consultancy services for preparation of detailed project report for Rehabilitation and upgrading of existing 3-lane road to 4-lane divided carriageway with provision of 100mm - 150mm parapet walls of 100-150 cm height in last (60-70) including drainage (Type) and other works. Project Name: III Progression of North-South corridor Project, Common Package-C2018

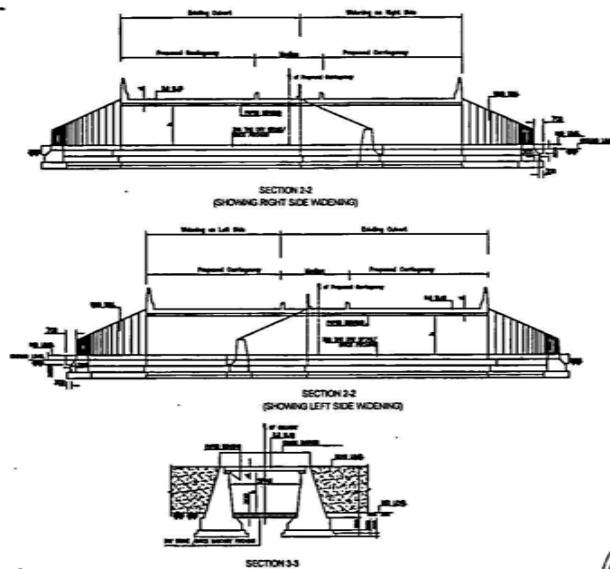
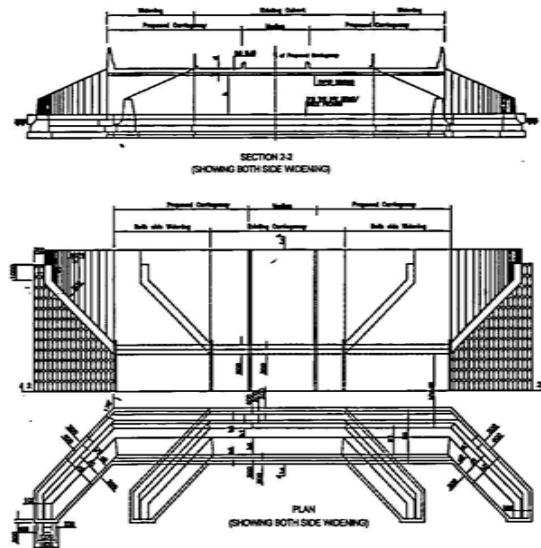
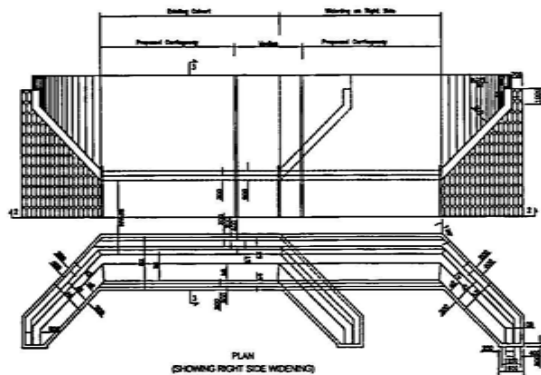
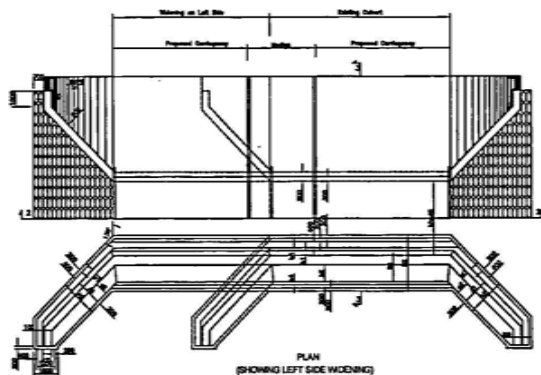
Scale: 1:100
 Date: 2009

NATIONAL HIGHWAYS AUTHORITY OF INDIA
 (Ministry of Shipping, Road Transport & Highways)

Drawing Title: TYPICAL SLAB CULVERTS DETAILS
 Drawing No.: SW/1019/ROADS/MISC-09
 Scale: 1:100
 Date: 2009

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| Revision | Details | By | CHK | Date | Surff |
|----------|---------|----|-----|------|-------|
| | | | | | |
| | | | | | |
| | | | | | |

Project Title
 Consulting services for preparation of detailed project report for Rehabilitation and upgrading of existing Slab culvert to 4-lane divided Contiguity configuration of Jammu - Udhampur section of NH-1 A (km 0.00 to km 64.00) including Jammu/Del/1 Nagar and Udhampur bypass Phase - III Programme of North-South corridor Project, Contract Package C-011

Client
NATIONAL HIGHWAYS AUTHORITY OF INDIA
 (Ministry of Shipping, Road Transport & Highways)

Drawing Title:
 TYPICAL DETAILS OF SLAB CULVERTS WIDENING

Drawing No.: SW/1019/ROADSMISC-09

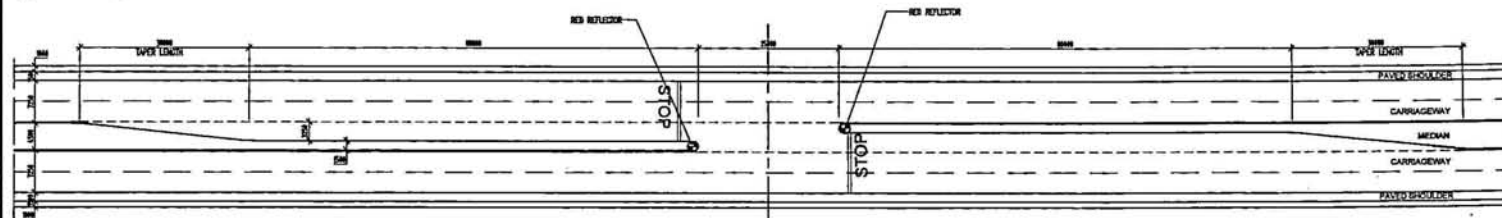
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Date: June-2009

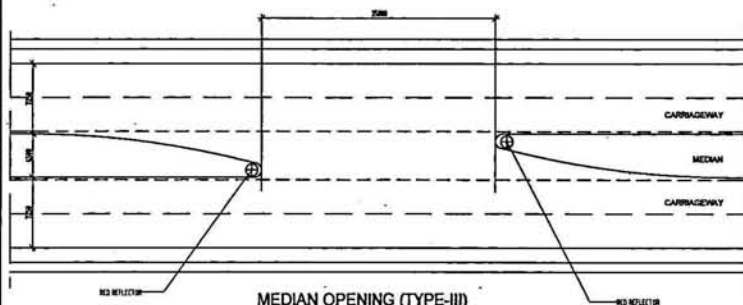
Drawn: K.L.N
Checked: K.PRASAD
Approved: C.D.PUR

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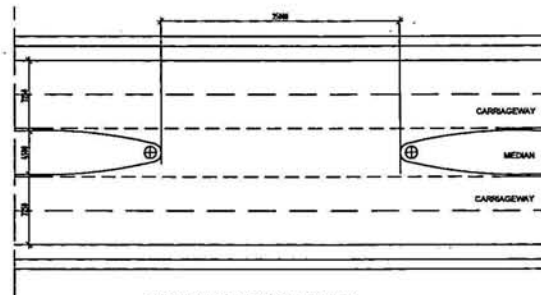




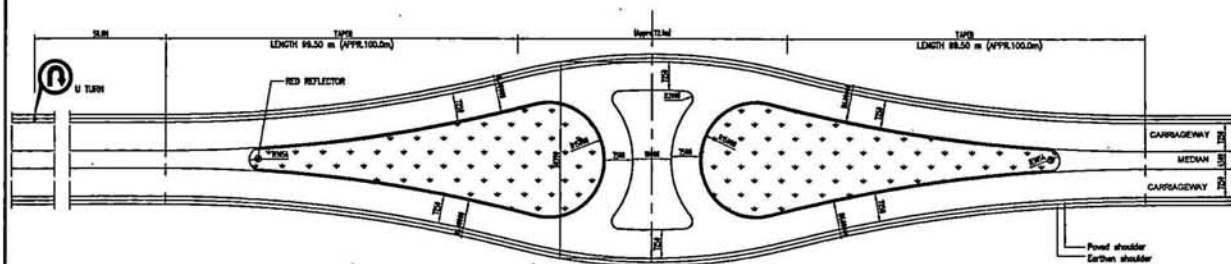
MEDIAN OPENING (TYPE-I)
SCALE: 1:500



MEDIAN OPENING (TYPE-III)
SCALE: 1:300



MEDIAN OPENING (TYPE-II)
SCALE: 1:300



MEDIAN OPENING (TYPE-IV)
(BULB TYPE)
SCALE: 1:750

NOTE:-

1. ALL DIMENSIONS ARE IN mm UNLESS OTHERWISE SPECIFIED.
2. MEDIAN WIDTH SHOULD BE VARY FROM 4.5m TO 1.5m RESPECTIVELY
3. BULB TYPE (TYPE-IV) OF MEDIAN OPENING ARE PROVIDED IN CERTAIN LOCATION WHERE 'U' TURN MAY REQUIRE, AS PER SITE CONDITIONS RADIUS OF BULB AND TAPER LENGTH ARE VARY.
4. TYPE- I MEDIAN OPENING IS GENERALLY USED FOR MAJOR INTER SECTION OF A FOUR LANE DIVIDED CARRIAGEWAY NATIONAL HIGHWAY WITH TWO -LANE NATIONAL/STATE HIGHWAY.

| Revision | By | Date | Scale |
|----------|----|------|-------|
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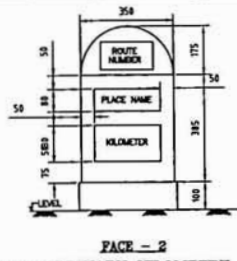
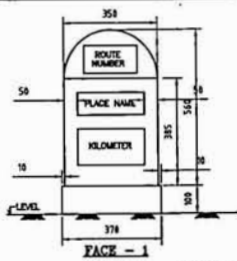
Project Title
Consultancy services for preparation of detailed project report for Rehabilitation and upgrading of existing 2-lane road to 4-lane divided Carriageway configuration of Jammu - Udhampur section of NH-1 A (One 0.00 to km 64.00) including Jammu/UD/UG/NG/MS and Udhampur bypass Phase - III Programme of North - South - corridor Project, Contract Package C-0718

Client
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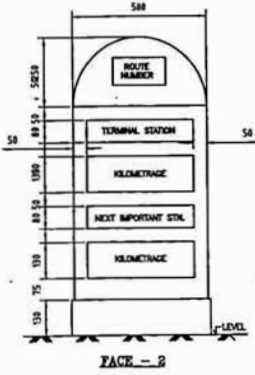
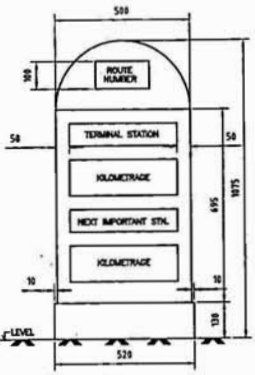
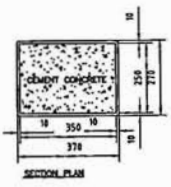
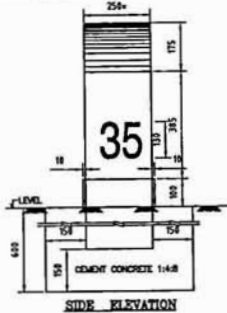
Client
NATIONAL HIGHWAYS AUTHORITY OF INDIA
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Drawing Title
MEDIAN OPENING DETAILS
Drawing No.-SW/1019/ROADS/MISC-12
Scale -- AS SHOWN
Dwg. K.L.N. Dgn. K.PRASAD Appd. C.D.PURB
Date: 26.06.2009

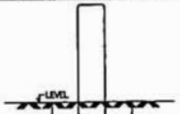
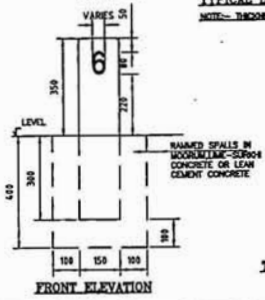
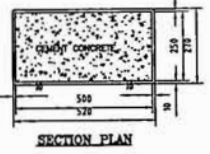
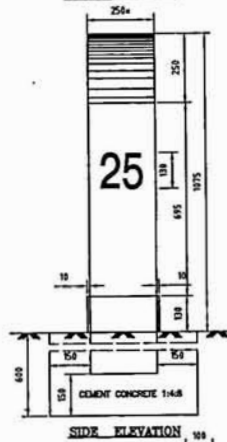
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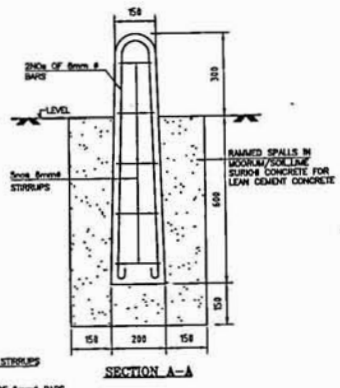
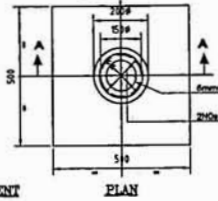
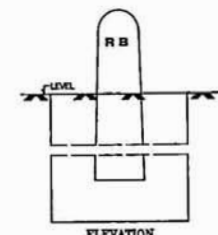
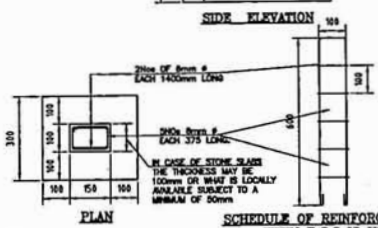
TYPICAL DESIGN OF ORDINARY KILOMETRE STONE
NOTE:- THICKNESS MAY VARY DEPENDING ON THE MATERIAL USED



TYPICAL DESIGN OF 5TH KILOMETRE STONE
NOTE:- THICKNESS MAY VARY DEPENDING ON THE MATERIAL USED



TYPICAL DESIGN OF 200 METRES STONE



TYP. BOUNDARY STONE DESIGN WITH R.C.C AS MATERIAL

| | | | | | |
|----------|---------|----|-----|------|-------|
| Revision | Details | By | Chk | Date | Staff |
| | | | | | |

Project Title
Consultancy services for preparation of detailed project report for Rehabilitation and upgrading of existing 3-lane road to 4-lane divided Carriageway configuration of Jaipur - Udaipur section of NH-1 A (Km 0.00 to Km 64.00) including Jaipur/Fort Jaipur and Udaipur Bypass Phase - II Programme of North-South corridor Project, Contract Package C09/18

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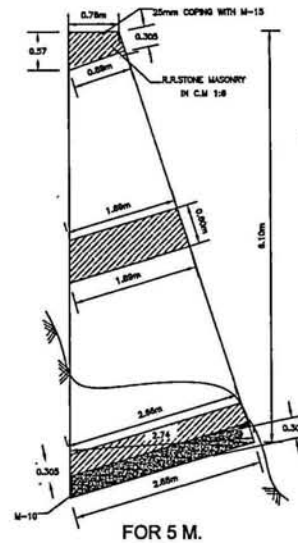
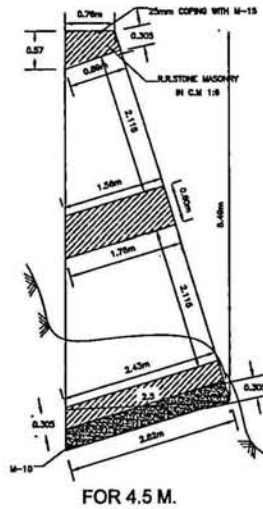
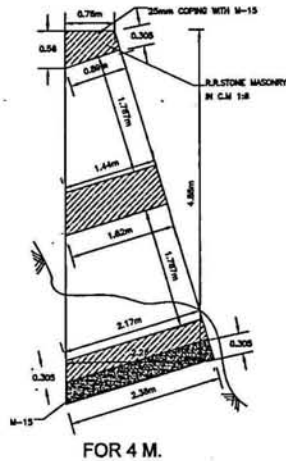
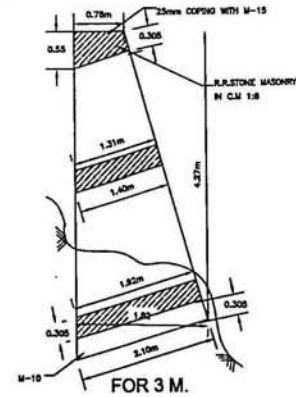
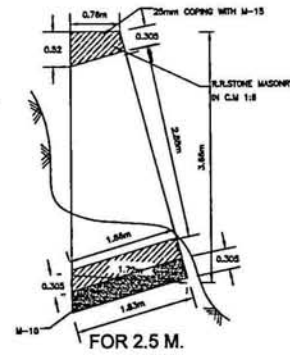
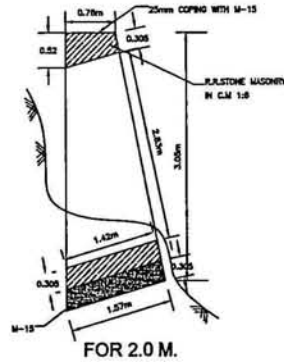
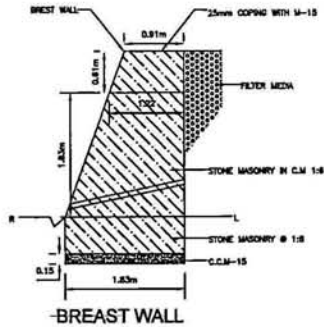
Original Size: A2
Publication: Public Release
Scale: 1:10

Client
NATIONAL HIGHWAYS AUTHORITY OF INDIA
(Ministry of Shipping, Road Transport & Highways)

Drawing Title:
ROAD SIDE FURNITURE DETAILS
Drawing No.: SW/1019/ROADSMISC-13
Scale: 1:10
Dtn: K.L.N. Dgn: V.PRASAD Appd: C.D.PURI Date: June-2009

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Project Title
 Consultancy services for preparation of detailed project report for
 Rehabilitation and upgrading of existing 2-lane road to class divided
 Category road of length of Approx. 150km on section of NH-1 A
 (Dm to km 64.00) including Jammu & Jhelum and Dabgar
 Bypass Phase - II Progression of North - South corridor Project,
 Contract Package C-0218

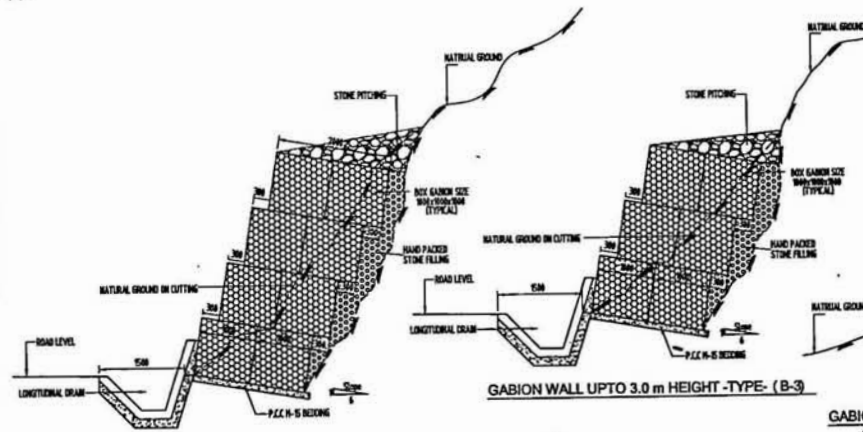
Client
**NATIONAL HIGHWAYS
 AUTHORITY OF INDIA**
 (Ministry of Shipping, Road Transport & Highways)

Drawing Title: **TYPICAL DETAILS OF RETAINING WALL
 AND BREAST WALL**
 Drawing No.: SW/1019/ROADS/MISC-14
 Scale: AS SHOWN
 Dm: K.L.N. Dgn: K.PRASAD Appd: C.D.PURI
 Date: June-2009

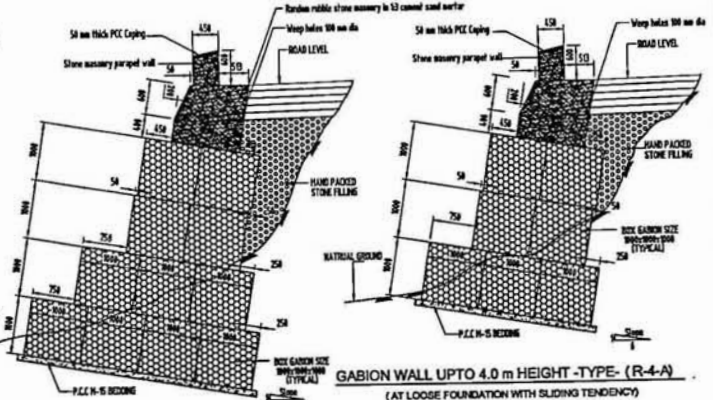
Sheet (1 of 3)
 Date: June-2009

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 NEW DELHI - 110044
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 E-mail: swindia@swindia.com





GABION WALL UPTO 3.0 m HEIGHT -TYPE- (B-3)



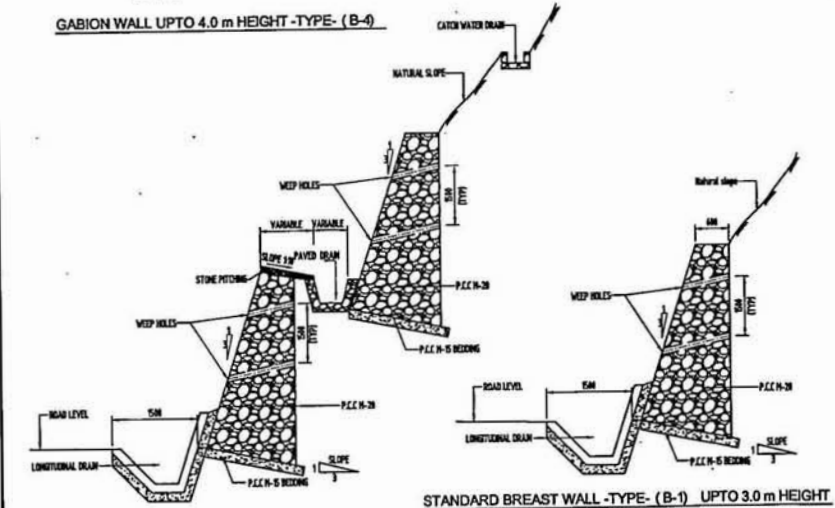
GABION WALL UPTO 4.0 m HEIGHT -TYPE- (R-4-A)

(AT LOOSE FOUNDATION WITH SLIDING TENDENCY)

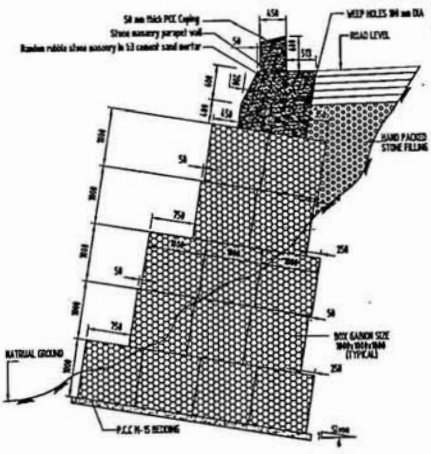
GABION WALL UPTO 5.0 m HEIGHT -TYPE- (R-4-B)

(AT LOOSE FOUNDATION WITH SLIDING TENDENCY)

- Notes:-
1. Parapet stone gable wall shall be 2.0m long each with 0.01m gap between two parapets and shall follow the geometrical edge of the road on valley side.
 2. Top of parapet may be given a slope towards valley side so that these are not used as resting place by pedestrians.
 3. 50 mm THICK P.C.C. capping shall be provided on top of parapets.



STANDARD BREAST WALL -TYPE- (B-1) UPTO 3.0 m HEIGHT



GABION WALL UPTO 6.0 m HEIGHT -TYPE- (R-4-C)

(AT LOOSE FOUNDATION WITH SLIDING TENDENCY)

STANDARD BREAST WALL -TYPE- (B-2) 3.0 TO 6.0m HEIGHT

| Revision | Details | By | CHKD | Date | Suppl. |
|----------|---------|----|------|------|--------|
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Project View
 Consultancy services for preparation of detailed project report for Rehabilitation and upgrading of existing 3-lane road to 4-lane divided carriageway configuration of Jammu - Udhampur section of NH-1 A (km 0.00 to km 66.00) including Jammu(Ur) Flyovers and Udhampur Bypass Phase - III Programme of North - South - corridor Project, Contract Package C-0218

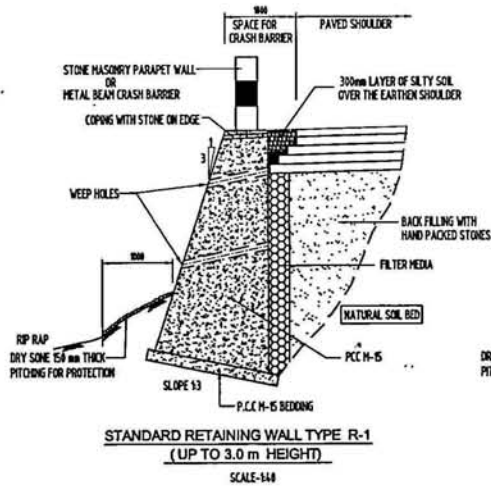
Scale: 1:100
 Date: June-2009

Client
NATIONAL HIGHWAYS AUTHORITY OF INDIA
 (Ministry of Shipping, Road Transport & Highways)

Drawing Title: BREAST WALL, GABION WALL & RETAINING WALL DETAILS
 Drawing No.: SW/1019/ROADS/MISC-14
 Scale: 1:100
 Dwg: K.L.N. Appd: C.D.PURI Date: June-2009

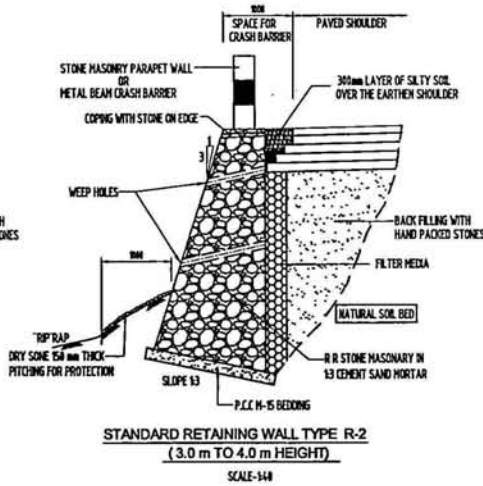
Scale: 1:100
 Smt. Wilson India Private Limited
 A-73/4, MOWAN CO-OPERATIVE INDL ESTATE
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STANDARD RETAINING WALL TYPE R-1
(UP TO 3.0 m HEIGHT)

SCALE-1:40



STANDARD RETAINING WALL TYPE R-2
(3.0 m TO 4.0 m HEIGHT)

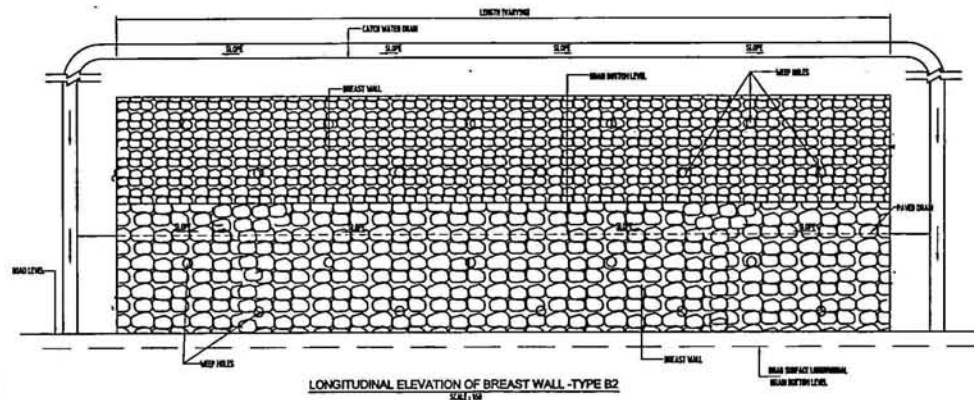
SCALE-1:40

NOTES:
Breast walls

1. ALL DIMENSIONS ARE IN MM UNLESS OTHERWISE SPECIFIED.
2. BREAST WALLS SHALL BE PROVIDED ALONG HILL SIDE SLOPES AT NEAR SPOTS TO GIVE PROTECTION AGAINST HILL SLIDES.
3. IN BREAST WALL CONSTRUCTION A DIP OF THE BASE OF WALL TOWARDS HILL SIDE SHALL BE PROVIDED TO THE EXTENT OF 1:3 SLOPE.
4. BREAST WALL B-1 AND B-2 SHALL BE PROVIDED AT FIRM FOUNDATION AS FAR AS POSSIBLE. BREAST WALL B-3 AND B-4 WITH GABIONS SHALL BE PROVIDED AT LOCATIONS OF POOR FOUNDATIONS /SEEPAGE CONDICTION TO TAKE CARE OF DIFFERENTIAL SETTLE MENTS AND PARTIAL SLOPE MOVEMENTS.
5. STONE PITCHING AT BENCH CUTTING AT TOP OF LOWER BREAST WALL B-2 SHALL BE PROVIDED WITH 1:10 SLOPE TOWARDS HILL SIDE SO THAT RAIN WATER PASSES THROUGH PAVED DRAINS AND THEN DRAINED TO ROAD SIDE DRAIN THROUGH CHUTES AT BOTH ENDS OF LOWER BREAST WALL. KATCHA DRAIN SHALL NOT BE ALLOWED BETWEEN TWO BREAST WALLS.
6. WIDTH OF BENCH CUTTING ON B-2 SHALL BE VARIABLE & SHALL DEPEND ON HILL SLOPE. THIS SHOULD BE ADEQUATE TO DISABLE THE SLOPE SEGMENTS TO ACT INDEPENDENTLY.
7. GABION BODIES OF B-3 & B-4 SHALL BE NORMALLY OF 1.5 X1.5 X1.0 METER SIZE. NET SIZE MAY BE MODIFIED AS PER HEIGHT & BASE REQUIREMENTS. THE BOX GABION STRUCTURE SHALL BE MADE OF HEXAGONAL DOUBLE TWISTED WIRE MESH (EARLY ZINC COATED WILD STEEL) WHICH CAN BE ENDED INTO CELLS BY MEANS OF DIAPHRAGMS. MAXIMUM SIZE OF STONE SHALL BE 300X300X300MM AND SHALL BE DURABLE AND NON POROUS.
8. THE GABIONS OF B-3 & B-4 SHALL BE FILLED WITH STONES/BOULDERS NOT LESS THAN 225X200X150MM.
9. ALL GABIONS SHALL BE CONNECTED TO EACH OTHER ALONG ALL CORNERS WITH THE LACING OPERATION. FOR CONNECTING LACING OPERATION THE WIRE SHALL BE PASSED THROUGH EACH MESH MAKING A DOUBLE TWIST WITH EVERY OTHER MESH.
10. COMPACT ION OF STONES IN GABIONS OF B-3 AND B-4 IS NOT NECESSARY BUT FILING OF CORNERS MUST BE ENSURED.
11. CATCH WATER DRAIN SHALL BE PROVIDED NEAR TO BREAST WALL TO AVOID SEEPAGE OF WATER INTO THE CUT. SLOPES/SEEPAGE SHOULD BE PROVIDED AT TOP OF SLP ZONES SUFFICIENTLY AWAY FROM CROWN POINT OF SLP ZONE.
12. 100 mm SIZE PVC PIPE USED FOR WEEP HOLES IN B-1 & B-2 SHALL BE PROVIDED AT INTERVAL OF 1500mm VERTICALLY AND HORIZONTALLY, (STAGGERED) WITH SLOPE 1:10.

Retaining walls

1. FRONT BUTTER AND BASE SLOPE SHALL BE 1:3.
2. RCC R. WALL B-3 SHALL BE PROVIDED AT FIRM FOUNDATIONS & GABION R. WALL B-4 SHALL BE PROVIDED AT UNDESIRABLE FOUNDATION OR AT LOCATIONS WITH SEEPAGE CONDICTIONS OR WITH TENDENCY OF DIFFERENTIAL SETTLEMENT.
3. B-5 (COUNTER FORT R. WALL) SHALL BE PROVIDED WHERE WIDE BASE IS NOT AVAILABLE OR IN THE APPROACHES OF BRIDGES.
4. 100 MM SIZE PVC PIPE SHALL BE USED FOR WEEP HOLES WHICH SHALL BE PROVIDED AT INTERVAL OF 1500 MM VERTICALLY & HORIZONTALLY (STAGGERED) WITH SLOPE 1:10.
5. FOUNDATION OF R.W. SHALL BE TAKEN DEEP ENOUGH TO REST ON SOLID FOUNDATION MATERIAL, SAFE FROM SEOUR, FROST & SURFACE WATER.
6. FOUNDATION PIT IN FRONT OF THE OF R.W. SHALL BE FILLED SLIGHTLY ABOVE THE ORIGINAL GROUND LEVEL WITH RIP RAP DRY STONE PITCHING IN 1000 MM LENGTH TO PREVENT THE EROSION IN SOFT STRATA.
7. 500 MM RUBBLE PACKING OF HAND PACKED LOOSE STONE OR GRANULAR MATERIAL PACKING IN THE FORM OF FILTER SHALL BE PROVIDED SUBSIDIARING THE WALEY OF WEEP HOLES FOR PROPER DRAINAGE.
8. IN CASE OF B-2, THE FOUNDATION CAN BE STEEPED UP IF HARD ROCK IS ENCOUNTERED AT THE BASE. THE COPING SHALL CONSIST OF STONES ON EDGE WITH LARGE STONES WITH TOP OF COPING SLOPING TOWARDS VALLEY SIDE.
9. FILTER MEDIA SHALL BE PROVIDED BEHIND R. WALLS TO DRAIN OFF GROUND WATER TABLE OR RAINWATER SEEPAGE WHICH SHALL CONFORM TO (APPENDIX-4) OF IRC : 78-2000.
10. IMPERVIOUS SILTY SOIL LAYER 300 MM THICK SHALL BE PROVIDED ON TOP OF EARTHEN SHOULDER TO PREVENT SEEPAGE OF RAINWATER WITHIN THE BACK FILL.
11. R.W. TOP SHALL BE KEPT SLIGHTLY LOWER THAN THE SHOULDERS AND SHALL BE SLOPING OUTWARD TO VALLEY SIDE SO THAT WATER RUNS OVER THE WALL INSTEAD OF STEPPING INTO THE BACKFILL.
12. BACK FILL LAYER IMMEDIATELY BEHIND THE R.WALL SHALL CONSIST OF HAND PACKED STONES OR SOME GRANULAR MATERIAL. REMAINDER OF BACK FILL SHALL BE RANMED IN 150 MM THICK LAYERS SLOPING TOWARDS THE BACK OF THE WALL. THE TOP SURFACE OF BACK FILL SHALL BE SLOPED TO PROVIDE FREE SEEPAGE OF WATER BEHIND THE RETAINING WALL.
13. B-1 RETAINING WALL SHALL BE IN PCC M-15 WITH MINIMUM GABION REINFORCEMENT OF 250 mm² OF STEEL AREA PER METER IN EACH DIRECTION WITH MAXIMUM SPACING 300 mm.



LONGITUDINAL ELEVATION OF BREAST WALL -TYPE B2
SCALE: 1:40

| | | | |
|------|------|--------|-------|
| 1000 | 1000 | 100000 | 1000 |
| By | Chk | Draft | Scale |

Project Title
Consultancy services for preparation of detailed project report for Rehabilitation and upgrading of existing 3-lane road to 4-lane divided carriageway configurations of Jammu - Dhamra section of NH-1 A (km 0.00 to km 44.00) including Jammu (P) & Nagrota and Dhamra Bypass Phase - III Programme of North - South corridor Project, Contract Package C-29/18

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Client: **NATIONAL HIGHWAYS AUTHORITY OF INDIA**
(Ministry of Shipping, Road Transport & Highways)

Client:
NATIONAL HIGHWAYS AUTHORITY OF INDIA
(Ministry of Shipping, Road Transport & Highways)

Drawing Title: **BREAST WALL, GABION WALL & RETAINING WALL DETAILS**
Drawing No.: **SWM/1018/ROADS/MISC-14**
Scale: **1:40**
Dwn: **K.L.N**
Dgn: **K.PRASAD**
Appd: **C.D.PURI**
Date: **June-2009**

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NOTES:

Breast walls

- ALL DIMENSIONS ARE IN MM OTHERWISE SPECIFIED.
- BREAST WALLS SHALL BE PROVIDED ALONG HILL SIDE SLOPES AT NEAR SPOTS TO GIVE PROTECTION AGAINST HILL SLIDES.
- IN BREAST WALL CONSTRUCTION A DIP OF THE BASE OF WALL TOWARDS HILL SIDE SHALL BE PROVIDED TO THE EXTENT OF 1:3 SLOPE.
- BREAST WALL B-1 AND B-2 SHALL BE PROVIDED AT FIRM FOUNDATION AS FAR AS POSSIBLE. BREAST WALL B-3 AND B-4 WITH GABIONS SHALL BE PROVIDED AT LOCATIONS OF POOR FOUNDATIONS /SEEPAGE CONDITION TO TAKE CARE OF DIFFERENTIAL SETTLE MENTS AND PARTIAL SLOPE MOVEMENTS.
- STONE PITCHING AT BENCH CUTTING AT TOP OF LOWER BREAST WALL B-2 SHALL BE PROVIDED WITH 1:10 SLOPE TOWARDS HILL SIDE SO THAT RAIN WATER PASSES THROUGH PAVED DRAINS AND THEN DRAINED TO ROAD SIDE DRAIN THROUGH CHUTES AT BOTH ENDS OF LOWER BREAST WALL. KATCHHA DRAIN SHALL NOT BE ALLOWED BETWEEN TWO BREAST WALLS.
- WIDTH OF BENCH CUTTING IN B-2 SHALL BE VARIABLE & SHALL DEPEND ON HILL SLOPE. THIS SHOULD BE ADEQUATE TO DOUBLE THE SLOPE SEGMENTS TO ACT INDEPENDENTLY.
- GABION BOXES OF B-3 & B-4 SHALL BE NORMALLY OF 1.5 X1.5 X1.0 METER SIZE BUT SIZE MAY BE MODIFIED AS PER HEIGHT & BASE REQUIREMENTS. THE BOX AND ION STRUCTURE SHALL BE MADE OF HEXAGONAL DOUBLE TWISTED WIRE MESH (HEAVILY ZINC COATED MILD STEEL) WHICH CAN BE DIVIDED INTO CELLS BY MEANS OF DIAPHRAGMS. MINIMUM SIZE OF STONE SHALL BE 300X200X100MM AND SHALL BE DURABLE AND NON POROUS.
- THE GABIONS OF B-3 & B-4 SHALL BE FILLED WITH STONES/BOLLIDERS NOT LESS THAN 225X200X150MM.
- ALL GABIONS SHALL BE CONNECTED TO EACH OTHER ALONG ALL CORNERS WITH THE LACING OPERATION. FOR CONNECTING LACING OPERATION THE WIRE SHALL BE PASSED THROUGH EACH MESH MAKING A DOUBLE TWIST WITH EVERY OTHER MESH.
- COMPACT ION OF STONES IN GABIONS OF B-3 & B-4 IS NOT NECESSARY BUT FILLING OF CORNERS MUST BE ENSURED.
- CATCH WATER DRAIN SHALL BE PROVIDED NEAR TO BREAST WALL TO AVOID SEEPAGE OF WATER INTO THE CUT SLOPE. THESE SHOULD BE PROVIDED AT TOP OF SLP ZONES SUFFICIENTLY AWAY FROM CROWN POINT OF SLP ZONE.
- 100 mm size PVC PIPE USED FOR WEEP HOLES IN B-1 & B-2 SHALL BE PROVIDED AT INTERVAL OF 1500mm VERTICALLY AND HORIZONTALLY, (STAGGERED) WITH SLOPE 1:10.

Retaining walls

- FRONT BATTER AND BASE SLOPE SHALL BE 1:3.
- RCC R. WALL B-3 SHALL BE PROVIDED AT FIRM FOUNDATIONS & GABION R. WALL B-4 SHALL BE PROVIDED AT UNSTABLE FOUNDATION OR AT LOCATIONS WITH SEEPAGE CONDITIONS OR WITH TENDENCY OF DIFFERENTIAL SETTLEMENT.
- B-5 (COUNTER FORT RCC R.WALL) SHALL BE PROVIDED WHERE WIDE BASE IS NOT AVAILABLE OR IN THE APPROACHES OF BRIDGES.
- 100 MM SIZE PVC PIPE SHALL BE USED FOR WEEP HOLES WHICH SHALL BE PROVIDED AT INTERVAL OF 1500 MM VERTICALLY & HORIZONTALLY (STAGGERED) WITH SLOPE 1:10.
- FOUNDATION OF R.W. SHALL BE TAKEN DEEP ENOUGH TO REST ON SOUND FOUNDATION MATERIAL, SAFE FROM SECCUR FROST & SURFACE WATER.
- FOUNDATION PIS IN FRONT OF TOE OF R.W. SHALL BE FILLED SLIGHTLY ABOVE THE ORIGINAL GROUND LEVEL WITH R.P. RVP DRY STONE PITCHING IN 1000 MM LENGTH TO PREVENT THE EROSION IN SOFT STRATA.
- 500 MM RUBBLE PACKING OF HAND PACKED LOOSE STONE OR GRANULAR MATERIAL PACKING IN THE FORM OF FILTER SHALL BE PROVIDED SURROUNDING THE INLET OF WEEP HOLES FOR PROPER DRAINAGE.
- IN CASE OF B-2, THE FOUNDATION CAN BE STEPPED UP IF HARD ROCK IS ENCOUNTERED AT THE BASE. THE COPING SHALL CONSIST OF STONES ON EDGE WITH LARGE STONES WITH TOP OF COPING SLOPING TOWARDS VALLEY SIDE.
- FILTER MEDIA SHALL BE PROVIDED BEHIND R. WALLS TO DRAIN OFF GROUND WATER TABLE OR RAINWATER SEEPAGE WHICH SHALL CONFORM TO (APPENDIX-6) OF IRC : 79-2000.
- IMPERVIOUS SILTY SOIL LAYER 300 MM THICK SHALL BE PROVIDED ON TOP OF EARTHEN SHOULDER TO PREVENT SEEPAGE OF RAINWATER WITHIN THE BACK FILL.
- R.W. TOP SHALL BE KEPT SLIGHTLY LOWER THAN THE SHOULDERS AND SHALL BE SLOPING OUTWARD TO VALLEY SIDE SO THAT WATER RUNS OVER THE WALL INSTEAD OF SEEPING INTO THE BACKFILL.
- BACK FILL LAYER IMMEDIATELY BEHIND THE R.WALL, SHALL CONSIST OF HAND PACKED STONES OR SOME GRANULAR MATERIAL. REMAINDER OF BACK FILL SHALL BE SHOWN IN 150 MM THICK LAYERS SLOPING TOWARDS THE BACK OF THE WALL. THE TOP SURFACE OF BACK FILL SHALL BE SOILED TO PREVENT DIRECT SEEPAGE OF WATER BEHIND THE RETAINING WALL.
- B-1 RETAINING WALL SHALL BE IN RCC R-15 WITH NOMINAL SURFACE REINFORCEMENT OF 250 mm² OF STEEL AREA PER METER IN EACH DIRECTION WITH MAXIMUM SPACING 300 mm c/c.

| Revision Details | By | CHK | Date | Staff |
|------------------|----|-----|------|-------|
| | | | | |
| | | | | |
| | | | | |
| | | | | |

Project Title
 Consultancy services for preparation of detailed project report for Rehabilitation and upgrading of existing 3 lane road to 4 lane divided. Contiguity construction of Access - 1 (Delhiana section of NH-1 A. Dist. Gurgaon in line 44.00) including Access (2nd) Highway and (1) Highway Bypass Phase - II Programme of North-South corridor project, Contract Package C-4219

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 Original title: 02
 Project title: 02
 Plot No: 02

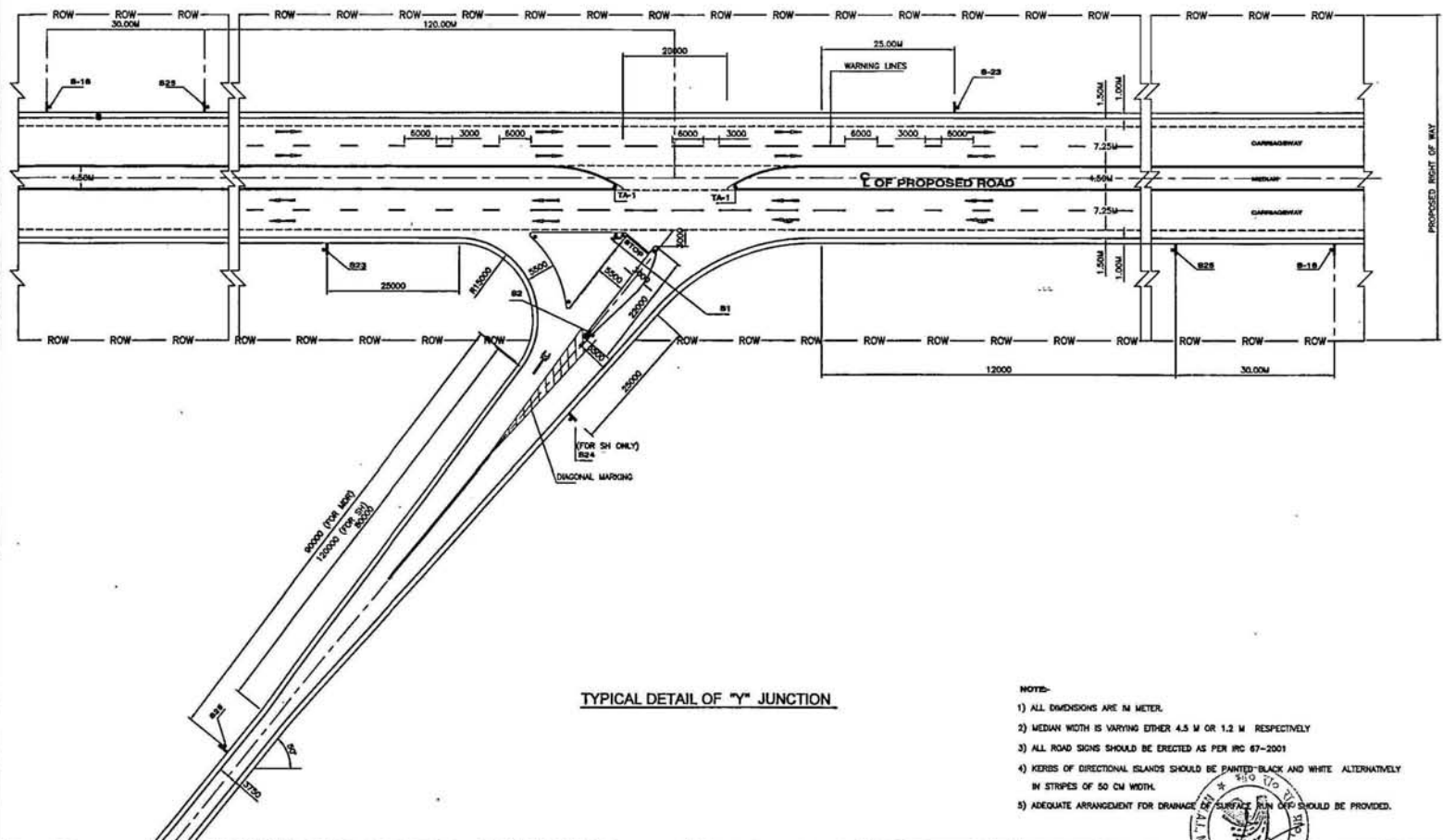
Client
**NATIONAL HIGHWAYS
 AUTHORITY OF INDIA**
 (Ministry of Shipping, Road Transport & Highways)

Drawing Title: BREAST WALL, GABION WALL & RETAINING WALL DETAILS
 Drawing No.: SW/M/019/ROADS/MISC-14
 Scale: 1/4" = 1' AS SHOWN
 Date: K.L.N. Dgn. K.P.RASAD Appd. C.O.PURN
 Issue No.: 2008
 Date: 2008
 Location: DELHI

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204



TYPICAL DETAIL OF "Y" JUNCTION

- NOTE-
- 1) ALL DIMENSIONS ARE IN METER.
 - 2) MEDIAN WIDTH IS VARYING EITHER 4.5 M OR 1.2 M RESPECTIVELY
 - 3) ALL ROAD SIGNS SHOULD BE ERRECTED AS PER IRC 67-2001
 - 4) KERBS OF DIRECTIONAL ISLANDS SHOULD BE PAINTED BLACK AND WHITE ALTERNATIVELY IN STRIPES OF 50 CM WIDTH.
 - 5) ADEQUATE ARRANGMENT FOR DRAINAGE OF SURFACE RUN OFF SHOULD BE PROVIDED.

| | | | | | |
|----------|---------|----|-----|------|------|
| Revision | Details | By | CHK | Date | Surf |
| | | | | | |
| | | | | | |
| | | | | | |

Project Title
 Consulting services for preparation of detailed project report for Rehabilitation and upgrading of existing 2-lane road to 4-lane divided Carriageway and upgrading of Jammu - Udhampur section of NH-1 A (See 0.00 to 66.00) including Jammu (Pur) Bypass and Udhampur Bypass Phase - III Programme of North-South corridor Project, Contract Package C-1874

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Original Size: A2
 Plot Scale: 1:1000
 Date: 2009

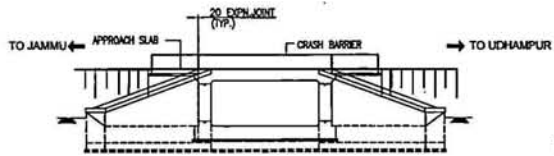
Client
NATIONAL HIGHWAYS AUTHORITY OF INDIA
 (Ministry of Shipping, Road Transport & Highways)

Drawing Title: TYPICAL DETAIL OF "Y" INTERSECTION FOR SH MDR
 Drawing No.: SW/1019/ROADS/MISC-16
 Scale: 1:1500
 Dwn: K.L.N
 Dgn: K.PRASAD
 Appd: C.D.PURI
 Date: June-2009

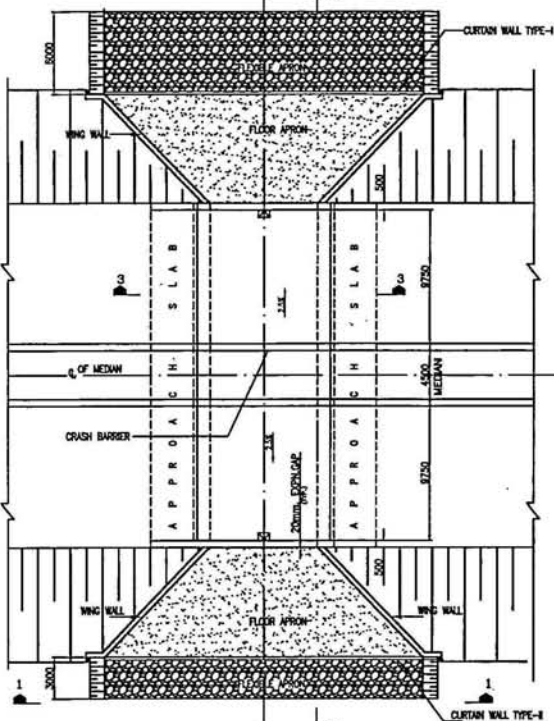


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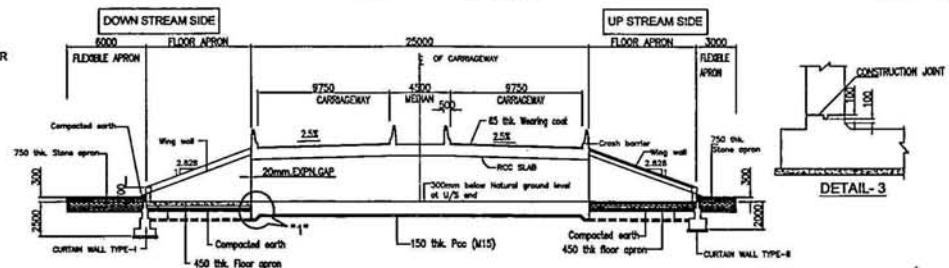




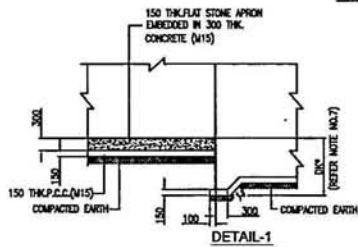
VIEW "1-1"



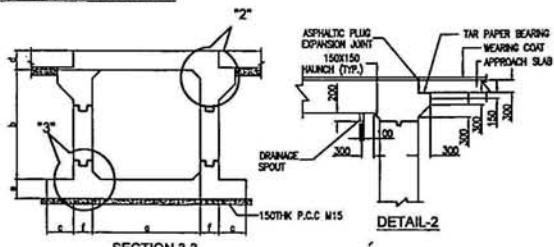
PLAN



TYPICAL CROSS SECTION "2-2"



DETAIL-1



DETAIL-2

SECTION 3-3

TABLE SHOWING SALENT DIMENSIONS OF SINGLE CELL BOX CULVERT

| Box Cell Designation | a (mm) | b (mm) | c (mm) | d (mm) | e (mm) | f (mm) | Required bearing capacity category | Design base pressure (T/M ²) |
|----------------------|--------|--------|--------|--------|--------|--------|------------------------------------|--|
| 1/22/0 | 2000 | 2000 | 500 | 350 | 380 | 300 | c | 11.52 |
| 1/23/0 | 2000 | 3000 | 1300 | 370 | 420 | 350 | c | 11.01 |
| 1/33/0 | 3000 | 3000 | 900 | 420 | 420 | 420 | c | 10.5 |
| 1/34/0 | 3000 | 4000 | 1700 | 450 | 480 | 480 | c | 11.37 |
| 1/43/0 | 4000 | 3000 | 400 | 450 | 500 | 500 | c | 10.53 |
| 1/44/0 | 4000 | 4000 | 1200 | 480 | 550 | 550 | c | 11.07 |
| 1/45/0 | 4000 | 5000 | 1900 | 570 | 620 | 630 | c | 12.45 |
| 1/53/0 | 5000 | 3000 | 300 | 500 | 570 | 570 | B | 9.83 |
| 1/54/0 | 5000 | 4000 | 800 | 520 | 600 | 620 | c | 10.66 |
| 1/55/0 | 5000 | 5000 | 1500 | 580 | 680 | 700 | c | 12.40 |
| 1/63/0 | 6000 | 3000 | 300 | 680 | 720 | 720 | B | 9.88 |
| 1/64/0 | 6000 | 4000 | 300 | 690 | 720 | 750 | c | 10.93 |
| 1/65/0 | 6000 | 5000 | 1100 | 690 | 750 | 750 | c | 11.77 |
| 1/66/0 | 6000 | 6000 | 1800 | 700 | 800 | 850 | c | 13.21 |
| 1/75/0 | 7000 | 5000 | 600 | 750 | 850 | 850 | c | 11.59 |
| 1/76/0 | 7000 | 6000 | 1200 | 780 | 920 | 950 | c | 13.00 |
| 1/77/0 | 7000 | 7000 | 1300 | 780 | 950 | 950 | c | 13.10 |
| 1/85/0 | 8000 | 5000 | 300 | 800 | 950 | 950 | c | 11.37 |
| 1/86/0 | 8000 | 6000 | 500 | 820 | 970 | 1000 | c | 12.52 |
| 1/87/0 | 8000 | 7000 | 1300 | 850 | 1100 | 1200 | c | 14.36 |

NOTES:-

1. ALL DIMENSIONS ARE IN MILLIMETERS UNLESS OTHERWISE SPECIFIED.
2. DIMENSIONS ARE NOT TO BE SCALED. ONLY WRITTEN DIMENSIONS SHALL BE FOLLOWED
3. SOIL DENSITY ON THE BACKFILL SHOULD CONFORM TO CLAUSE 305.2.1.5 OF MOST SPECIFICATION.
4. SOFT AND LOOSE PATCHES IN THE BEARING AREA ARE TO BE REPLACED BY COMPACTED GRANULAR FILLS WITH LAYERS NOT EXCEEDING 300mm.
5. DESIGN FOR THE BOX CELL STRUCTURE ONLY. THESE HAVE NO BEARING WITH DESIGN OF EMBANKMENT WHICH WILL BE TAKEN UP BY ENGINEER-IN-CHARGE SEPARATELY
6. "DK" IS DEPTH OF KEY AT BASE SLAB UPTO 900mm VALUE OF "DK" 1200mm
GREATER THAN 900mm = +300mm (= Base slab thickness)



Project Title
Consultancy services for the preparation of detailed project report for Rehabilitation and upgrading of existing 2-lane road to 4-lane divided Carriageway and upgrading of Jammu - Udhampur section of NH-1A (Km 0.00 to Km 64.00) including Jammu/Ur/ Nagrota and Udhampur bypass Phase - III Programme of North-South corridor Project, Contract Package C-02/18

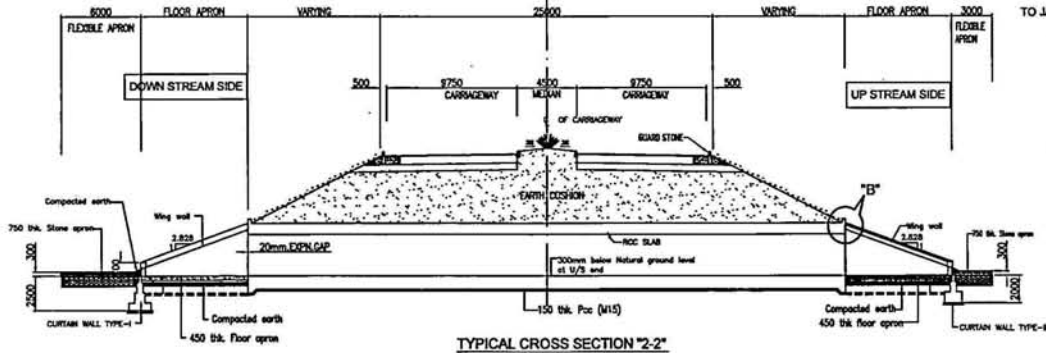
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Client
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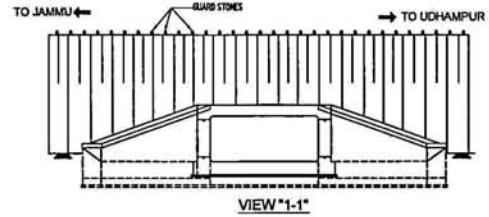
Drawing Title: TYPICAL BOX CULVERT DETAILS WITHOUT EARTH CUSHION
Drawing No.: SW/1019/ROADS/MISC-17
Scale: 1:50
Date: June-2009

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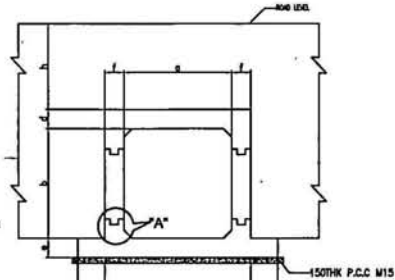




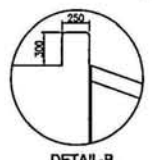
TYPICAL CROSS SECTION "2-2"



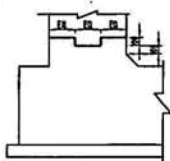
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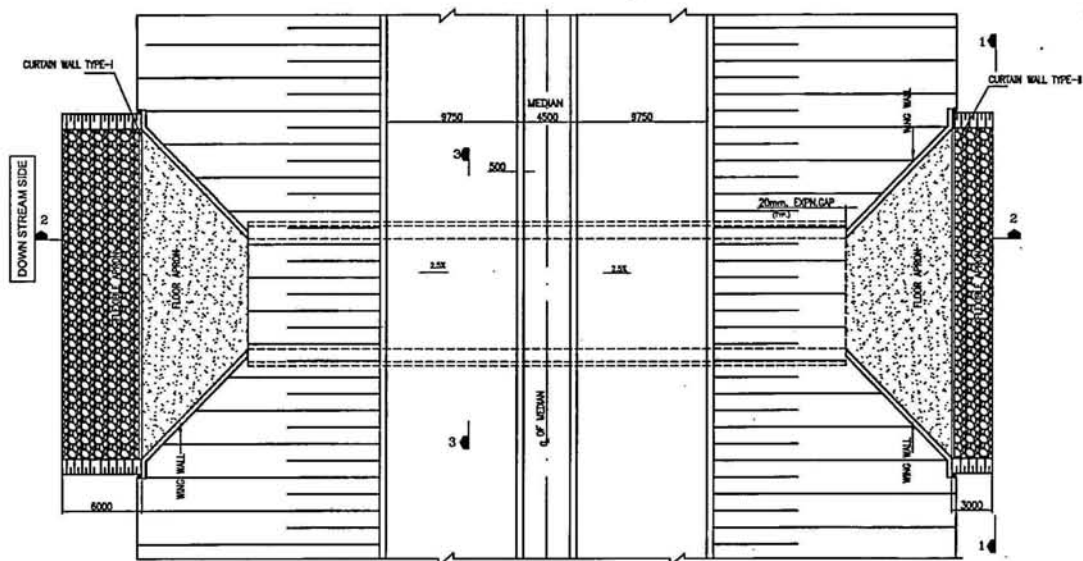
SECTION 3-3



DETAIL-B



DETAIL-A



PLAN ROAD LEVEL

| | | | | |
|------------------|----|-----|------|-------|
| Revision Details | By | Chk | Date | Scale |
| | | | | |

Project Title
 Consultancy services for preparation of detailed project report for Rehabilitation and upgrading of existing 2-lane road to 4-lane divided Carriageway configuration of Jammu - Udhampur section of NH-1-A (On 0.00 to Km 64.00) including Jammu(Pur) Bypass and Udhampur Bypass Phase - III Programme of Work - South - corridor Project, Contract Package C-09/18

Client
NATIONAL HIGHWAYS AUTHORITY OF INDIA
 (Ministry of Shipping, Road Transport & Highways)

Client
NATIONAL HIGHWAYS AUTHORITY OF INDIA
 (Ministry of Shipping, Road Transport & Highways)

Drawing Title: TYPICAL BOX CULVERT DETAILS WITH EARTH CUSHION
Drawing No.: SWW1019/ROADS/MISC-17
Scale: 1:20
Drawn: K.L.N
Checked: K.PRASAD
Appd: C.O.PUR
Date: June-2009

Scale: 1:20
Sheet: 3 of 4
Date: June-2009
Scale: 1:20
Sheet: 3 of 4
Date: June-2009



TABLE SHOWING SAILENT DIMENSIONS OF SINGLE CELL BOX CULVERT

| Box Cell Designation No/ab/Ec | a (mm) | b (mm) | c (mm) | d (mm) | e (mm) | f (mm) | h (mm) | Required bearing capacity Category | Design base pressure (T/M ²) |
|-------------------------------|--------|--------|--------|--------|--------|--------|--------|------------------------------------|--|
| 1/22/3 | 2000 | 2000 | 600 | 250 | 300 | 250 | 3000 | C | 8.80 |
| 1/22/4 | 2000 | 2000 | 500 | 280 | 350 | 280 | 4000 | C | 10.17 |
| 1/22/5 | 2000 | 2000 | 500 | 300 | 350 | 300 | 5000 | C | 11.31 |
| 1/23/3 | 2000 | 3000 | 1100 | 280 | 300 | 350 | 3000 | C | 10.59 |
| 1/23/4 | 2000 | 3000 | 1000 | 320 | 370 | 400 | 4000 | C | 12.17 |
| 1/23/5 | 2000 | 3000 | 1100 | 320 | 350 | 380 | 5000 | C | 13.14 |
| 1/33/3 | 3000 | 3000 | 800 | 370 | 450 | 400 | 3000 | C | 10.35 |
| 1/33/4 | 3000 | 3000 | 750 | 380 | 470 | 420 | 4000 | C | 11.57 |
| 1/33/5 | 3000 | 3000 | 750 | 420 | 500 | 420 | 5000 | C | 12.86 |
| 1/34/3 | 3000 | 4000 | 1400 | 370 | 470 | 480 | 3000 | C | 12.17 |
| 1/34/4 | 3000 | 4000 | 1400 | 400 | 550 | 500 | 4000 | C | 13.68 |
| 1/34/5 | 3000 | 4000 | 1300 | 450 | 550 | 520 | 5000 | C | 14.85 |
| 1/43/3 | 4000 | 3000 | 500 | 470 | 550 | 420 | 3000 | C | 10.12 |
| 1/43/4 | 4000 | 3000 | 400 | 500 | 600 | 450 | 4000 | C | 11.42 |
| 1/43/5 | 4000 | 3000 | 300 | 570 | 670 | 500 | 5000 | C | 12.88 |
| 1/44/3 | 4000 | 4000 | 1000 | 470 | 600 | 550 | 3000 | C | 11.87 |
| 1/44/4 | 4000 | 4000 | 1000 | 500 | 600 | 550 | 4000 | C | 13.07 |
| 1/44/5 | 4000 | 4000 | 1000 | 500 | 650 | 550 | 5000 | C | 14.31 |
| 1/45/3 | 4000 | 5000 | 1600 | 450 | 600 | 630 | 3000 | C | 13.85 |
| 1/45/4 | 4000 | 5000 | 1600 | 500 | 650 | 650 | 4000 | D | 15.01 |
| 1/45/5 | 4000 | 5000 | 1500 | 550 | 700 | 700 | 5000 | D | 16.38 |
| 1/53/3 | 5000 | 3000 | 300 | 550 | 650 | 500 | 3000 | C | 10.11 |
| 1/53/4 | 5000 | 3000 | 300 | 620 | 700 | 520 | 4000 | C | 11.54 |
| 1/53/5 | 5000 | 3000 | 300 | 670 | 770 | 550 | 5000 | C | 12.98 |
| 1/54/3 | 5000 | 4000 | 800 | 550 | 700 | 530 | 3000 | C | 11.56 |
| 1/54/4 | 5000 | 4000 | 700 | 600 | 750 | 570 | 4000 | C | 12.90 |
| 1/54/5 | 5000 | 4000 | 600 | 650 | 800 | 600 | 5000 | C | 14.23 |
| 1/55/3 | 5000 | 5000 | 1400 | 550 | 750 | 650 | 3000 | D | 13.40 |
| 1/55/4 | 5000 | 5000 | 1200 | 600 | 780 | 700 | 4000 | C | 14.66 |
| 1/55/5 | 5000 | 5000 | 1200 | 650 | 820 | 730 | 5000 | C | 16.04 |

| Box Cell Designation No/ab/Ec | a (mm) | b (mm) | c (mm) | d (mm) | e (mm) | f (mm) | h (mm) | Required bearing capacity Category | Design base pressure (T/M ²) |
|-------------------------------|--------|--------|--------|--------|--------|--------|--------|------------------------------------|--|
| 1/63/3 | 6000 | 3000 | 300 | 650 | 750 | 580 | 3000 | C | 10.39 |
| 1/63/4 | 6000 | 3000 | 300 | 750 | 850 | 630 | 4000 | C | 12.06 |
| 1/63/5 | 6000 | 3000 | 300 | 820 | 900 | 670 | 5000 | C | 13.51 |
| 1/64/3 | 6000 | 4000 | 400 | 700 | 800 | 580 | 3000 | C | 11.47 |
| 1/64/4 | 6000 | 4000 | 300 | 750 | 850 | 630 | 4000 | C | 12.80 |
| 1/64/5 | 6000 | 4000 | 300 | 800 | 950 | 700 | 5000 | C | 12.40 |
| 1/65/3 | 6000 | 5000 | 1000 | 650 | 850 | 680 | 3000 | C | 13.05 |
| 1/65/4 | 6000 | 5000 | 900 | 650 | 900 | 720 | 4000 | C | 14.39 |
| 1/65/5 | 6000 | 5000 | 850 | 770 | 1000 | 750 | 5000 | D | 15.93 |
| 1/66/3 | 6000 | 6000 | 1500 | 650 | 900 | 850 | 3000 | C | 14.87 |
| 1/66/4 | 6000 | 6000 | 1400 | 700 | 1000 | 880 | 4000 | D | 16.29 |
| 1/66/5 | 6000 | 6000 | 1250 | 750 | 1050 | 950 | 5000 | D | 17.58 |
| 1/75/3 | 7000 | 5000 | 700 | 750 | 1000 | 700 | 3000 | C | 12.97 |
| 1/75/4 | 7000 | 5000 | 500 | 850 | 1050 | 770 | 4000 | C | 14.35 |
| 1/75/5 | 7000 | 5000 | 400 | 920 | 1150 | 800 | 3000 | D | 15.82 |
| 1/76/3 | 7000 | 6000 | 1300 | 750 | 1100 | 800 | 3000 | C | 14.86 |
| 1/76/4 | 7000 | 6000 | 1100 | 820 | 1100 | 880 | 4000 | D | 16.06 |
| 1/76/5 | 7000 | 6000 | 1100 | 870 | 1150 | 900 | 5000 | D | 17.48 |
| 1/77/3 | 7000 | 7000 | 1700 | 750 | 1100 | 1000 | 3000 | D | 16.56 |
| 1/77/4 | 7000 | 7000 | 1600 | 820 | 1150 | 1050 | 4000 | D | 17.95 |
| 1/77/5 | 7000 | 7000 | 1500 | 900 | 1200 | 1100 | 5000 | D | 19.37 |
| 1/85/3 | 8000 | 5000 | 300 | 870 | 1100 | 750 | 3000 | C | 12.83 |
| 1/85/4 | 8000 | 5000 | 0 | 950 | 1200 | 850 | 4000 | C | 14.18 |
| 1/85/5 | 8000 | 5000 | 0 | 1100 | 1300 | 900 | 5000 | D | 16.00 |
| 1/86/3 | 8000 | 6000 | 800 | 850 | 1200 | 900 | 3000 | C | 14.55 |
| 1/86/4 | 8000 | 6000 | 700 | 950 | 1250 | 950 | 4000 | D | 15.99 |
| 1/86/5 | 8000 | 6000 | 600 | 1050 | 1300 | 1000 | 5000 | D | 17.44 |
| 1/87/3 | 8000 | 7000 | 1300 | 850 | 1200 | 1050 | 3000 | D | 16.19 |
| 1/87/4 | 8000 | 7000 | 1200 | 950 | 1350 | 1100 | 4000 | D | 17.90 |
| 1/87/5 | 8000 | 7000 | 1100 | 1050 | 1400 | 1150 | 5000 | D | 19.35 |

NOTES:-

1. SAFE BEARING CAPACITY REQUIRED FOR SOIL IS DIVIDED IN FOUR CATEGORIES AS SOIN

| Category | TONE/M ² | Category | TONE/M ² |
|----------|---------------------|----------|---------------------|
| A | 5 T/M ² | C | 15 T/M ² |
| B | 10 T/M ² | D | 20 T/M ² |

2. ALL CASES WITH EARTH CUSHION HEIGHTS OF 3m TO 5m ARE FOR DRAW DOWN CONDITION.

3. FOR EARTH CUSHION BETWEEN 3m AND 4m, BOX WITH 4m EARTH CUSHION IS TO BE USED AND FOR EARTH CUSHION BETWEEN 4 m TO 5m, BOX WITH 5m EARTH CUSHION IS TO BE USED. NO RECOMMENDATION IS GIVEN FOR BOX CULVERT WITH EARTH CUSHION HEIGHT IN BETWEEN 0m TO 3m

4. IN CASE ALLOWABLE SOIL BEARING CAPACITY IS LESS THAN ACTUAL BEARING PRESSURE, THE SOIL SHOULD BE STABILISED TO ACHIEVE SAME.

5. FOR BOX CELL STRUCTURES WITH EARTH CUSHION, NO WEARING COAT SHALL BE PROVIDED.

6. INVERT LEVEL OF TOP SURFACE OF BOTTOM SLAB IS ASSUMED AS BED LEVEL.

MATERIAL SPECIFICATIONS CONCRETE

| | MODERATE CONDITION | SEVERE CONDITION |
|------------------------|--------------------|------------------|
| (a) BOX CELL STRUCTURE | M20 | M25 |
| (b) WING WALL | M20 | M20 |
| (c) CURTAIN WALL | M15 | M20 |
| (d) LEVELLING COURSE | M15 | M15 |

7. BOX CELL DESIGNATION IS No/ab/Ec STANDS FOR

No of CELLS/CLEAR WIDTH-CLEAR HEIGHT/HEIGHT OF EARTH CUSHION.

8. ALL REINFORCEMENT SHALL BE HIGH-YIELD STRENGTH DEFORMED BARS (GRADE DESIGNATION S-415) CONFORMING TO IS:1786

| Revision Details | NO | DATE | BY | REVISION |
|------------------|----|------|----|----------|
| | | | | |
| | | | | |
| | | | | |

Project Title
 Consultancy services for preparation of detailed project report for Rehabilitation and upgrading of existing 3-lane road to 4-lane divided Category and upgrade of Jammu - Srinagar section of NH-1 A (Km 0 to Km 16.40) including Jammu - Srinagar and Lishnagar Bypass Phase - II Programme of North - South corridor Project, Central Package C-0914

Client
NATIONAL HIGHWAYS AUTHORITY OF INDIA
 (Ministry of Shipping, Road Transport & Highways)

Drawing Title: TYPICAL BOX CULVERT DETAIL WITH EARTH CUSHION
 Drawing No.: SW/1019/ROADS/MISC-17
 Scale: 1:50
 Dn: KLN
 Appd: K.PRASAD
 Date: June-2009

Sheet: (4 OF 4)
 NEW DELHI - 110044
 Tel: (011) 41673340 - 49
 Fax: (011) 41673350
 E-mail: svn@nhai.com

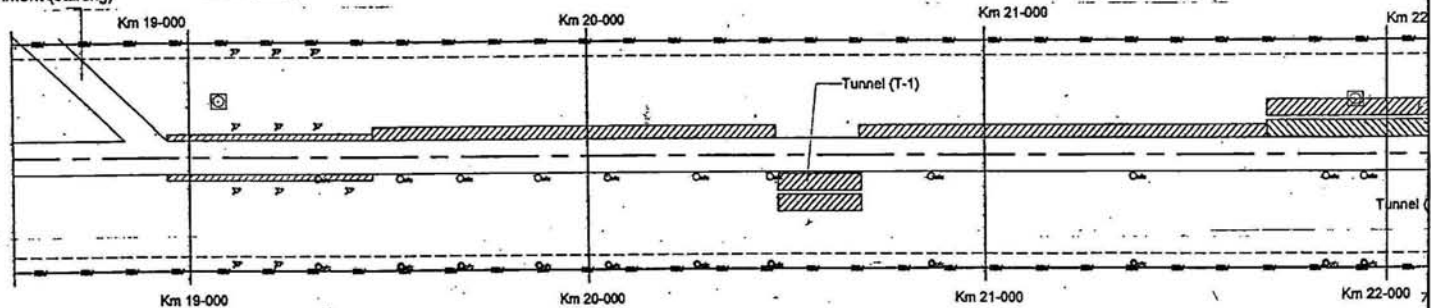
Scott Wilson India Private Limited
 K-26/4, MOHAN CO-OPERATIVE IND. ESTATE
 MATHERA ROAD
 NEW DELHI - 110044
 Tel: (011) 41673340 - 49
 Fax: (011) 41673350
 E-mail: svn@nhai.com



207

New alignment (starting)

End of Nagrola Bypass



LEGEND

- | | | | | | | | |
|--|-----------------|--|----------|--|---------------------|--|-----------------|
| | Religious | | Nala | | Concentric widening | | Electrical Pole |
| | Electrical Pole | | Culvert | | Ecentric widening | | OFC |
| | Telephone Pole | | Bridge | | Tunnel | | Telephone Pole |
| | OFC | | HT Tower | | | | |

RELOCATE UTILITIES

| | | | | |
|--|--|--|--|--|
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| | | | | |
| | | | | |
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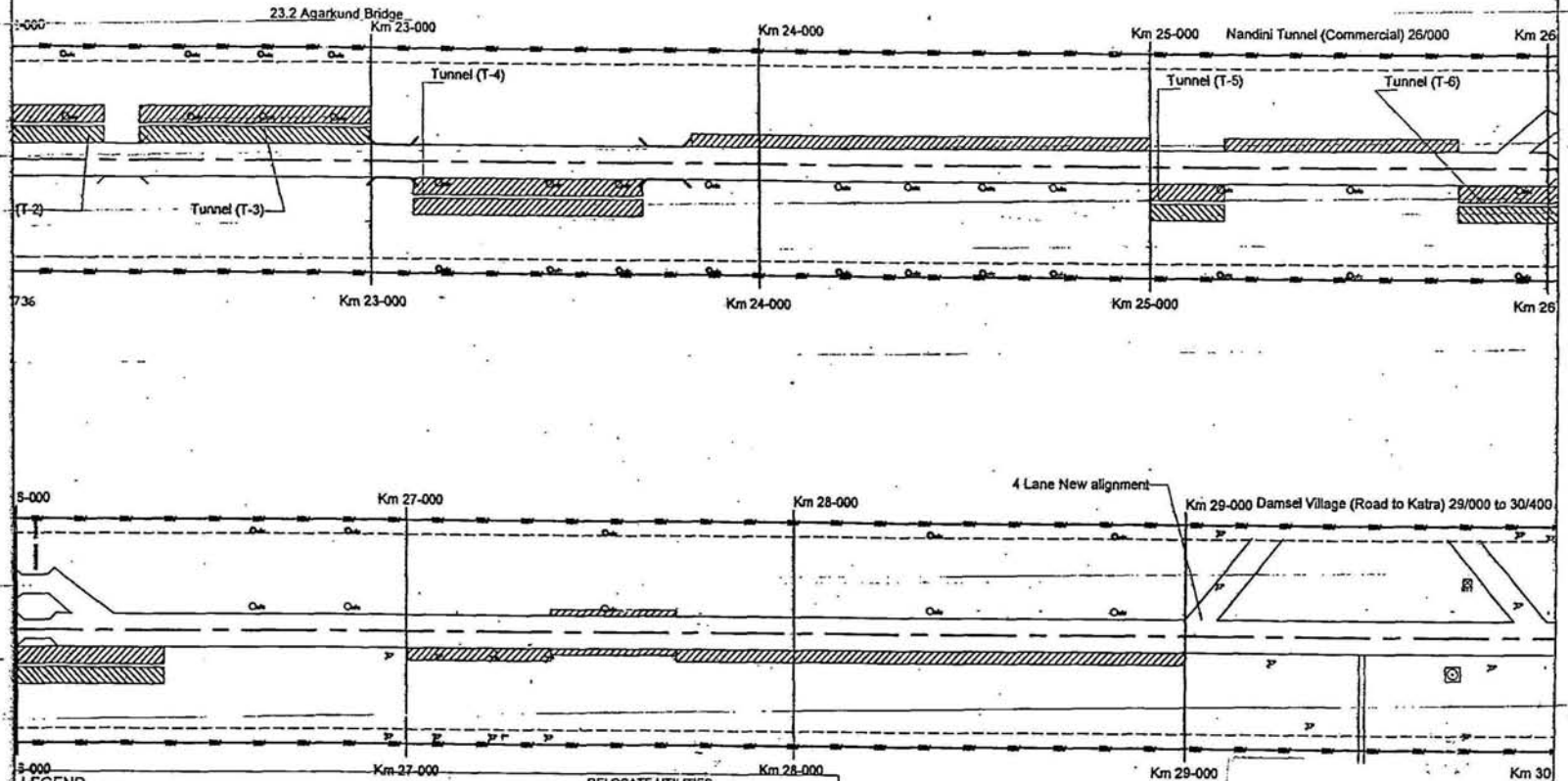
Project Title
 Preparation of Feasibility and Detailed Project Report for Four Lanes of National Highway No.1 from Jammu to Udhampur including Jammu Nagrola and Udhampur Bypass of North South Corridor in the state of J&K

Original Size: A3
 Plot Scale: 1:10000

Client
NATIONAL HIGHWAYS AUTHORITY OF INDIA
 (Ministry of Road Transport & Highways)

Drawing Title: **UTILITY SHIFTING STRIP PLAN**
 JAMMU - UDHAMPUR BARR ROAD SECTION
 EXISTING KM FROM 18.00 TO KM 22.00
 Drawing Number: **SNV/WR/NH-1A/UTL/PLAN**
 Scale: **NTS**
 Dn: **ABHARWAJ** Dgn: **AJUNAR** Appd: **K.PRASAD** Date: **FEB-09**

Professional seals and stamps of Wilson & Co Ltd. and Scol, including a circular seal for 'Wilson & Co Ltd. India Private Limited' and another for 'Wilson'.



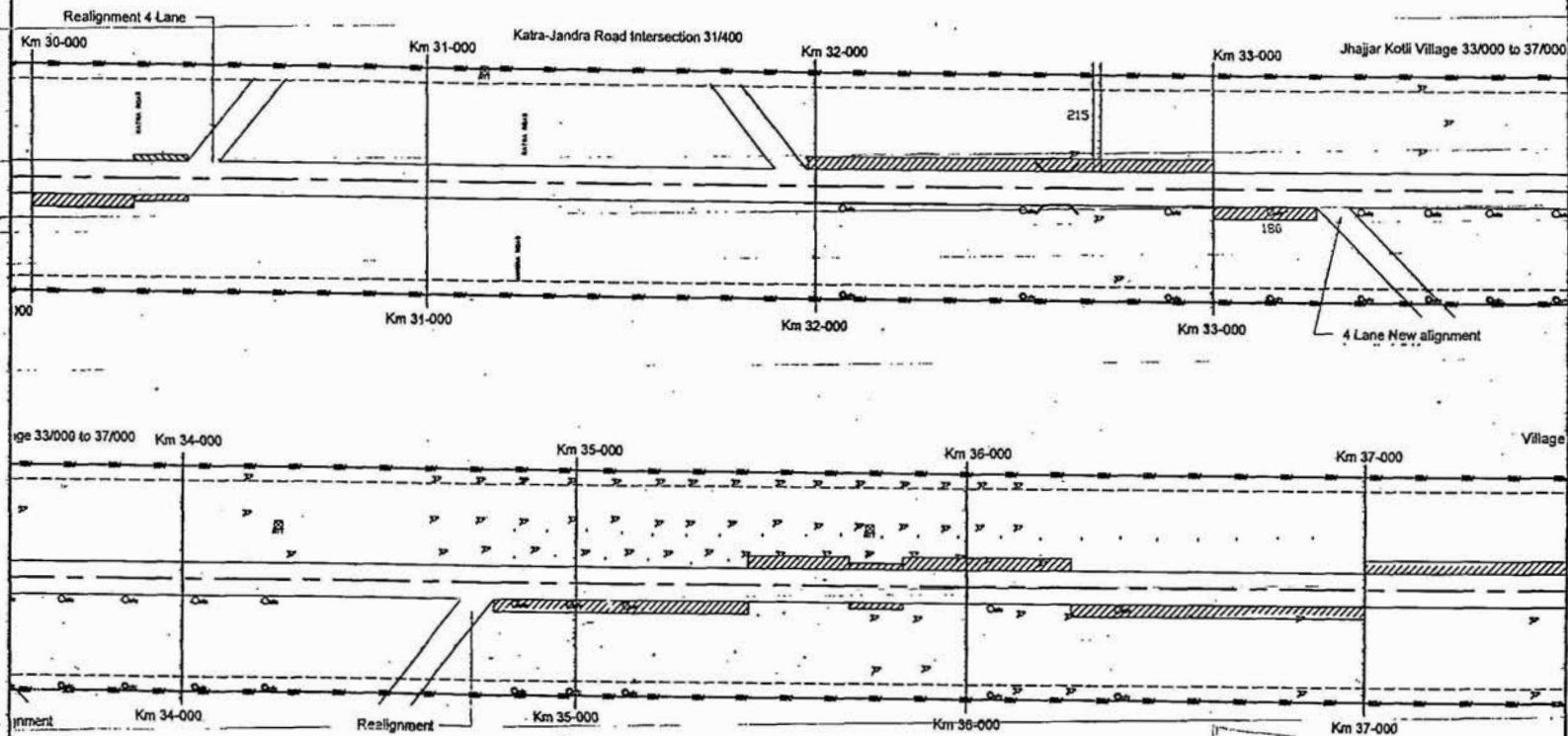
| LEGEND | | RELOCATE UTILITIES | |
|--------|---------------------|--------------------|-----------------|
| | Religious | | Electrical Pole |
| | Electrical Pole | | OFC |
| | Telephone Pole | | Telephone Pole |
| | OFC | | Nala |
| | Concentric widening | | Culvert |
| | Eccentric widening | | Bridge |
| | HT Tower | | Tunnel |

| | |
|--|------------|
| Project Title | |
| Preparation of Feasibility and Detailed Project Report for Four Laning of National Highway No.1 and from Jammu to Udhampur including Jammu Nagrota and Udhampur Bypass of North South Corridor in the state of J&K | |
| Original Size: A3 | Plot Scale |
| Revision Details | |
| By | Date |

Client: NATIONAL HIGHWAYS AUTHORITY OF INDIA (Ministry of Road Transport & Highways)

Drawing Title: UTILITY SHIFTING STRIP PLAN
 JAMMU - UDHAMPUR BAHU ROAD SECTION
 EXISTING KM FROM 23.00 TO 30.400
 Drawing Number: SW/NS/SH-W/UT/PLAN
 Scale: 1:1000
 Dn: A SHARDMAJ, Dgn: A KUNWAR, Appd: K PRASAD

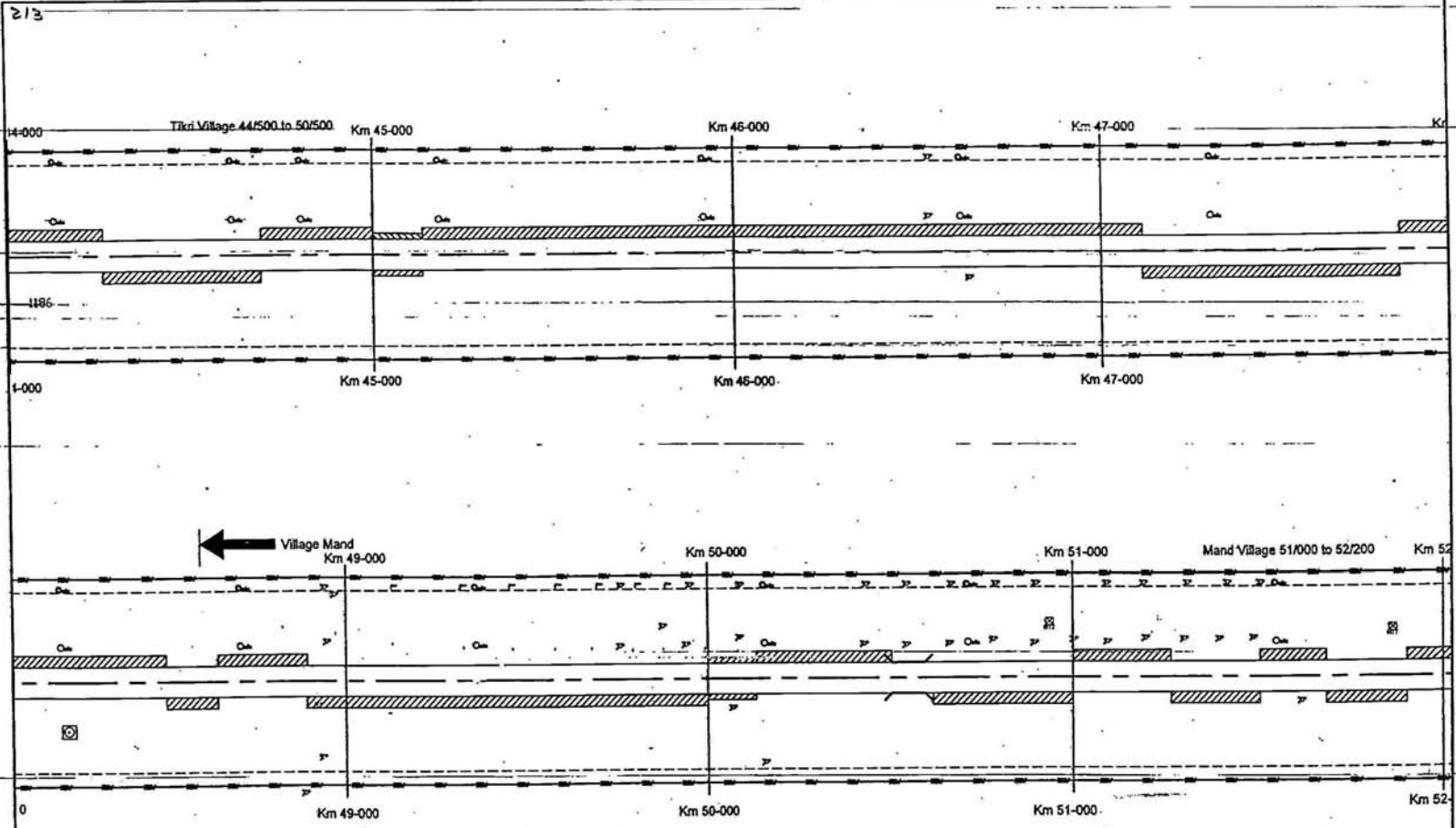
Scott Wilson & Co Ltd.
 &
 Scott Wilson India Private Limited
 110004
 310030
 310030
 310030



| LEGEND | | RELOCATE UTILITIES | |
|--------|-----------------|--------------------|---------------------|
| | Religious | | Electrical Pole |
| | Electrical Pole | | OFC |
| | Telephone Pole | | Telephone Pole |
| | OFC | | |
| | Nala | | Concentric widening |
| | Culvert | | Eccentric widening |
| | Bridge | | Tunnel |
| | HT Tower | | |

| | | | |
|---|---|--|---|
| Project Title Preparation of Feasibility and Detailed Project Report for Four Laning of National Highway No.1 & from Jammu to Udhampur including Jammu Magroia and Udhampur Bypass of North South Corridor in the state of J&K | Drawing Title: UTILITY SHIFTING STRIP PLAN JAMMU - UDHAMPUR Bypass ROAD SECTION POSTING KM FROM 30.000 TO 37.000 Drawing Number: SW/2010/100/100/100/100 Scale: --- Date: 01/03/09 | Client NATIONAL HIGHWAYS AUTHORITY OF INDIA (Ministry of Road Transport & Highways) | Scott Wilson & Co Ltd. & Scott Wilson India Private Limited A-2/4, 10th CROSSROAD, 4th CROSS NEW DELHI - 110028 Tel: (91) 11 261340 10 11 Fax: (91) 11 261340 11 Email: s.wilson@scottwilson.com |
| | | | |





| LEGEND | | RELOCATE UTILITIES | |
|--------|-----------------|--------------------|---------------------|
| | Religious | | Electrical Pole |
| | Electrical Pole | | OFC |
| | Telephone Pole | | Telephone Pole |
| | Oe | | |
| | Nala | | Concentric widening |
| | Culvert | | Eccentric widening |
| | Bridge | | Tunnel |
| | HT Tower | | |

| Revision | By | Date | Scale |
|----------|----|------|-------|
| | | | |
| | | | |
| | | | |

Project Title
Preparation of Feasibility and Detailed Project Report for Four Laning of National Highway No.1 A from Jammu to Udhampur including Jammu Nagrota and Udhampur Bypass of North South Corridor in the state of J&K

Original Size: A3
Path:XXXXXXXXXXXX

Plot Scale

Client
NATIONAL HIGHWAYS AUTHORITY OF INDIA
(Ministry of Road Transport & Highways)

Drawing Title: **UTILITY SHIFTING STRIP PLAN**
JAMMU - UDHAMPUR MAIN ROAD SECTION
EXISTING KM FROM 44.00 TO KM 52.00

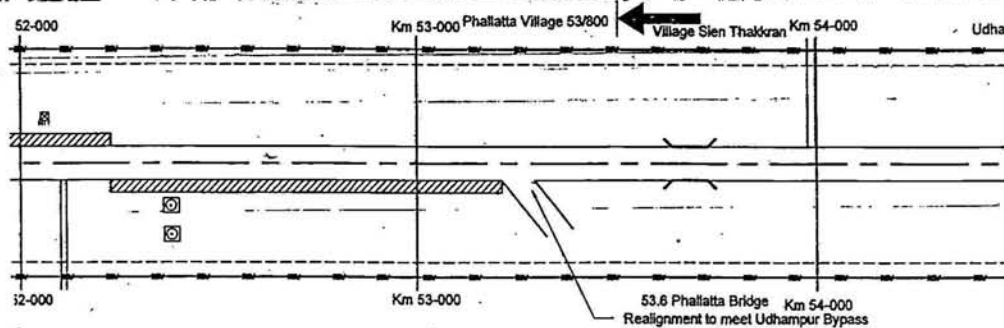
Drawing Number: **SW/NS/01-W/UT/PLAN**

Scale: **MTS**

Drawn: **A.B.HARDWAJ** Dgn: **A.KUMAR** Appd: **K.PRASAD** Date: **FEB-09**

Client
Scott Wilson & Co Ltd.
&
Scott Wilson India Private Limited
8-29/A, MOHAN COOPERATIVE HO. ROAD
NEW DELHI - 110044
Tel: +91(11) 2610340 to 44
Fax: +91(11) 2610320
E-mail: scottwilson@scottwilson.com





LEGEND

- Religious
- Electrical Pole
- Telephone Pole
- OFC
- Nala
- Culvert
- Bridge
- HT Tower
- Concentric widening
- Eccentric widening
- Tunnel

RELOCATE UTILITIES

- Electrical Pole
- OFC
- Telephone Pole

| | | | | | |
|-----|------|----|---------|---------|--|
| No. | Date | By | Checked | Suffice | Project Title Preparation of Feasibility and Detailed Project Report for Four Lanes of National Highway No.1 A from Jammu to Udhampur including Jammu Nagrota and Udhampur Bypass of North South Corridor in the state of J&K |
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Original Size: A3
 Plot No: XXXXXXXXXX
 Plot Scale: 1:1000

Client
NATIONAL HIGHWAYS AUTHORITY OF INDIA
 (Ministry of Road Transport & Highways)

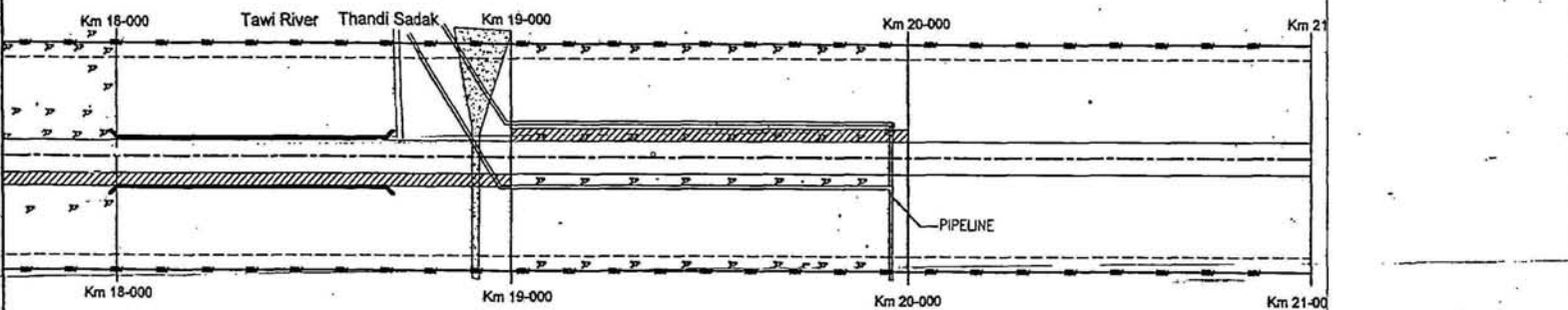
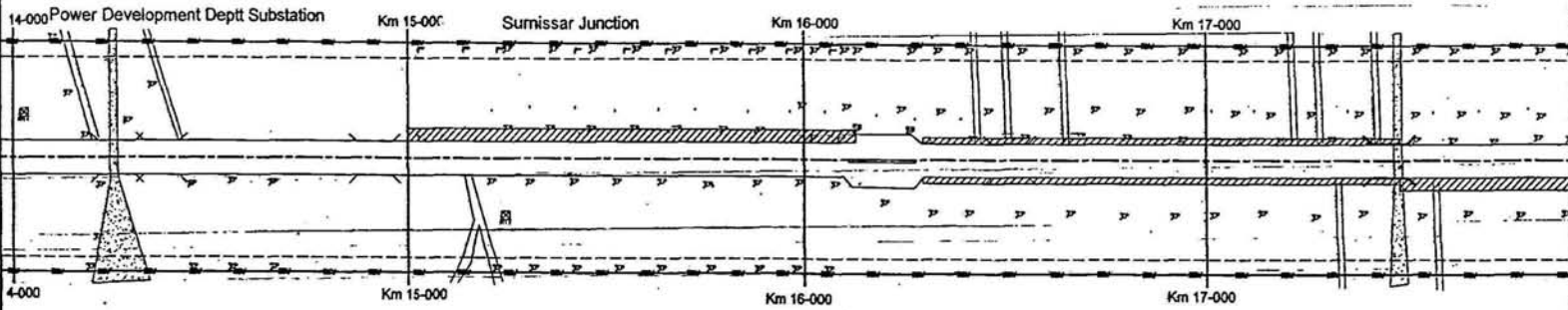
Drawing Title: **UTILITY SHIFTING STRIP PLAN**
 Jammu - Udhampur Bypass Road Section
 EXISTING KM FROM 52.00 TO KM 53.00

Drawing Number: SW/2018/NH-1A/UTILPLAN
 Scale: NTS
 Dtn: ALEKHARWAJ Dgn: AKUMAR Appd: K.PRASAD Date: FEB-09

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 &
 Scott Wilson India Private Limited
 A-20/4, Sector 29, Gurgaon, Haryana

NEW DELHI - 110044
 Tel: (011) 26102400 to 49
 Fax: (011) 26102100
 E-mail: info@scottwilson.com





LEGEND

| | | | |
|-----------------|----------|---------------------|-----------------|
| Religious | Nala | Concentric widening | Electrical Pole |
| Electrical Pole | Culvert | Eccentric widening | OFC |
| Telephone Pole | Bridge | Tunnel | Telephone Pole |
| OFC | HT Tower | | |

RELOCATE UTILITIES

| | |
|----------------|-----------------|
| EP | Electrical Pole |
| OFC | OFC |
| Telephone Pole | Telephone Pole |

| Rev. | Issued | By | Date | Scale |
|------|--------|----|------|-------|
| | | | | |
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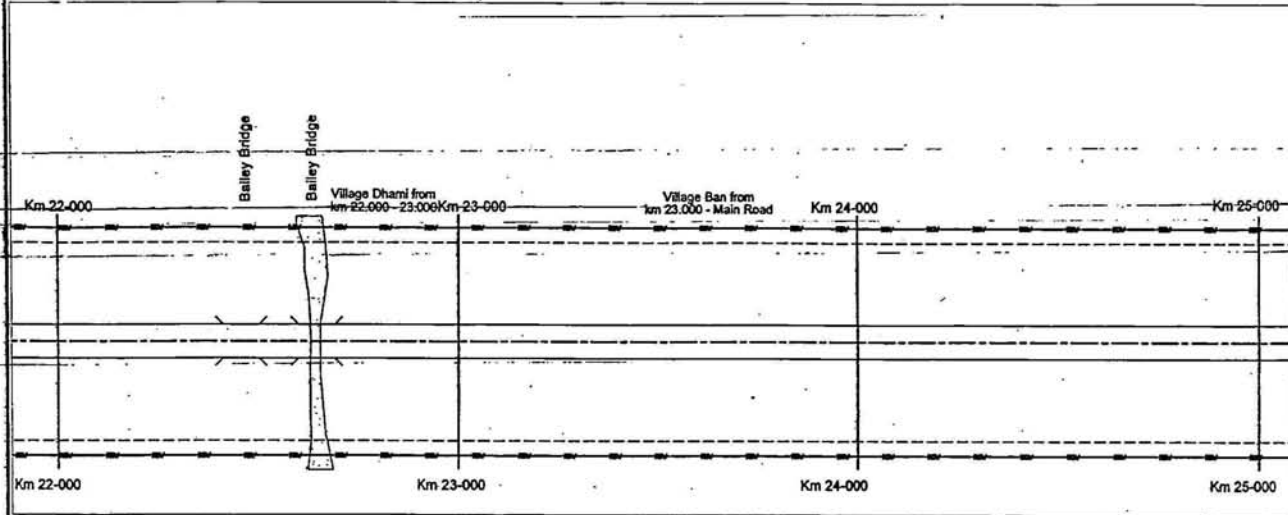
Original Scale: 1:1000
 Plot Scale: 1:1000

Client: **NATIONAL HIGHWAYS AUTHORITY OF INDIA**
 (Ministry of Road Transport & Highways)

Drawing Title: **UTILITY STRIP PLAN**
 DRAWING FOR SECTION 14.00 TO 21.00
 Drawing Number: **SW/HR/MS-1A/UTPLAN**

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 &
 Scott Wilson India Private Limited
 4-1/2A, ABOVE COOPERATIVE HO. BLDG.
 GROUND FLOOR
 NEW DELHI - 110048
 Tel: (011) 5167340 to 4
 Fax: (011) 5167350
 E-mail: india@swi.com





LEGEND

- Religious
- Electrical Pole
- Telephone Pole
- OFC
- Nala
- Culvert
- Bridge
- HT Tower
- Concentric widening
- Eccentric widening
- Tunnel

RELOCATE UTILITIES

- EP Electrical Pole
- OFC
- Telephone Pole

| | | | | | | | |
|--|-----|------|-------|--|--|--|--|
| Revision Details | | | | | | | |
| By | Chd | Dote | Suffa | | | | |
| Project Title Preparation of Feasibility and Detailed Project Report for Four Lanes of National Highway No.1 & from Jammu to Udhampur including Jammu Nagrota and Udhampur Bypass of North South Corridor in the state of J&K | | | | | | | |

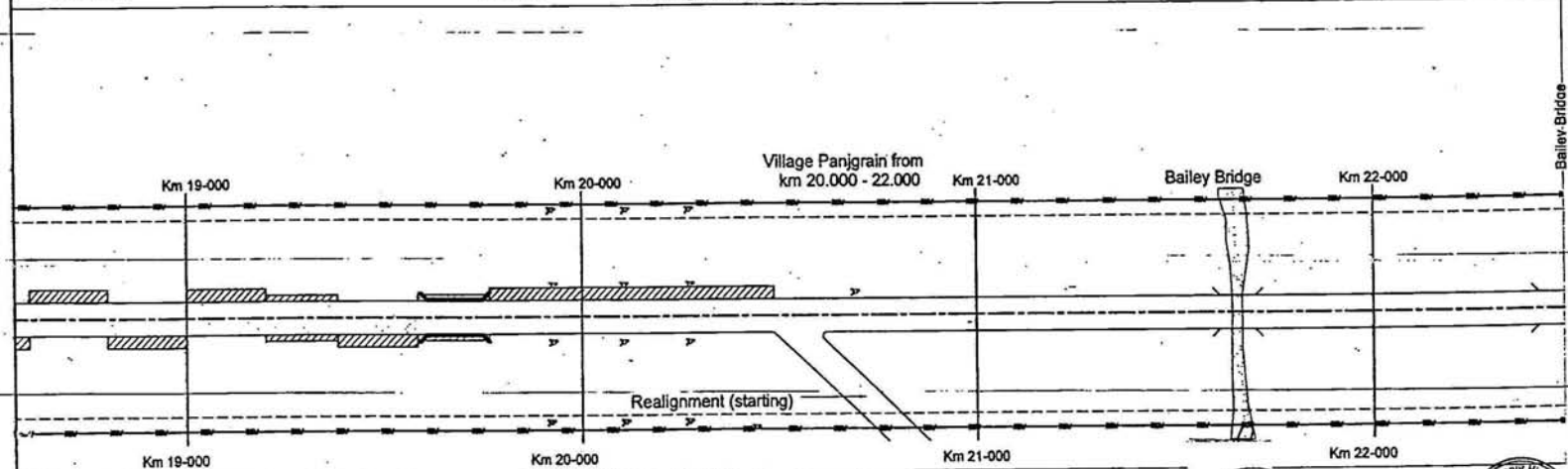
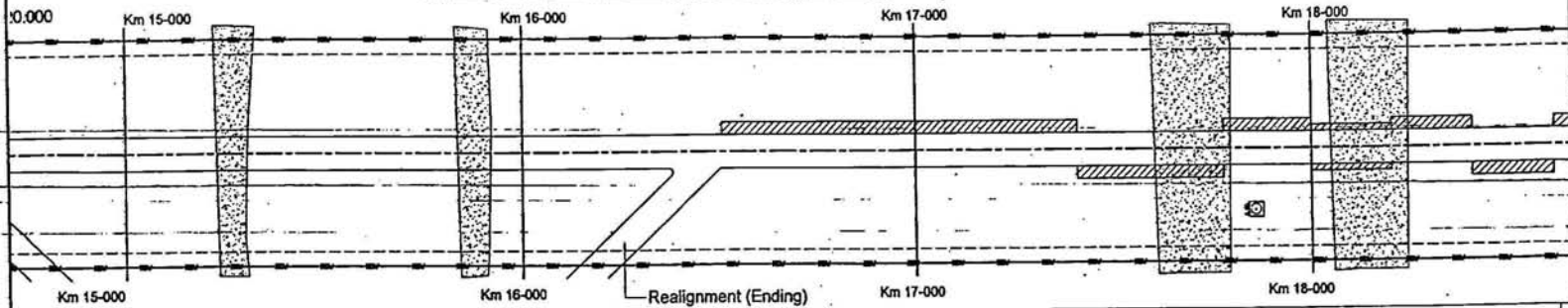
Original Size: A3
 Plot Scale: 1:1000
 Date: 15/02/09

Client
NATIONAL HIGHWAYS AUTHORITY OF INDIA
 (Ministry of Road Transport & Highways)

Drawing Title: **UTILITY STRIP PLAN**
 JAMMU - UDHAMPUR MAIN ROAD & NAGROTA BYPASS
 EXISTING KM FROM 22.00 TO KM 25.00
 Drawing Number: **SWU/N/S/06-1A/UT/PLAN**
 Scale: **NTS**
 Dm: **ALBHARDWAJ** Dgn: **A.KUMAR** Appd: **K.PRASAD** Date: **FEB-09**

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 NEW DELHI - 110044
 Tel: 91(011) 2679340 to 48
 Fax: 91(011) 2679328
 E-mail: info@scottwilson.com





| LEGEND | | RELOCATE UTILITIES | |
|--------|----------------|--------------------|---------------------|
| | Religious | | Electrical Pole |
| | Telephone Pole | | OFC |
| | Cable OFC | | Telephone Pole |
| | Nala | | Concentric widening |
| | Culvert | | Eccentric widening |
| | Bridge | | Tunnel |
| | HT Tower | | |

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Project Title
Preparation of Feasibility and Detailed Project Report for Four Laning of National Highway No.1 A from Jammu to Udhampur including Jammu Nagrota and Udhampur Bypass of North South Corridor in the state of J&K

Client
NATIONAL HIGHWAYS AUTHORITY OF INDIA
(Ministry of Road Transport & Highways)

Original Scale: A3
Plot Scale: 1:10000

Drawing Title: **UTILITY SHIFTING STRIP PLAN**
JAMMU - UDHAMPUR MAIN ROAD & MAGROTA BYPASS
EXISTING KM FROM 15.00 TO KM 22.00

Drawing Number: **SM/W9/NH-1A/UTPLAN** Sheet:

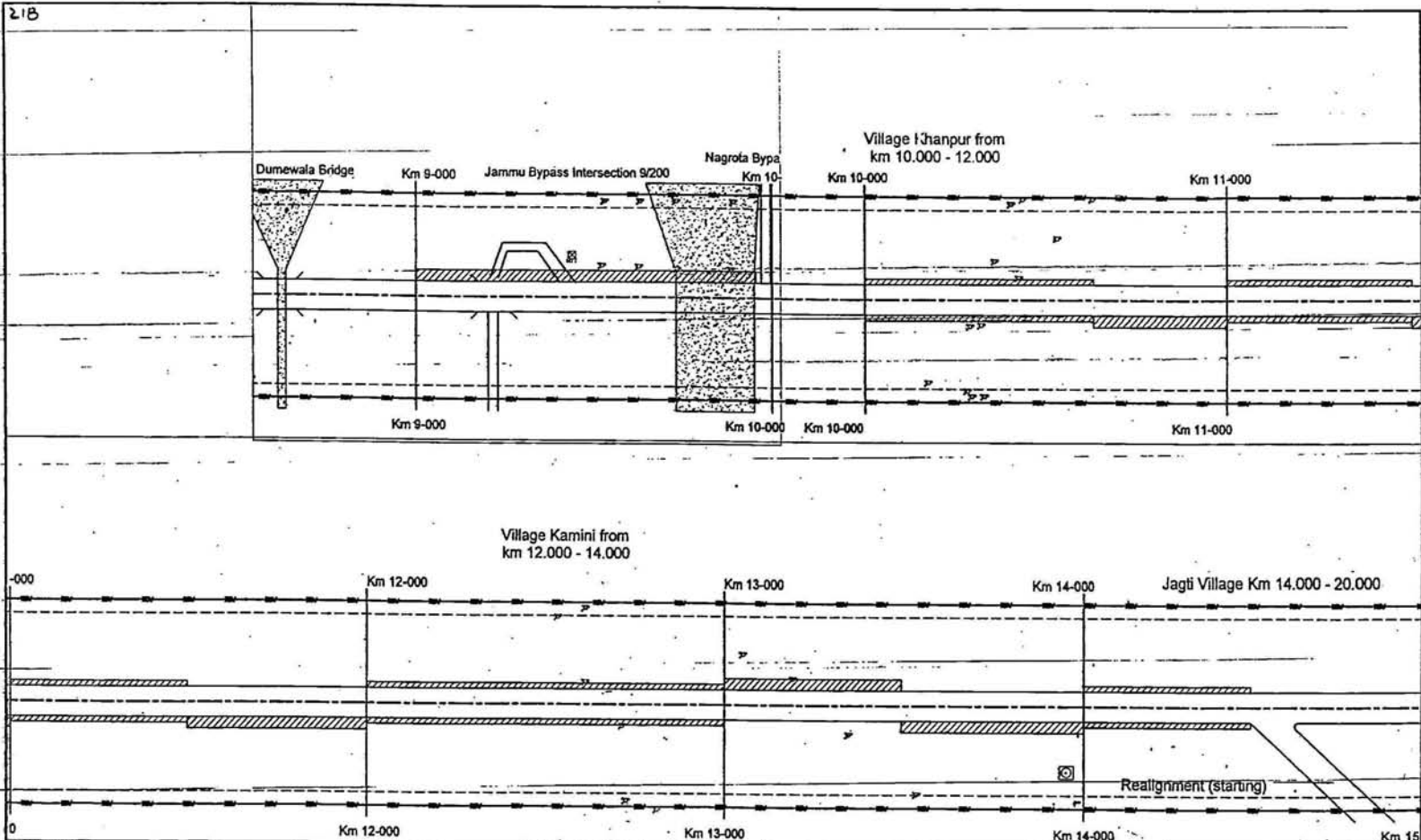
Scale: 1:10000

Disc: Dgn: Appd: Date:

A.SHARDAWAL A.KUMAR K.PRASAD FEB-09

Scott Wilson & Co Ltd.
Scott Wilson India Private Limited
A-16/4, Mohan Cooperative Ind. Estate
New Delhi - 110044
Tel: (91) 11 267140 to 44
Fax: (91) 11 267150
E-mail: scwilson@scwilson.com



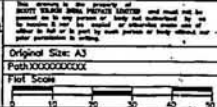


LEGEND

| | | | |
|----------------|----------|---------------------|-----------------|
| Religious | Nala | Concentric widening | Electrical Pole |
| Telephone Pole | Culvert | Eccentric widening | OFC |
| OFC | Bridge | Tunnel | Telephone Pole |
| | HT Tower | | |

RELOCATE UTILITIES

| | |
|----------------|--|
| Project Title | Preparation of Feasibility and Detailed Project Report for Four Lanes of National Highway W-1 & from Jammu to Udhampur including Jammu Nagrota and Udhampur Bypass of North South Corridor in the state of J&K |
| Original Size: | A3 |
| Plot Scale: | 1:1000 |



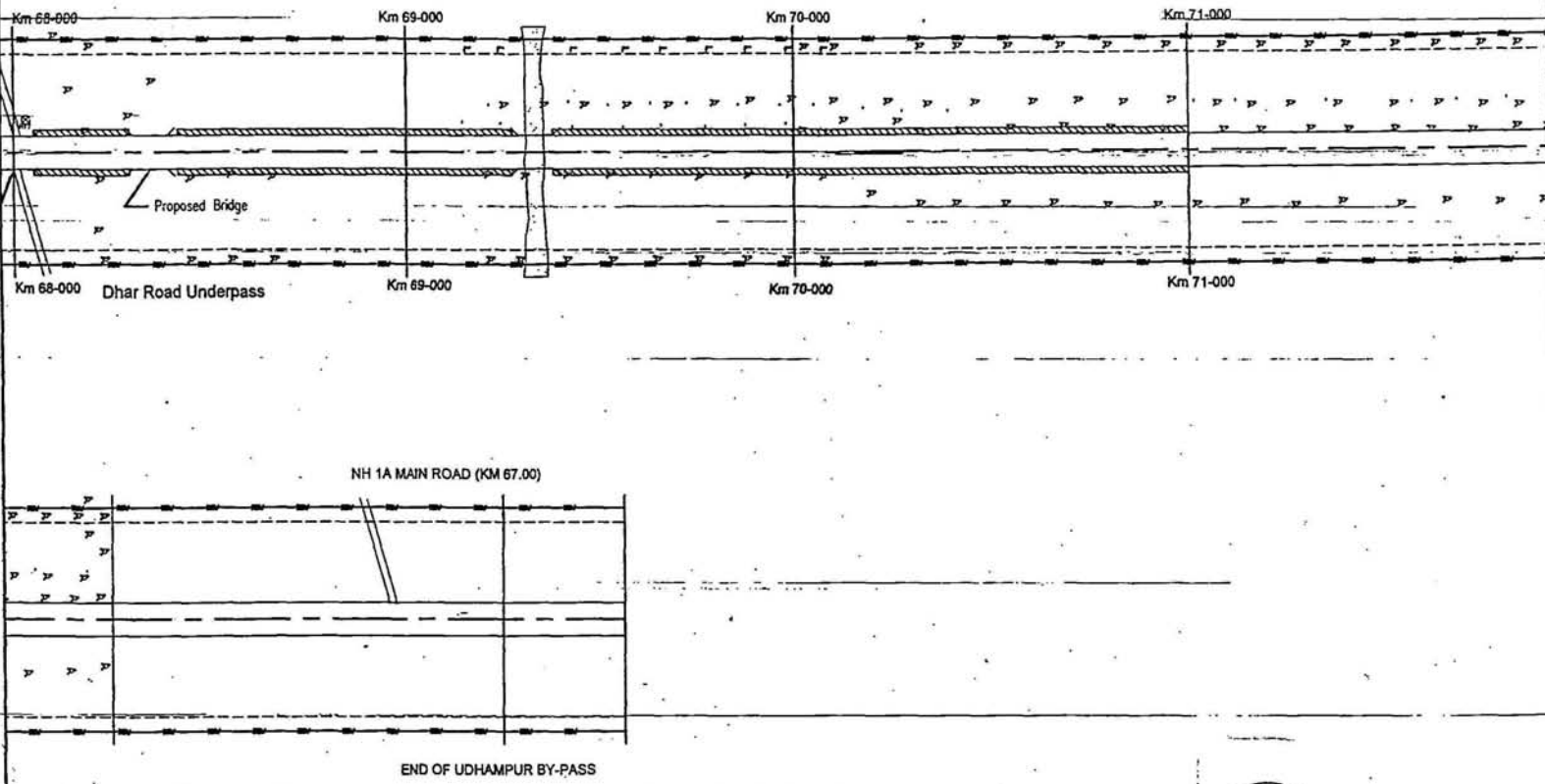
Client
NATIONAL HIGHWAYS AUTHORITY OF INDIA
 (Ministry of Road Transport & Highways)

Drawing Title: **UTILITY SHIFTING STRIP PLAN**
 JAMMU - UDHAMPUR MAIN ROAD & BYPASSES
 EXISTING ROW FROM 0 TO KM 15.000
 Drawing Number: **SVU/WB/NH-1A/UTPLAN**
 Scale: **1:1000**
 Dn: **A.BHARDWAJ** Dgn: **A.KUMAR** Appd: **K.PRASAD** Date: **FEB-09**

Scott Wilson & Co Ltd.
 &
 Scott Wilson India Private Limited
 4-20/4, BOWER CORPORATE, NEW GATE
 NEW DELHI - 110044
 Tel: +91(11) 26174340 to 48
 Fax: +91(11) 26174330
 E-mail: india@scotwilson.com



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| LEGEND | | RELOCATE UTILITIES | |
|--------|---------------------|--------------------|-----------------|
| | Religious | | Electrical Pole |
| | Electrical Pole | | OFC |
| | Telephone Pole | | Telephone Pole |
| | OFC | | |
| | Nala | | |
| | Concentric widening | | |
| | Cutvert | | |
| | Eccentric widening | | |
| | Bridge | | |
| | Tunnel | | |
| | HT Tower | | |

| Revision | By | Chk | Date | Suffix |
|----------|----|-----|------|--------|
| | | | | |
| | | | | |

Project Title
Preparation of Feasibility and Detailed Project Report for Four Lining of National Highway No-1 & from Jammu to Udhampur including Jammu Nagrota and Udhampur Bypass of North South Corridor in the state of J&K

Original Size: A3
Permit No: XXXXXXXXXXXX
Plot Scale: 1:5000

Client
NATIONAL HIGHWAYS AUTHORITY OF INDIA
(Ministry of Road Transport & Highways)

Drawing Title: **UTILITY SHIFTING STRIP PLAN**
UDHAMPUR BYPASS SECTION
EXISTING KM FROM 68.00 TO KM 71.00

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Scale: NTS

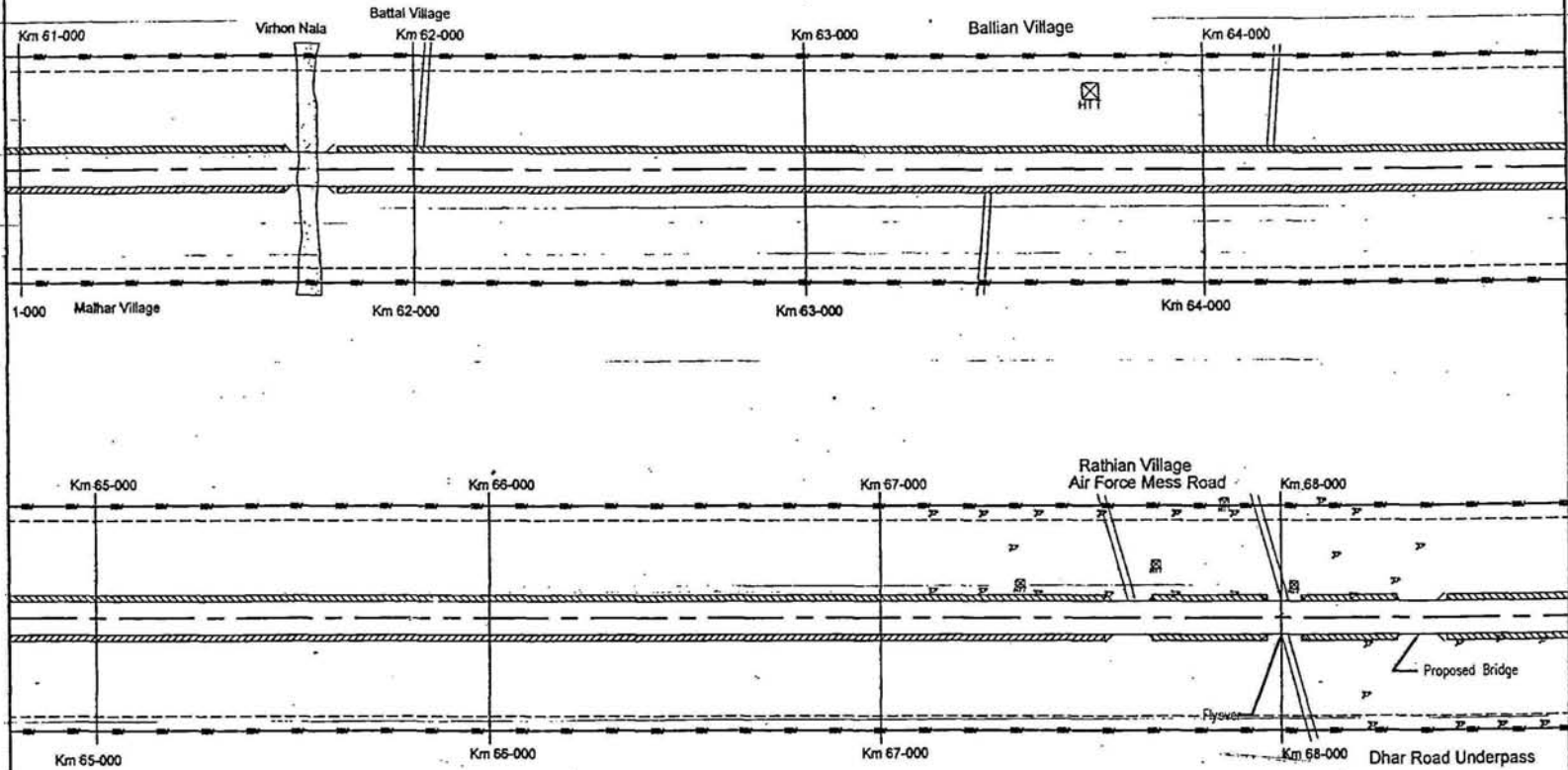
Drn: ALBHARTI/NA-2
Dgn: ALJUNAR
Appd: K.PRASAD
Date: FEB-09

Scott Wilson & Co Ltd.
&
Scott Wilson India Private Limited
A-24/4, WINDY COOPERATIVE HO. ESTATE
New Delhi - 110044
Tel: (011) 26125448 to 49
Fax: (011) 26125320
E-mail: scottwilson@rediffmail.com

Scott Wilson



220



| LEGEND | | RELOCATE UTILITIES | |
|--------|-----------------|--------------------|---------------------|
| | Religious | | Nala |
| | Electrical Pole | | Concentric widening |
| | Telephone Pole | | Eccentric widening |
| | OFC | | Bridge |
| | | | HT Tower |
| | | | Culvert |
| | | | Tunnel |
| | | | Electrical Pole |
| | | | OFC |
| | | | Telephone Pole |

| | |
|--|------|
| Project Title | |
| Preparation of Feasibility and Detailed Project Report for Four Laning of National Highway No.1 A from Jammu to Udhampur including Jammu Nagrota and Udhampur Bypass of North South Corridor in the state of J&K | |
| Revision | Date |
| | |
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| | |

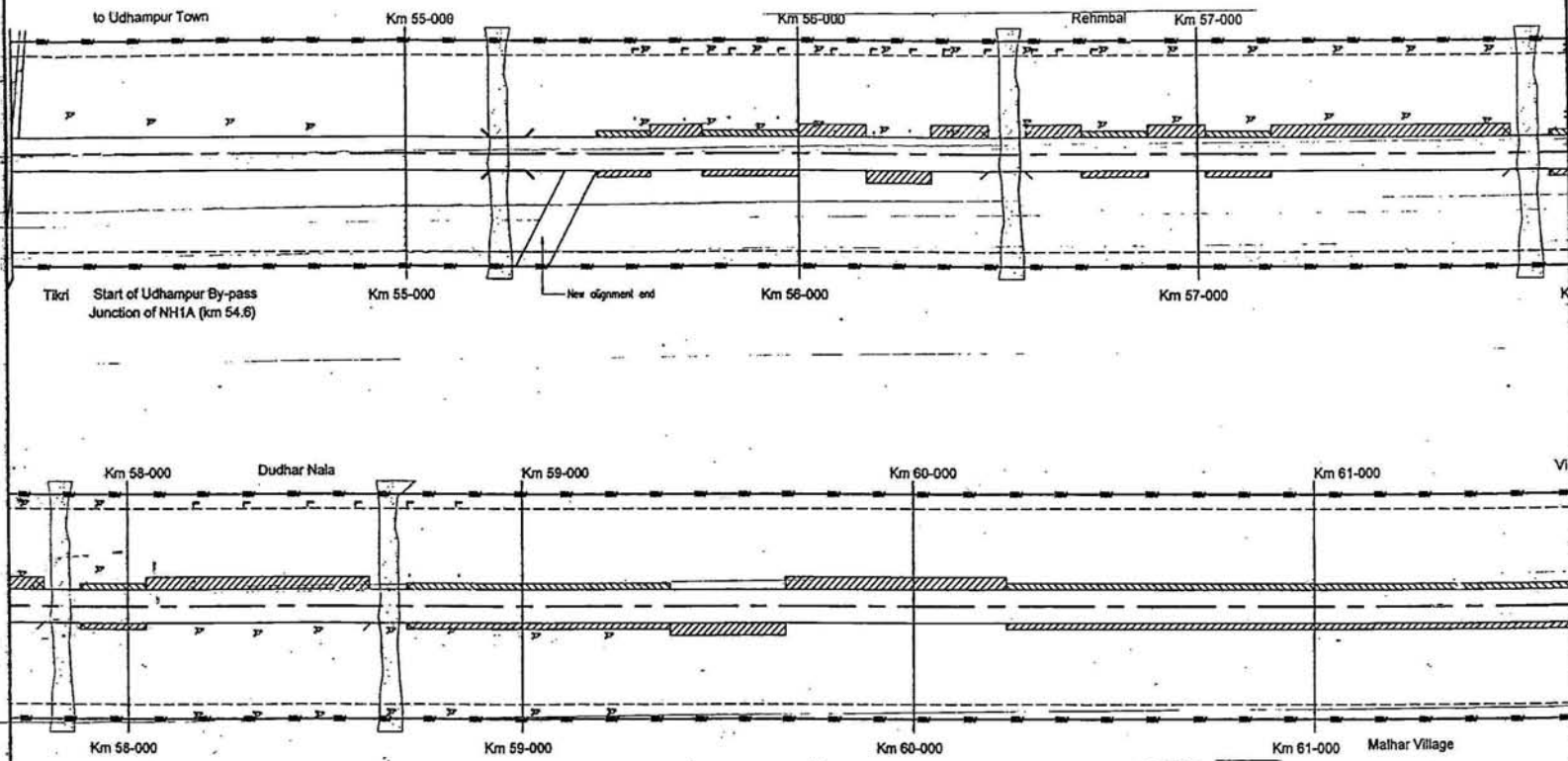
Scale: 1:1000
 Original Size: A3
 Plot Scale: 1:1000

Client
NATIONAL HIGHWAYS AUTHORITY OF INDIA
 (Ministry of Road Transport & Highways)

Drawing Title: **UTILITY SHIFTING STRIP PLAN**
 Drawing Number: SW/19/161-1A/UTLPLAN
 Scale: HTS
 Date: FEB-09

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 &
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 NEW DELHI - 110044
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 E-mail: scottwilson.com





LEGEND

| | | | |
|-----------------|----------|---------------------|---------------------------|
| Religious | Nala | Concentric widening | RELOCATE UTILITIES |
| Electrical Pole | Culvert | Eccentric widening | Electrical Pole |
| Telephone Pole | Bridge | Tunnel | OFC |
| OFC | HT Tower | | Telephone Pole |



| | | | | | |
|------------------|----|------|-------|---------|-------|
| Revision Details | By | Date | Drawn | Checked | Scale |
| | | | | | |

Project Title
Preparation of Feasibility and Detailed Project Report for Four Laning of National Highway No.1 & from Jammu to Udhampur including Jammu Nagrota and Udhampur Bypass of North South Corridor in the state of J&K

Original Size: A3
Path: XXXXXXXXXXXXX
Plot Scale: 1:XXXXXX

Client
NATIONAL HIGHWAYS AUTHORITY OF INDIA
(Ministry of Road Transport & Highways)

Drawing Title: UTILITY SHIFTING STRIP PLAN
UDHAMPUR BYPASS SECTION
EXISTING KM FROM 55.00 TO KM 61.00

Drawing Number: SW/19/SH-1A/UTPLAN
Scale: 1: NTS
Date: FEB-09

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